

SUGAR FACTORY + STEAM: SUB AREA PLAN

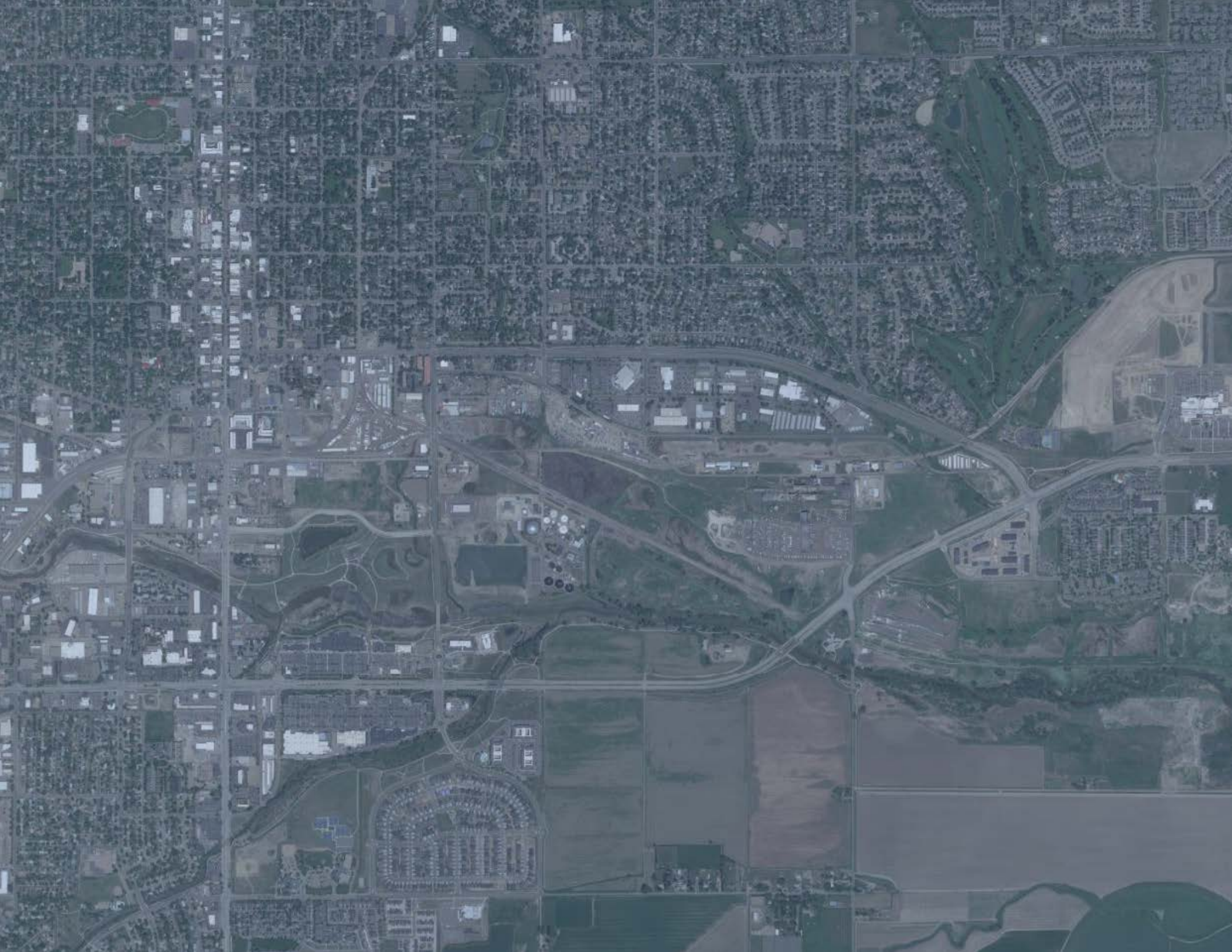
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EXECUTIVE SUMMARY

EXECUTIVE SUMMARY

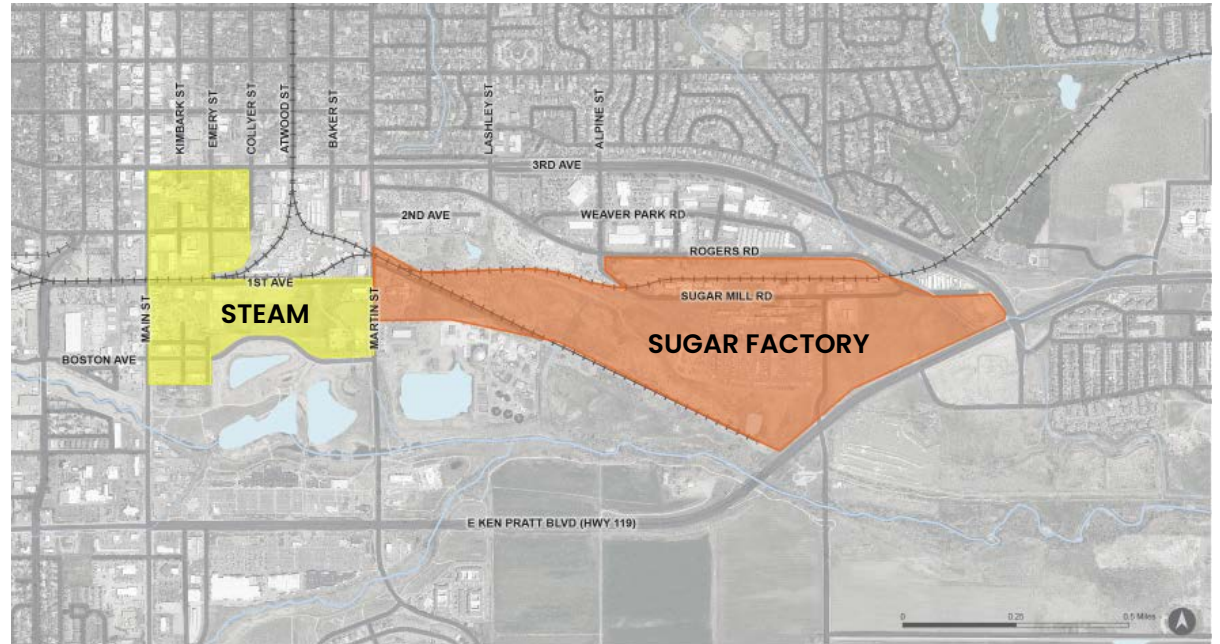
Project Need

This sub-area plan includes two distinct planning areas: the Sugar Factory area and the STEAM area. The interrelated connectivity challenges and redevelopment opportunities of these two areas sets forth the need for a joint planning effort. In addition, the two areas are linked in terms of storm drainage and high-level utility frameworks. An integrated sub-area plan is needed to ensure that development in the two areas is coordinated and focused, and avoids direct market competition that could have negative impacts on one or both areas.

Project Scope

The study area encompasses the three properties that constituted the historic Great Western Sugar Factory site and the area surrounding it, as well as the 'STEAM' area generally bounded by Main Street, 3rd Avenue, Martin Street and Boston Avenue. The two areas together comprise approximately 284 acres and 83 land parcels: 200 acres and 16 parcels in the Sugar Factory area and 84 acres and 67 parcels in the STEAM area.

This plan contains a development framework that provides guidance in three broad areas: land use, character and urban design, and mobility. It evaluates opportunities and challenges in each of the three areas, and provides strategic recommendations and implementation considerations.



The sub-area study area comprises approximately 284 acres and 83 parcels. Martin Street bisects the two areas, with STEAM (shown in yellow) to the west and Sugar Factory (shown in orange) to the east.

Project Process

Market update. The sub-area plan uses market data from two sources: the December 2020 Longmont Area Market Assessment, and a new retail observations/residential market snapshot. Acknowledging the significant market shifts precipitated by the COVID-19 pandemic, the study also rechecked the market assumptions and demand potential from the 2020 report.

Community Outreach. The study deployed a robust public outreach process that included a spectrum of in-person and digital input opportunities, such as pop-up information at community events, virtual community forums, city council presentations and elementary school activities. Input was largely supportive of development in both the STEAM and Sugar Factory areas, with particular interests in affordable housing and sustainability.

Sugar Factory Strategy

Development in this area will focus restoration and reuse of the historic sugar factory buildings as the core of the neighborhood, both physically and philosophically. Land uses will focus on an urban mix of uses with a medium-density residential focus and an emphasis on building a walkable neighborhood. The community was particularly favorable toward an urban agriculture sub-theme in the area, which could be exhibited in a variety of ways that might include neighborhood gardens, greenhouses, limited entertainment or educational facilities. Notable challenges in the Sugar Factory area are the need for extensive brownfield mitigation in advance of development and extremely limited circulation opportunities to connect with adjacent development. Opportunities include demonstrated developer interest and the large size and 'blank slate' nature of the area.

STEAM Strategy

With several development projects already proposed and in progress, the STEAM framework focuses on creation of cohesive neighborhood character and internal and external connectivity. A new performing arts/cultural center represents one sizeable land use opportunity that could also significantly influence character-setting in the area. Like the Sugar Factory area, the

area is anticipated to be very flexible in terms of land use, but with a more robust mix of commercial and retail interspersed with medium-density residential. Area challenges include existing rail alignments and their noise and vibration impacts on nearby uses, while opportunities include the continuation of Longmont's successful Main Street and direct access to well-developed open space resources in the St. Vrain Greenway and Dickens Farm Natural Area.

Implementation

Having largely industrial pasts, both the STEAM and Sugar Factory areas have relatively permissive development policies that will require some level of zoning modifications and/or policy overlays to promote the City's vision for the overall sub-area. Significant area for review will include heights, setbacks, street sections, and park/open space requirements and management.

The sub-area will need to use a variety of public and private funding mechanisms in combination to bring concepts to fruition. These tools include but are not limited to public private partnerships (PPP), urban renewal district (URD) tax increment financing, and a variety of district structures such as general improvement districts (GID), business improvement districts (BID), and metropolitan districts.



Image: rbbarchitects.com



The STEAM area offers a mix of popular destinations and opportunity parcels (top), while the Sugar Factory area is more of a blank slate with industrial history (bottom).

INTRODUCTION

1.

- 1.1 PROJECT NEED
- 1.2 PROJECT SCOPE
- 1.3 RELATED PLANNING
- 1.4 HOW TO USE THIS DOCUMENT

1.1 PROJECT NEED

TWO AREAS, ONE PLAN

As the name suggests, this sub-area plan includes two distinct planning areas: the Sugar Factory area and the STEAM area. Although each area has very distinct characteristics, their interrelated circulation and connectivity challenges and redevelopment opportunities sets forth the need for joint planning.

Adjacency

Although east-west connections between the two areas is sparse at present, there is hope that mid- to long-term potential to provide essential transportation connections between the areas will evolve. The two areas are also linked in terms of storm drainage and high-level utility frameworks, so that a joint plan can identify complementary improvements or studies that may be necessary and impact both areas.

Industrial Scale

Both areas are anticipated to transition from a hodge-podge collection of marginal commercial, storage and industrial uses to a more diversified and urbanized mix of uses. These anticipated changes call for the definition of a new street block structure

and roadways, as well as the potential for policy changes relative to zoning, regulatory overlays, funding districts and the like. Planning both areas together ensures that these potential changes are uniformly applicable.

Complementary Uses

The areas' proximity to each other can breed development competition that, unless given direction, can lead to the detriment of both areas. Accordingly, it is important to create a flexible development framework that distributes an appropriate mix of land uses in both areas, but avoids direct competition so that one area's gain is not the other's loss.

Development Pressure

Both areas are experiencing current development interest and activity, whether inquiries or development plan submittals to the City, leading to a pressing need to establish a guiding development framework to ensure that new development in these areas aligns with Longmont's community vision. Planning the areas together provides the necessary development guidance as expediently as possible, establishing concrete expectations for both developers and the community.

WHAT'S IN A NAME?



The iconic cluster of brick buildings, silos, smokestack and tin shed located on Sugar Mill Road and visible from Colorado State Highway (SH) 119 are well-known to most Longmont residents and readily visible to travelers entering the City from the east. Once owned by the Great Western Sugar Company, the facility once produced over a million pounds of sugar every day, and is a very tangible and significant tie to the City's history and growth. Although commonly referred to it as the Sugar Mill, the facility was operated as a factory. The sub-area plan process began with the name 'Sugar Mill' and was changed during the process to the more accurate '**Sugar Factory**'.

The **STEAM** name, in contrast, reflects not a historic use but a forward-looking aspiration. First crafted in 2019, the **Science-Technology-Engineering-Art-Maker** acronym references a City-Council backed exploration that suggested this area south of existing downtown could redevelop as an innovation district focused on these creative and inventive interests.

1.2 PROJECT SCOPE

STUDY AREA

The study area encompasses the three properties that constituted the historic Great Western Sugar Factory site and the area surrounding it, as well as the ‘STEAM’ area generally bounded by Main Street, 3rd Avenue, Martin Street and Boston Avenue. The two areas together comprise approximately 284 acres and 83 land parcels: 200 acres and 16 parcels in the Sugar Factory area and 84 acres and 67 parcels in the STEAM area.

Also of note are the existing freight rail lines running through and adjacent to both areas. Significant additional abutting uses – presenting both opportunities and challenges – include the City’s water treatment plan (south of the Sugar Factory), the Dickens Farm Nature Area and the St. Vrain Creek corridor (south of both areas).

WHAT IS INCLUDED

This sub-area plan includes a guiding framework of governmental and community expectations and desires regarding Land Use, Urban Design and Character, and Mobility.

Plan Priorities define the key and desired outcomes of the sub-area plan. There are many ways to accomplish each outcome, and these priorities define the goal against which prospective development proposals should be measured: if they support, detract or are neutral relative to these topic-specific statements.

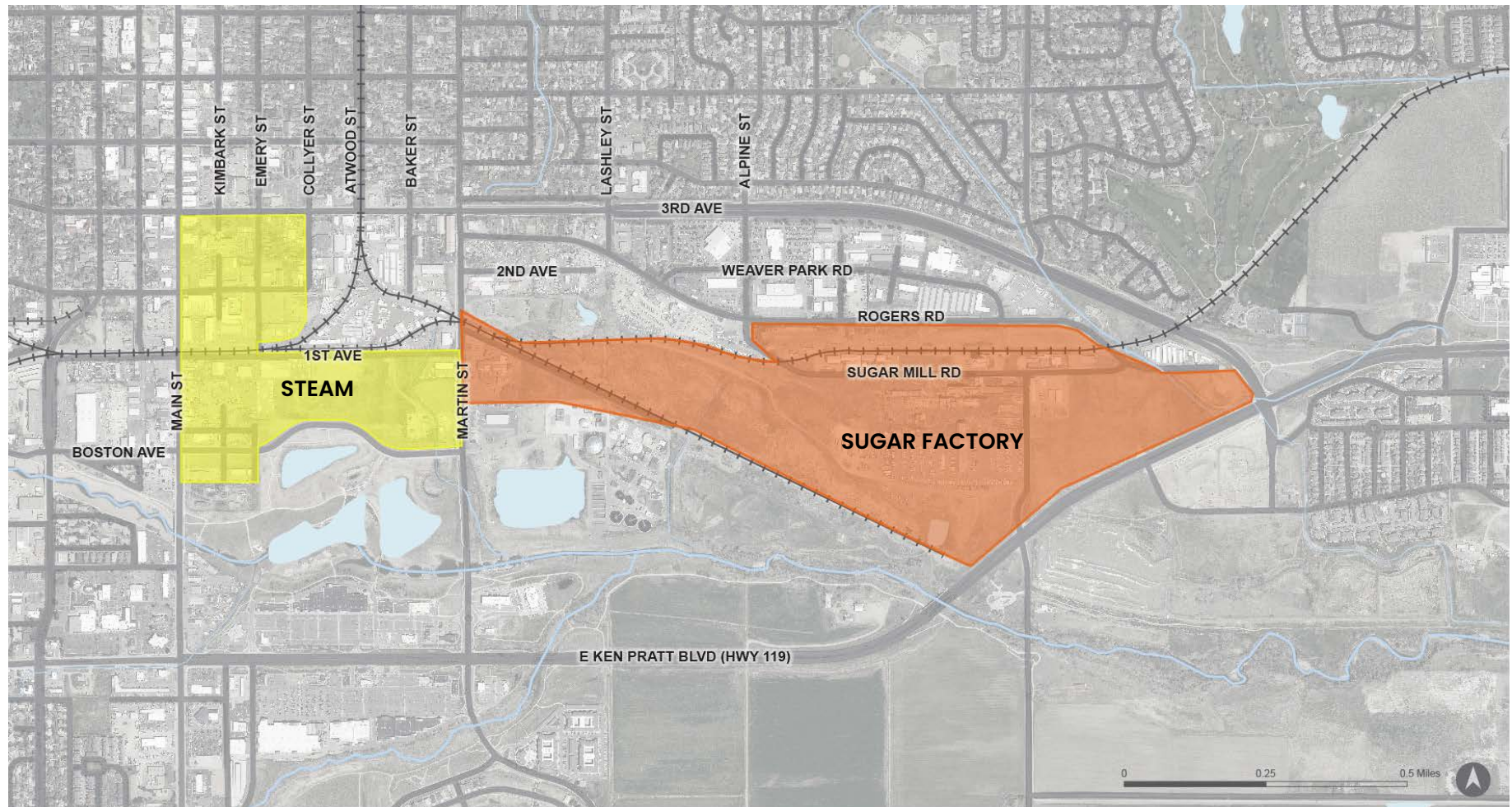
Land Use Strategies define preferred uses for each area. The recommended mix of uses is flexible, and is closely related to the architectural form of new development. In the case of the STEAM area, uses are intended to blend with existing uses that are expected to remain, primarily in the historic downtown section north of 2nd Avenue. In the Sugar Factory area, preservation and re-use of the existing historic structures is an emphasis and priority, and future uses are to be compatible with these buildings and each other.

Urban Design and Character Strategies

underline the importance of human experience and scale. They focus on creating a sense of place and creating a balanced community representative of Longmont’s goals and priorities as a community.

Mobility Strategies prioritize a multimodal approach to transportation network design that balances pedestrian, bicycle and vehicular needs and offers choice based on trip length, need, time of day, and even weather conditions. These strategies emphasize the connection between transportation and land use, and support area goals of offering mixed-use neighborhoods that reduce the need to travel to separate locations for employment, dining, living and recreation.

Implementation Considerations recognize a likely extended build-out period in each area, with evolving conditions based on market trends. In place of defining specific phases or implementation tools, this section notes things to be considered as development progresses.



The sub-area study area comprises approximately 284 acres and 83 parcels. Martin Street bisects the two areas, with STEAM (shown in yellow) to the west and Sugar Factory (shown in orange) to the east.

1.3 RELATED PLANNING

PRIOR EFFORTS

1st & Main Station Transit & Revitalization Plan (2012)

This study defined the concept of a multimodal transportation hub on the west side of the 1st Avenue & Main Street intersection abutting the STEAM area. This concept has been further developed by the City in cooperation with the Denver Regional Transportation District (RTD) and is as an important precursor to the multimodal strategies in this sub-area plan.

Downtown Longmont Master Plan of Development (2017)

The master plan establishes goals, policies and recommendation relative to the operations of the Longmont Downtown Development Authority (LDDA) and land use, design and development within the LDDA District boundaries. The plan has relevance to a significant portion of the STEAM sub-area covering the entire area north of 1st Avenue and a large 12 acre tract of undeveloped land sandwiched between 1st Avenue and Boston Avenue.

Main Street Corridor Plan (2019)

The Main Street Corridor Plan extends from State Highway 66 on the north to Plateau Road on the south, thus covering most of the STEAM area. The plan provides recommendations for land use and transportation that are applicable to the corridor, character areas and catalyst sites. This sub-area planning initiative took into consideration and expanded on the plan's recommendations.

Building STEAM Visioning Process (2019)

This visioning process directed by City Council focused largely on visioning redevelopment within the STEAM area focusing on land use mix, density and residential housing types. The process provided a number of recommendations on mobility and urban design. The STEAM name and recommendations were incorporated into the sub-area planning process, where the land uses envisioned were evaluated as having potential, while not being restrictive uses within the area.

ULI Technical Advisory Panel (TAP) Report: Revitalizing Longmont's Great Western Sugar Mill (2020)

This report was produced during a two-day virtual charrette in August 2020. The goal of this effort was to brainstorm ideas and financing options for the Sugar Factory area. A significant idea from this report that has been carried forward into the sub-area plan is the concept of developing the area as an "agri-hub" community, integrating agricultural elements and supporting initiatives as part of the new development in recognition of Longmont's and the Sugar Factory's agricultural heritage.

Art & Event Center Hotel Feasibility Study (2021)

This study evaluated the market for a performing arts and conference facilities in or in proximity to downtown Longmont; it detailed the type and size of facilities that were considered feasible given proximity to and competition from local and regional facilities and population growth within a capture area. This idea has been carried forward in the STEAM area noted as a Cultural & Event Center.

Technical Brownfields Assessments (TBA)

The U.S. Environmental Protection Agency (EPA), on behalf of the City and property owner, recently conducted Phase I and Phase II environmental assessments of the soils, water, and buildings for the property upon which the historic buildings are situated. The results of the testing are now being used in conjunction with the preparation of a remediation strategy, including funding and site development plans.

Resilient St. Vrain Project

For the last several years, the City of Longmont has proceeded with improvements to the St. Vrain Creek to mitigate flooding conditions. In conjunction with this effort the entirety of the STEAM area is to be removed from the 100-year floodplain, thus making the area once again viable for redevelopment. More recent channel improvements have effectively accomplished its removal subject to approval and remapping by the Federal Emergency Management Agency (FEMA).

CONCURRENT EFFORTS

Aggregate Extraction Mining and Restoration

A sliver of land abutting the Burlington Northern Santa Fe (BNSF) rail tracks in the Sugar Factory area is subject to mining permit. Upon completion of the aggregate extraction, the impacted land is to be restored and may become available for development. The timeline for extraction is not set at this time, but efforts are being made to accelerate the activity.

Current Development Proposals

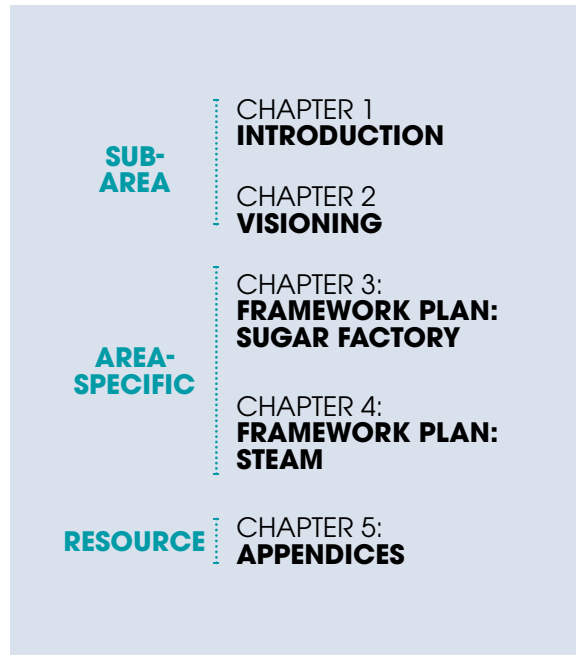
There has been heightened interest for redevelopment within both planning areas. There are currently development proposals in process for about 80% of the Sugar Factory area. There are three projects in process in the STEAM area, one of which comprises about 12 acres of land in a strategic central location. It is anticipated the removal of the STEAM area from the floodplain will result in another 12 acres or so going under contract for development quickly. (see map, page 48)



Image: coloradocreativecorridor.com

The 2021 Art & Event Center Hotel Feasibility Study outlined a facility with a variety of performance and practice spaces.

4. HOW TO USE THIS DOCUMENT



The sub-area plan contains both overarching and area-specific information.

Sub-Area Information

Chapter One provides an introduction to the study, including needs and parameters.

Chapter Two describes information that applies to both areas, including a market analysis, a summary of public engagement, and overarching opportunities and constraints.

Area Information

Recognizing that stakeholders are likely to be interested in a specific area, Chapters 3 and 4 are written as ‘pull-out’ chapters that provide area-specific recommendations. Chapter Three: Sugar Factory and Chapter Four: STEAM provide more detailed opportunities and challenges, as well as unique strategies and roadway sections.

Resources

Resource documents are provided in Chapter 5: Appendices. The full market report, existing conditions assessment, raw community engagement input, and interim alternatives can be found in this section.

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2. **VISIONING**

2.1 OUTREACH SUMMARY

2.2 OPPORTUNITIES AND
CONSTRAINTS

2.3 PLAN PRIORITIES

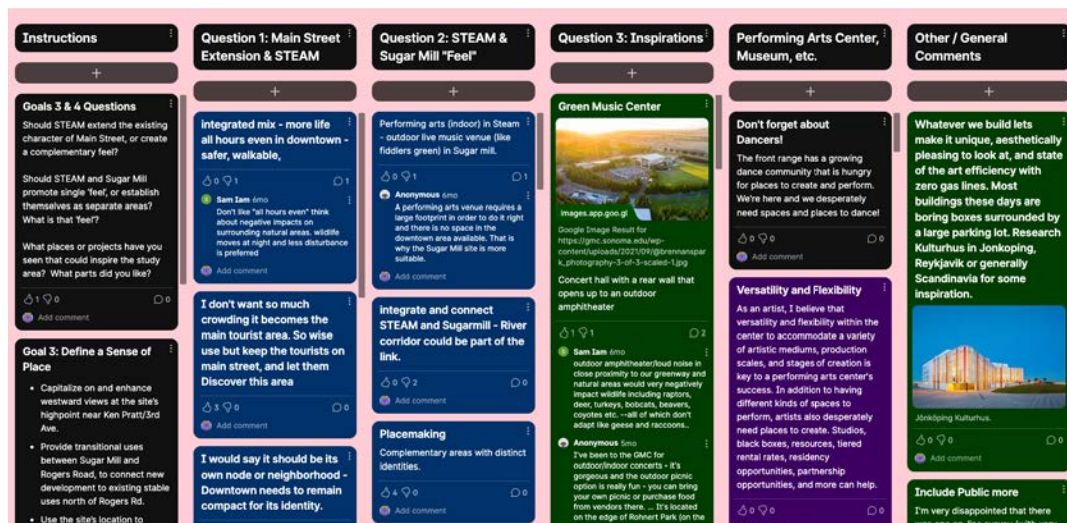
2.4 OVERALL IMPLEMENTATION

2.1 OUTREACH SUMMARY

OUTREACH GOALS

Early in the project, the team developed the following goals for community outreach and engagement, to ensure a thoughtful and meaningful process:

- Develop a solid understanding of the community's needs and interests and build on redevelopment efforts currently underway;
- Engage ALL affected stakeholders and create an inclusive approach to incorporate community voice;
- Gather community input and utilize it to shape the final plan.



A transcript of comments from the Community Forum (top) is available in this document's appendices; students paint their model of the Sugar Factory neighborhood (left); project information board with community comments from Sample the Slope event (right).

OUTREACH PROCESS

In order to meet those goals, the team worked on different fronts: online engagement, community events, stakeholder outreach, youth engagement, City Council updates and presentations, and boards and commissions updates and presentations. Through this process, we were able to engage with a large number of community members and stakeholders with diverse backgrounds and experiences, and to gather feedback rich in content. All these activities are summarized in the table at right.

Project Phase	Existing Conditions	Goals & Strategies	Alternatives	Preferred Plan
Timeline	Dec 2021-Jan 2022	Feb - Mar 2022	Apr - Jun 2022	Aug - Sep 2022
City Council Presentations	Briefing (12/14/21)		Briefing (3/29/22)	
City Boards & Commissions	Presentations & Q&A (TAB, PZC, APPC)	Presentations & Q&A (HPC, LDDA)	Presentations & Q&A (PRAB, SAB, ECAT)	
Engage Longmont	Project Goals Survey		Feedback on Plan Alternatives Survey	Share Out (Sep 22)
Community Events	Boards at Longmont Lights (12/10/21)	Community Online Forum (3/2/22) The Slope Event (3/12/22)	Earth Day Outreach (4/22/22)	
Stakeholders Outreach	Meetings with Stakeholders (12/1/21) & Property Owners (1/7/22)	Chamber Commercial Brokers Neighborhood Group Leaders	Developer Workshop (5/18/22)	Sugar Factory Agri Hub Discussion (developer led)
Youth Outreach		Elementary School Engagement Presentations Part 1: 3/10/22 Presentations Part 2: 4/25/22 CU Design Studio		
Total No. of Participants	500+	200+	500+	

WHAT WE HEARD

Data from these different outreach activities can be found in the appendices of this report, and is summarized by key themes here. Through the plan development process, these ideas were integrated and have shaped the final plan.

“ Seems to me we could get both affordable housing and urban greenery. Some features like green roofs.

“ I'm a renter in old town, I would move there if I could afford... Maybe 70% income bracket housing?

SUSTAINABILITY

- Encourage green and sustainable building materials and practices (e.g. rooftop solar panels)
- Integrate green infrastructure in the urban landscape
- Favor native & hardy low water plants
- Make space for natural habitat in parks and open spaces
- Re-purpose buildings where possible
- Include electric vehicle infrastructure

HOUSING TYPES

- Accommodate a variety of housing types, from multi-story apartments and condominiums to rowhouses and townhouses
- Make room for more attainable and affordable housing
- Include a mix of uses, such as residential, commercial, retail, community amenities (e.g. childcare, community space, co-working...)
- Be intentional about integrating uses for different demographic groups
- Ensure future developments are equitable & diverse

“YES to bike, pedestrian, last mile solutions, multiple “bike/ped-ways” to avoid conflict between pedestrians/ e-bikes, and other multi-modal technologies. Need to consider seniors, juniors and physically disabled and their last mile (wheelchair, visibly impaired, etc.) needs.

MOBILITY

- View and integrate bicycle and pedestrian connections as mobility options, not only recreational
- Include bicycle facilities (storage, racks, dedicated lanes & bikeways)
- Connect to St. Vrain River
- Explore implementing a transit hub
- Make the area less car-centric and explore car-free zones

ADAPTIVE REUSE

- Make the Sugar Factory a gateway to Longmont
- Reuse / repurpose historic structures (silos, warehouse) where possible, reuse materials (e.g. bricks)
- Highlight Longmont’s history: for example use Sugar Factory branding & naming, include museums and arts that reflect the history
- Preserve industrial and agricultural heritage and uses
- Connection to the river & natural environment

GATHERING AND ENTERTAINMENT SPACES

- Include gathering spaces with food & beverages
- Incorporate outdoor programming, entertainment, performing flex space
- Integrate active & exercising space
- Consider water play and natural outdoor space
- Make some open spaces pet-friendly
- Consider placemaking through the arts

2.2 OPPORTUNITIES AND CONSTRAINTS

MARKET SUMMARY

The sub-area plan uses market data from two sources: the December 2020 *Longmont Area Market Assessment*, and a new retail observations and residential market snapshot (neither of which were included in the office and industrial-focused 2020 report). Acknowledging the significant market shifts precipitated by the COVID-19 pandemic, the sub-area plan market evaluation also rechecked the market assumptions and demand potential from the 2020 report.

» The full *Longmont Area Market Summary and Residential Market Snapshot* are included in the appendix of this document.

Office

The Longmont area has a scarcity of modern, high quality, flexible and sustainable office space that is attractive to today's modern office users, including larger floorplate space. In addition, Longmont is strategically positioned to capture potential spill over demand from Boulder for companies seeking attractive, lower cost options.

Target opportunities for the STEAM and Sugar Factory areas include:

- Agri-hub employment uses such as agriculture technologies and research and development
- Food and beverage business incubation and entrepreneurship
- Professional and business services, such as finance, engineers, architects, designers, and consulting
- Live-work and co-working spaces to support entrepreneurship and startup small business
- Technology and information businesses looking for flex/office space
- Medical office/healthcare sector

Industrial

The regional and national industrial market has been one of the strongest performing real estate sectors.

Target opportunities for the STEAM and Sugar Factory areas include:

- Smaller-scale flex/warehouse/manufacturing opportunities <25,000 SF
- Ag/entrepreneurial hub for small scale food producers including R&D, processing, production, and ghost kitchens that prepare online food orders for direct delivery to customers
- Micro-fulfillment centers and distribution hubs
- Adaptive reuse of older warehouses
- Flex/R&D space for technology and life science companies
- Smaller-scale manufacturing opportunities such as natural sciences, renewable energy, natural foods, breweries/distilleries



Delivery or 'ghost' kitchens are a relatively new use that could find a home in either STEAM or Sugar Factory area.

Retail

The pandemic accelerated the retail sector's long-term downturn, and continued weakness in the retail market is expected to continue. The 'great retail reset' continues as convenience and movement toward more mixed-use centers and neighborhood-centric shopping gain steam. This trend may drive more diverse retail tenant mixes, and adding mixed use elements such as residential, office and micro-fulfillment may be tools toward creating more compelling and attractive retail centers.

Target opportunities for the STEAM and Sugar Factory areas include:

- Food and beverage such as restaurants, cafés and food halls
- Shared kitchens for start-up food businesses and culinary 'maker spaces'
- Service and specialty tenants such as healthcare and wellness, education, fitness centers, garden and housewares
- Niche retail opportunities that offer authenticity with a focus on experience
- Experiential retail, active entertainment, smaller independent shops

Residential

Longmont's low inventory of townhouses and condominiums, low market share of total housing sales and faster rising sales prices relative to single family detached houses suggest there is pent up demand for medium- to high-density housing product type. In addition, demographic shifts such as aging adults, and households without children support continued demand for this type of housing.

Target opportunities for the STEAM and Sugar Factory areas include:

- Multifamily housing including for-rent apartments and for-sale condos
- Single family attached housing such as townhouses, duplexes and triplexes
- Live-Work units containing separate residential and non-residential areas
- Stacked flats
- Compact detached cluster housing such as bungalow courts or cottage courts



LAND USE

OPPORTUNITIES

- Interest is strong from the developer community.
- Vertically and horizontally mixed uses can promote 'one stop' neighborhoods that reduce travel time and distance.

CONSTRAINTS

- Zoning and land use policies could constrain future development.
- Higher commodity and labor costs may deter development as developer returns may not cover cost to build.
- Parking requirements could promote surface parking and reduce developable area.
- Current state law pertaining to housing warranty defects inhibits the developers' interest in constructing some forms of townhouses and condominium units.
- Environmental challenges exist in some areas with the existence of hazardous materials and byproducts



CHARACTER + URBAN DESIGN

OPPORTUNITIES

- The variety and character of existing development in and adjacent to the sub-area can integrate with multiple scales and densities of new development.

CONSTRAINTS

- Extended build-out means that many existing, more industrial uses may remain even while new development takes place over a relatively long period of time.



MOBILITY

OPPORTUNITIES

- First / Last Mile (FLM) improvements can connect the study area with future Bus Rapid Transit (BRT) and commuter rail at the study area's edge.
- The community has expressed strong interest in 'active' transportation such as walking and biking.
- 'Trails as Mobility' - existing and new trails through and adjacent to new development can create a truly interconnected multimodal transportation system.

CONSTRAINTS

- Minimal connectivity potential between STEAM and Sugar Factory, due to railroad tracks.
- 3rd Avenue configuration is unappealing, unsafe and challenging for pedestrians and cyclists.

2.3 PLAN PRIORITIES

SETTING DIRECTION

The sub-area plan is guided by five high-level priorities. These themes are not unique to the areas covered in this plan, but rather represent elements that the citizens of Longmont have expressed a commitment to supporting on a city-wide basis and that Sugar Factory and STEAM areas are particularly suited to support. These five priorities were formulated from public input and from assessment of opportunities and constraints, reviewed and refined by City Council on March 29, 2022, and carried through all subsequent levels of the plan's public engagement process.

HOUSING

Encourage housing options with a range of types and price points



TRANSPORTATION

Provide for both regional and local transportation connections by a variety of travel choices.



DEVELOPMENT

Encourage connectivity between parcels and to the broader Longmont community.



COMMUNITY

Incentivize and encourage cultural arts facilities and community hubs for creativity and innovation.



SUSTAINABILITY

Focus long term efforts on sustainable building and development practices.



2.4 OVERALL IMPLEMENTATION

ZONING MODIFICATIONS

The STEAM area is situated in the Mixed Use-Downtown (MU-D) Zone District that allows for a wide variety of commercial and residential uses, including single family housing subject to specific development requirements. Parts of the Sugar Factory Area located within the City limits are zoned Mixed Use-Employment (MU-E) that provides for a range of uses from light industrial to commercial to housing. Housing, however, is identified as only being permitted as a “secondary use” to employment-producing development. Further, single family detached housing is not permitted in the MU-E District. It is expected that the balance of the Sugar Factory area currently not within the city limits will be zoned MU-E upon annexation.

Significant development challenges to creating an urban environment exist, based on some of the City’s development-related codes. Elements that may require modification are discussed in the text that follows.

Height. The Zoning Districts applicable to development within these areas presently limits the heights of buildings to 4-stories within both Districts, with height incentives that may cumulatively allow up to seven stories. The requirements for additional height include proximity to a transit facility, inclusion of affordable housing and vertical mixed-uses. Neither zone district sets a minimum height requirement, thus allowing for single story buildings to be constructed. The current minimum and maximum height restrictions within the contemplated zone districts should be evaluated and adjusted to enhance development opportunity.

The Districts are also silent to any requirements for ground floor commercial space in multi-level buildings. This element too should be addressed as it could have an impact on implementing the vision for an activated higher density urbanized area in both locations.

Height-related recommendations for further study include:

- Increase or eliminate the maximum height allowance/restriction in both the MU-E and MU-D Zone Districts. Given the STEAM area is currently zoned MU-D District and includes the historic downtown area north of 2nd Avenue, care should be taken to also preserve the historic parts of the downtown character. As for the Sugar Factory area, the adjustments could be benchmarked on the existing larger factory mill and silo structures.
- Set minimum height requirements for commercial and residential buildings dependent on location and development opportunity.
- Identify areas where ground-floor commercial is to be required in a multi-story building (i.e. Main Street), establish minimum “frontage” requirements, and codify in regulatory framework.

Building Setbacks and Utilities. A primary function of urban-type development is to bring buildings closer to the roadways. The continuing imposition of easements outside the public street right-of-way have the tendency of pushing the buildings further away from the street. This is particularly challenging relative to utility easements, where providers are increasingly seeking separate easements within private property which to site infrastructure.

Setback and utility-related recommendations for further study include:

- Establish policy requiring all public utilities to locate local facilities within the public street right-of-way or upon City property with authorization.

Street Section Design and Emergency Access. In both the STEAM and Sugar Factory areas the intent is to size the streets appropriately to promote and support the pedestrian experience in an urbanized area. However, the City's current street standards continue to be oriented towards suburban environments, generally having wider travel lane widths and incorporating longer, sweeping curbs at intersections to facilitate faster vehicular turning movements. The width of streets is of particular concern given a current fire safety requirements for providing clearance areas for ladder trucks adjacent to any buildings 3-stories or more in height. This is contrary to a preferred urban street section having travel lanes 9-10 feet in width along with on-street parking.

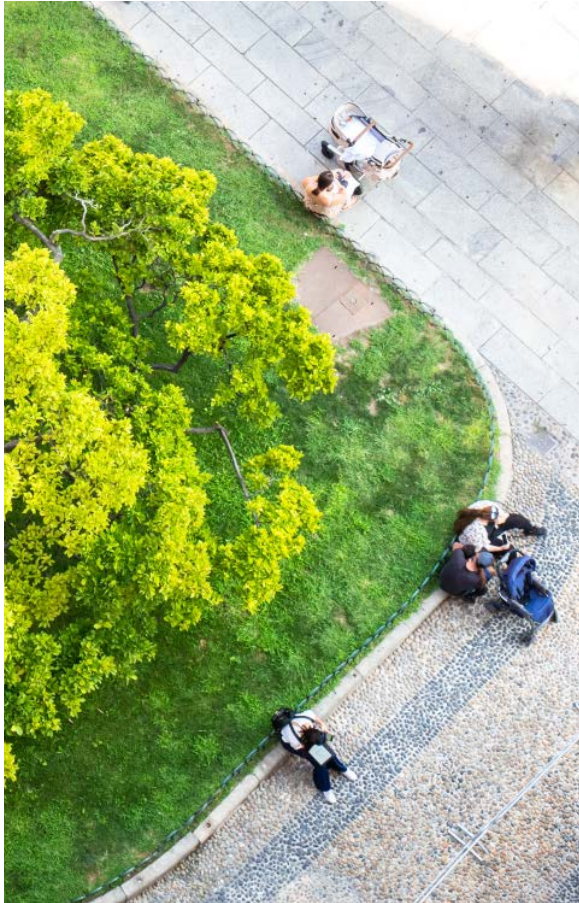
Street section related recommendations for further study include:

- Formulate strategy or conditions to right-size fire apparatus access requirements.
- Create, adopt and codify appropriate street sections for urban-style development.

Parking. The current City requirements for parking are rather significant and will require unused spaces. While there is some leniency on meeting the requirements, by virtue of incorporating affordable housing or preparing a parking study, the hurdle to meet the requirements can deter development interest. The Code also does not include conditions or restrictions on the type (surface or structure) or placement or design of parking areas, particularly surface lots on neighborhood streets. Proposed amendments setting maximum parking requirements in the MU-D and MU-C districts have been recently approved by City Council.

Parking-related recommendations for further study include:

- Formulate alternative parking requirements.
- Establish parameters for the placement/design of surface parking so as to minimize visibility from public streets, parks in neighborhoods, and spaces.
- Establish parameter/allowances for shared parking arrangements.
- Evaluate elimination of minimum parking requirements.



New neighborhood plaza and parks will need to establish responsibility for long-term maintenance.

Storm Detention/Water Quality. The City's on-site detention and water quality requirements can pose challenges to smaller-scale developments and are not consistent with creating urban environments. Both the Sugar Factory and STEAM area, having proximity to City-owned open space, provides the opportunity to create "regional" facilities designed in a manner to enhance wetlands and better protect the adjacent St. Vrain Creek/River with the development community contributing financially.

Storm and detention-related recommendations for further study include:

- Prepare studies to incorporate "regional" storm detention and water quality basins on adjacent or nearby City property.
- Encourage urban hard-space plazas and courts that support multiple functions and programs for satisfying open space requirements.

Parks and Open Space. The sub-area plan calls for the development of smaller parks and public spaces throughout both project areas. The small size of these spaces provides a challenge for city departments that rely on economies of scale to maintain parks. The City has not established criteria relative to efficient maintenance of these smaller public spaces.

Parks and open space-related recommendations for further study include:

- Encourage urban plazas and courts that support multiple functions and programs to satisfy individual project open space requirements
- Develop strategy for incorporating smaller public spaces into City parks inventory and explore mechanisms for operations and maintenance.

REGULATORY OPPORTUNITIES

The City's Zoning Ordinance and development-related standards applicable to the City's Land Development Code (LDC) regulates the design and development standards that are uniformly applied throughout the City. Many of the standards and criteria have evolved over the years to support the outward growth of the City. Thus the roots of the LDC are supportive of suburban rather than urban development. But now, as a more matured City where much focus is now on infill and redevelopment, the current standards and criteria do not necessarily align with current development interests or sustainable design.

The unique development opportunities and interests associated with the Sugar Factory and STEAM properties require an evaluation, and as needed, modification to some of the City's design criteria and standards. Both areas are envisioned to be developed in a manner incorporating, among other things, higher densities and height, tighter street sections, more lenient and/or maximum parking requirements, building setbacks, and storm detention/water quality facilities.

It would be highly challenging, however, to rework the City's codes and standards to accommodate the interests of varying development areas throughout the City, primarily due to the associated complexity that would arise and length of time required. Thus, a more pragmatic approach

would be to create a regulatory overlay district establishing detailed development standards and criteria distinctive to both the Sugar Factory and STEAM areas and ideally address future infill and redevelopment sites. While some aspects may be applicable to both areas there are other desires and expectations that are more specific to each area.

- Evaluate and identify regulatory challenges within both the Sugar Factory and STEAM areas independent from one another.
- Establish boundaries of overlays districts and codify development allowances/restrictions accordingly.

PUBLIC PRIVATE PARTNERSHIPS (PPP OR P3)

The significant impediments to preparing these areas for development include the amount and cost of infrastructure and improvements (including environmental remediation) required; the multitude of property ownerships; and the anticipated incremental construction of infrastructure. The incremental installation of infrastructure as development occurs is not in the City's or nor developers' best interest due to its inefficiencies and eventual design challenges. Likewise, the City and a "first developer in" does not necessarily have the desire or ability to incur the extensive cost of building the entire system up front. Public-Private Partnerships provide an effective means of fulfilling these improvements and absorbing the costs without putting an excessive burden on any party or the public at large.

The range of partnerships has the ability to include the city government, other governmental and quasi-governmental agencies, property owners, and developers utilizing a multitude of financial mechanisms from City capital improvement contributions to tax increment funds to property tax proceeds to developer equity. Not only can a public-private partnership provide the mechanism for constructing the infrastructure or preparing property for development, but also offers a means for long-term maintenance and operations of public facilities, thus minimizing or alleviating such responsibilities from the City. Next steps in this area include:

- Gauge the interest of property owners in creating a Business Improvement District (BID), a General Improvement District(s) or Metropolitan District to generate property tax revenue.
- Identify needed improvements and associated costs to be funded by a district.

- Establish a General Improvement District(s) or Metropolitan District as applicable and viable.
- Create a new or expand the existing urban renewal area to include unincorporated areas upon annexation into the City.
- Use public-private partnerships to make redevelopment opportunities attractive for private investment.
- A P3 arrangement can provide the mechanism to solicit and secure financial grants from a multitude of sources including federal government, non-profits, and foundations.

POTENTIAL INFRASTRUCTURE AND FUNDING MECHANISMS

General Improvement District (GID)

A GID is a quasi-governmental entity, permitted by Colorado Revised Statutes, that can provide for financing, operations and maintenance within established district boundaries. The GID, while operating as an independent entity, is created by a municipality as a means of facilitating the construction, operation and maintenance of infrastructure within a defined sub-area of the municipality. Funding for such improvements and operations is through the imposition of an additional property tax mill levy on properties within the district. The City Council that serves as the Board of the GID, has effective control for funding of improvements and operational costs.

Business Improvement Districts (BID)

Another mechanism available to assist in funding infrastructure improvements, providing maintenance and promoting a development is the BID. Created by the developer, an additional property tax mill levy can be imposed on commercial property within the District. The additional tax cannot be applied against residential properties. The regulatory authority is a board of directors either selected by the property owners or, as an option, the city council can be seated in such a capacity.

Metropolitan (Metro) District

The Colorado Revised State Statutes permit land developers, with the approval of local municipalities, to create a Metro District where, through the imposition of an additional property tax mill levy on properties within the District, the District provides a means of financing, operating and maintaining public infrastructure and amenities. The City's current Ordinance pertaining to Special Districts currently permits the creation of a Metropolitan (Metro) District with strict limitations on the incorporation of residential development. The current ordinance only permits the incorporation of residential development into a district as part of a mixed use project. Residential development as part of a mixed use district is limited to not more than 50 percent by gross square footage of the district. This residential restriction applies to both owner-occupied units and investor-owned buildings such as apartments. This square footage restriction effectively eliminates the ability to pursue the creation of a Metro District to support development of a large scale mixed use project.

Urban Renewal District (URD) Tax Increment Funding

The Longmont Urban Renewal Authority (LURA) was created by the Longmont City Council to facilitate investment and redevelopment activity in areas determined to have “blighted conditions”. LURA has the ability to generate and reinvest incremental tax revenues, known as TIF, towards assisting in funding public improvements and redevelopment projects. The TIF represents the increase in tax revenue generated from new development activity over the tax revenue generated prior to the development (referred to as the base). It does not involve an increase in the taxes paid by property owners, but rather is premised on existing taxing mill levies imposed by the various taxing bodies such as schools, counties, special districts, and municipalities. Given the TIF is generated from these entities’ mill levies, LURA negotiates the amount of TIF that can be applied towards reinvestment into a development or project. The STEAM area and a limited section of the Sugar Factory area are currently situated within an established urban renewal district. The balance of the Sugar Factory area, currently outside the City limits, is a strong candidate for such designation by the City Council upon annexation.

“Brownfields” Grants and Loans

A “Brownfield” is defined as a property(s) either having, or perceived to have, some level of environmental contamination. This generally applies to an older area(s) of the city that is showing or susceptible to disrepair or decay, and having seen little to no private investment for many years. The U.S. Environmental Protection Agency (EPA) and the Colorado Department of Public Health and Environment (CDPHE) offer funding to assist financially in the assessment and remediation of environmental contamination including asbestos product. Funding ranges from direct grants to cities or non-profits. Land owners and developers are not eligible to receive remediation grants. However, they are eligible to pursue low-interest loans having extremely flexible and suitable terms. Grant proceeds from EPA are extremely competitive given the interest for such funding across the nation.

Longmont Downtown Development Authority (LDDA)

The LDDA boundaries cover a significant portion of the STEAM area incorporating property as far south as the south side of 1st Avenue and a large, vacant 12 +/- acre parcel situated between 1st Avenue and Boston Avenue east of Main Street. Similar to the LURA, the LDDA has the ability to capture and reinvest tax increment funds towards improvements, development, maintenance and operations within its district boundaries. In addition, the LDDA has the ability to raise revenue against a property tax mill levy that is currently set at 5 mills, providing another source of revenue to stimulate and support downtown development and activities. The LDDA has the ability to adjust its boundaries subject to a level of property owner support and consent of the Longmont City Council. The LDDA mill levy can also be increase, raising additional revenue, subject to approval of eligible voters within the district.

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FRAMEWORK PLAN: SUGAR FACTORY

3.

- 3.1 OVERVIEW
- 3.2 OPPORTUNITIES AND
CONSTRAINTS
- 3.3 STRATEGIES
- 3.4 ROADWAY SECTIONS

3.1 OVERVIEW

DESCRIPTION

For the purposes of this study, the Sugar Factory area is defined as a roughly triangular collection of parcels comprising approximately 180 acres. It is bounded by the existing, active railroad storage track to the southwest, Highway 119/Ken Pratt Boulevard to the southeast, additional active rail and Rogers Road to the north, and a small portion of 3rd Avenue at its eastern extremity. A significant portion of the land along the southern, diagonal boundary currently serves wetland/drainage functions; the eastern portion of this area is slated to be mined/reclaimed in a near- to mid-term timeframe.

EXISTING USES

The site's most notable features are the historic sugar factory building itself, along with its smokestack, silos and associated outbuildings. The Factory building is red brick on a structural steel frame; the structure is vacant and in disrepair, having also sustained significant damage in a somewhat recent vandalism-set fire. The silos are still in use by the agricultural company immediately adjacent on the north side of Sugar Mill Road.



Scenes of the Sugar Factory area (clockwise): the factory cluster with the Rocky Mountains beyond; the main building, smokestack and silos; the historic Tin Shed; a winter cyclist on Sugar Mill Road; inside the factory.

Also notable is the so-called Tin Shed, an approximately 460-long metal structure immediately south of the factory building. A large fenced yard area around the shed is used for RV and large vehicle storage.

A seed company and landscape materials center occupy that portion of property between Rogers and Sugar Mill Roads; both companies are active.

CURRENT DEVELOPMENT PLANNING

Several developers are actively planning the bulk of the area south of the east-west railroad/Sugar Mill Road, with plans in various stages of development. The portion of land west of 119th Street will require brownfield mitigation, soils stabilization and annexation into the City of Longmont; the portion of land east of 119th is already within City boundaries. The land between Sugar Mill and Rogers Roads is a potential, likely later area of development that is included in this plan to ensure appropriate transition between the primary largely vacant redevelopment area and the light industrial south of 3rd Avenue.

TOPOGRAPHY AND VIEWS

The site slopes from northeast to southwest, with a notable highpoint in the northeast corner. The height of the historic structures, particularly the factory smokestack, create an iconic, uniquely Longmont view of the historic factory, the City's tree canopy spread behind it, and the Rocky Mountains rising in the background.

HISTORIC REUSE

The City and community strongly hope that some portion of the historic Great Western Sugar Factory structures can be re-used. The developer with an option on these properties



The historic Great Western Sugar Company complex, seen from the east.

has expressed similar priorities and is currently exploring the feasibility of various re-use options, although how many of the structures are candidates for restoration is unclear at this time. Simultaneously, the City is pursuing brownfield mitigation of the larger factory site.

» A full summary of existing physical conditions can be found in the Design Brief, included in the appendix of this document.

3.2 OPPORTUNITIES AND CONSTRAINTS



LAND USE

OPPORTUNITIES

- Inclusion of urban agricultural uses can increase use of hard-to-use and 'leftover' parcels, while lending unique character to the area.
- A totally new roadway system offers flexibility in block size and potential for different housing types.
- Developers have expressed interest in aggregated open space that builds on existing riparian and open space assets.
- There is opportunity for restoration/ adaptive reuse of historic buildings.
- The site can accommodate a diverse range of development types.

CONSTRAINTS

- Proximity of existing, active rail tracks can limit the distribution of land uses, particularly residential, due to noise and vibration.



CHARACTER AND URBAN DESIGN

OPPORTUNITIES

- The area offers opportunity to express local history in the character of new development.
- As a 'blank slate', the area can promote consistent, coordinated character.
- Location, topography and existing historic structures offer opportunity to create a striking visual gateway to the City.
- Redevelopment could create a thriving community activity center

CONSTRAINTS

- Re-use of historic building can be difficult due to internal configuration; financial feasibility could result in a re-use strategy different than that discussed in community visioning.
- Application of conventional/ standardized infrastructure requirements will limit development.



MOBILITY

OPPORTUNITIES

- New roadway sections can promote safer walking and biking and contribute to placemaking.
- Reconfiguration of existing bus routes or incorporation of microtransit could enhance external connectivity.
- A future roadway connection to 1st or Boston Avenues could be made across the railroad tracks at or below grade.

CONSTRAINTS

- Minimal access points to connect with adjacent development.
- Limited access opportunities to St. Vrain Creek corridor.
- Existing at-grade railroad crossings limit flexibility of new roadway system.

3.3 STRATEGIES



LAND USE

Historic Core

- Preserve and restore the historic Sugar Factory buildings in a way that integrates into the new community.
- Emphasize the historic core as the center of the development by reducing height and density of development as it moves outward from the core.
- Include flex multi-use spaces that can evolve or adapt to fit a variety of uses.

The restored sugar factory buildings will act as the visual and physical ‘town center’ of the development. Although which buildings are retained and restored, and the uses that they will contain, will be determined by the market, it is anticipated that the Sugar Factory will be a true mixed-use center with both public and private uses, such as retail, food and beverage, office, potentially maker-type uses. It will also include publicly accessible open space that will feature the historic buildings as a backdrop for formal and informal events.

High Intensity Residential/Mixed-Use

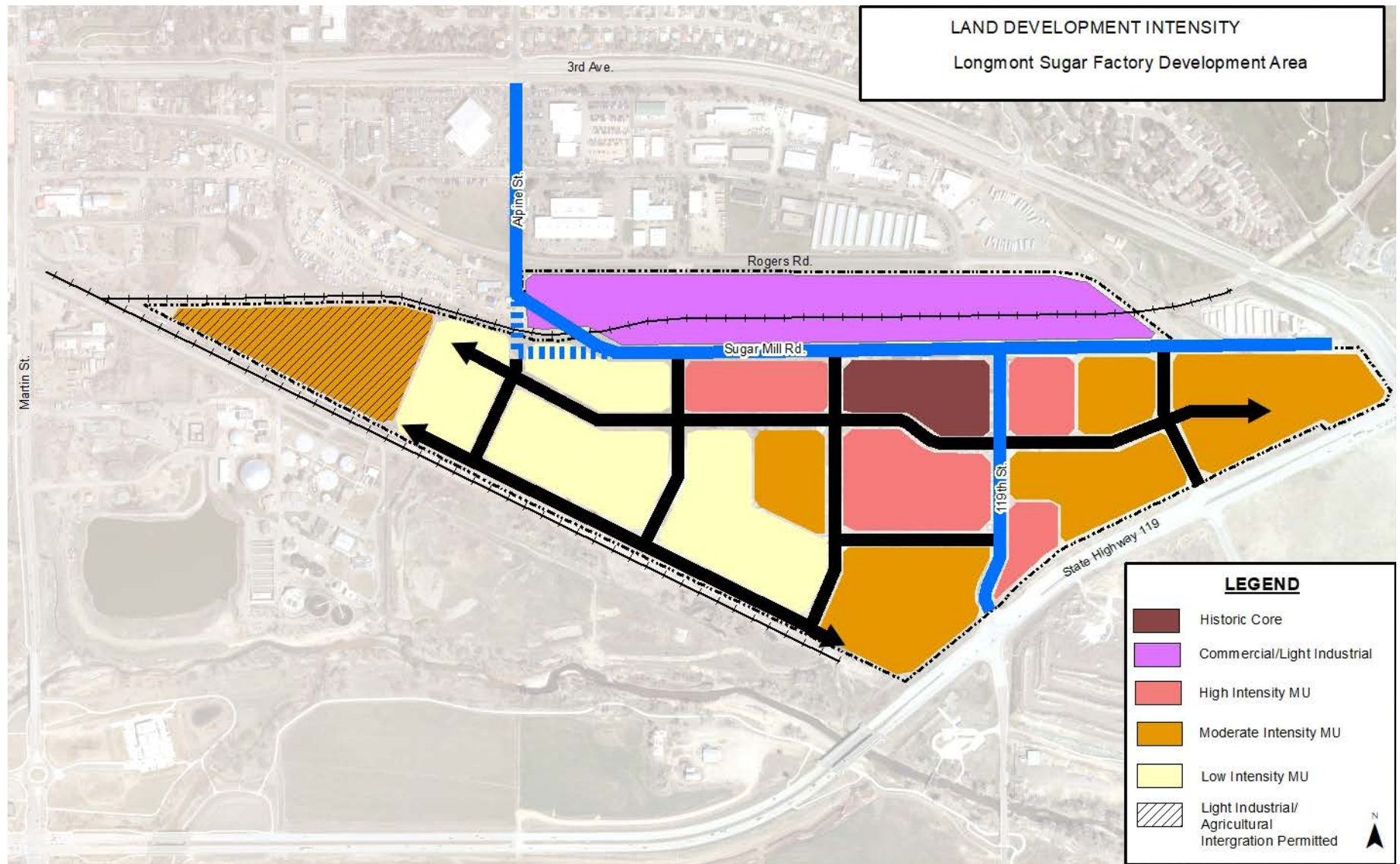
- Recognize and develop as a Mixed Use Urban Center.
- Include employment opportunities that reduce out-commuting.
- Provide a spectrum of housing options, including ‘missing middle housing’ and higher density multi-family.
- Arrange uses to promote walking and biking to dining, retail, employment

Moving outward from the historic core, development will offer horizontally and vertically mixed uses. Higher density housing such as apartments and condos may be concentrated as a next ‘ring’ of development outside the sugar factory blocks, or can be spread throughout the area in mini-nodes. Employment uses and ‘missing middle’ residential products should also be included. It is anticipated that even these ‘missing middle’ buildings may still host a mix of uses, including home occupations, studios, or even ground floor small-scale commercial.

Supportive Residential

- Provide integrated housing/amenity options that appeal to a spectrum of life stages.
- Include small-scale, aggregated private open spaces.

The outer edges of the community will offer a variety of housing options, with a focus on missing middle products such as duplexes, fourplexes, and cottage courts. Since the roadway system will be built along with the development, block sizes can be designed to accommodate a variety configurations. These residential options are encouraged to include small-scale, private but still aggregated open space, such as shared courtyards. Single family detached housing product is not suitable in the Sugar Factory planning area.



Key land use elements within the Sugar Factory redevelopment area

FRAMEWORK PLAN: SUGAR FACTORY



Historic re-use is at the heart of the community's character, and may take many forms including historic structures as backdrop, restoration and internal renovation of the structure, and re-use and expansion.



Higher intensity mixed-use residential should be scaled to integrate with the historic buildings, and may include a variety of uses at the ground floor.



'Missing Middle' housing can include a diversity of multi-family housing types, such as cottage courts (left), townhomes (middle) and stacked flights (right), among others.

Open Space

- Provide variety and scale and type of public space.
- Aggregate main open space.

A feature of the community should be a public space that can serve as a “festival plaza” in proximity to the historic Sugar Factory buildings. Enhanced streetscape will connect this space physically and visually to the community’s main open space in the area’s southern portion. Aggregating public open space into a single location allows the community to optimize use of the core development area, and to capitalize on the proposed park area, which is slated to be mined and restored. This more park-like green space will provide contrast to the more urban character of the festival plaza, providing residents and workers a larger variety of formal and informal programming opportunities. It is anticipated that both spaces will be constructed in earlier phases of development.

Urban Agriculture

- Use urban agriculture to define character, buffer uses from rail, and optimize utility of hard-to-use parcels.

The sugar factory area should reflect the city and the site’s agricultural history, as a larger theme but not a literal recreation of past activities. Urban agriculture – such as greenhouses or educational facilities – provides an opportunity to capitalize on hard-to-use parcels at the site’s ‘corners’ and to provide a spatial and potentially noise buffer from the active rail on the site’s northern and southern sides. Uses should be selected for both form and function, since some of the identified locations for this use are also high visibility and will act as a gateway for the community. Uses may be overseen by a community organization, or through partnership with local agricultural interests.



Image: JessBlackwell photography



Image: JessBlackwell photography



Image: architectureworks.com

Urban agricultural uses could be chosen for both form and function, depending upon their location. Integrated greenhouses, community gardens and education pavillions might all be viable options.



CHARACTER AND URBAN DESIGN

Suburban vs. Urban Design

- Site layout will promote walkability and human scale through appropriate block size and an enhanced public realm.
- Buildings of all types will use minimal setbacks and provide horizontal and vertical variation in the façade.
- Vehicular access drives will be limited and parking will be located behind buildings or in parking structures.
- Building systems and site layout will be designed to enhance fire and emergency access and minimize roadway widths.

The sugar factory community will be built on a walkable, human scale with a focus on urban principles of design. Many of these principles may be defined in more detail in later developer-led submittals, the following items provide a snapshot of community character.

Walkability. We feel most comfortable in places that are designed for us as humans – designed to make us feel safe, create reasonable distances to walk or bike from place to place, provide visual interest and variety at street level, and where streetscapes are treated as a critical part of placemaking. The community will strive to use smaller block sizes and to create roadways of reasonable width for use by all modes. Uses will be mixed, so that daily needs including walking, shopping and employment can be met within walking or biking distance of home.

Human Scale and Enclosure. Buildings will be located at or near the pedestrian sidewalk - with no or minimal setbacks - to create a consistent street wall and provide enclosure and a sense of scale. Façades will vary vertically and horizontally to avoid monotony, with the height of the building giving reasonable consideration for access to light and air.

Vehicular access and parking. To promote safe walking and biking, curb cuts and driveways will be limited to secondary streets, and should be shared between properties when feasible. Vehicular parking should be interior to the block where possible, and if structured parking fronts the public realm the façade should be wrapped with active ground floor uses such as retail. If not wrapped, it should be enhanced with architectural detail and/or art.

Compact Roadways. Curb to curb widths should be as compact as possible while still providing full mobility for pedestrians and bicycles. Emergency access is critical, and should be provided across all community systems so that roadways will not be needlessly wide.

Images (right): the Sugar Factory area will provide an urban aesthetic that may draw from modern, industrial and even European aesthetics.



Festival Plaza

- Provide a flexible, urban space that can host a variety of events.
- Provide an enhanced linear park or greenway connection to the area's southern, aggregated open space.

The festival plaza will be designed as a multi-use urban space that can host a variety of events, from markets and fairs to movie nights and music. Design should maximize flexibility with elements like full electrification and numerous hookups, movable seating and thoughtfully placed planting and shade. Development should provide direct visual and physical connection, as well as enhanced wayfinding, between this northern space around the sugar factory to the area's southern aggregated green space.

Views and Gateway Parcels

- Promote culture and identity of Longmont and existing context through inclusion of art and focus on history.
- Capitalize on and enhance westward views at the site's highpoint near Ken Pratt/3rd Avenue.

The site is a bit of an island, largely separated from adjacent uses by railroads and arterial roadways; for this reason, the site's limited entry points are especially important in establishing the character of the community. Gateway sites at the north and south should include large-scale visual elements that reference the site's history and current identity. In addition to these functional entry points, community design should also emphasize westward views of the sugar factory and the Rocky Mountains, particularly from the site's highpoint in the northwestern corner.

Sustainable Features and Technology Pilots

- Create incentives for green infrastructure and sustainable development
- Integrate renewable energy throughout the study area whenever possible.
- Brand the community as a innovator in new energy technologies and promote small-scale pilots.

Building the community from the ground up, including all systems, provides an opportunity for innovation. Developers should create incentives for green infrastructure and sustainable development practices at both the horizontal (site planning, adjacencies, multimodal systems) and vertical (building orientation and materials, renewable energy) scales. Metrics such as those found in the USGBC's Leadership in Energy and Environmental Design (LEED) and ISI's Envision system are useful resources, but emphasis should be on

performance rather than formal certification. New residents should be aware of and even choose the community because of its emphasis on sustainability and innovation. The community may include an evolving selection of pilot projects, some of which may become permanent and some which may be replaced.

Green Infrastructure

- **Design streetscape as an integrated part of stormwater management.**

Roadways as an impervious surface represent a significant contribution to site stormwater, and new approaches and technologies can upgrade them from a source to a solution (or, part of a solution). Roadways and the adjacent streetscape will be designed with a variety of green infrastructure strategies, potentially including but not limited to stormwater planters, bioswales, cisterns, infiltration rain gardens, disconnected roof drainage and permeable paving (street or sidewalks).



MOBILITY

Roadway Hierarchy

- Prioritize pedestrians and bicycles over motorized vehicles on internal streets.
- The principal project entry street should be creatively designed as a central pedestrian retail/restaurant spine.
- Support higher levels of vehicular travel on 119th Street and Sugar Mill Road.

An east-west Pedestrian Spine will act as the site's primary pedestrian area, lined with shops, cafés and other active ground floor uses. This detail should extend across 119th Street and into the first block of the eastern development area, to ensure an integrated development character. Driveways should be prohibited or minimized.

Sugar Mill Road and 119th Street will be the main circulation corridors in and out of the site, and will ultimately gather all vehicular traffic coming into or out of the site. They will be designed with these higher traffic levels in mind, while still providing safe and unbroken pedestrian and bicycle circulation along their entire lengths.

Parking

- Capture commercial and retail vehicle parking at the edge of the historic core in an area parking structure.
- Shared parking will reduce the overall amount of land dedicated to vehicle storage.

The entire sugar factory community will emphasize pedestrian safety, particularly the historic core. To minimize unnecessary circulation within this core pedestrian area, including the well-known circling for parking, an district parking structure off of 119th Street will capture commercial, restaurant and other non-residential visitors/workers as they enter the community. Sharing parking between uses with opposite or minimally overlapping time demands – such as daytime shops and evening entertainment venues – will reduce the overall number of parking spaces needed.

Trails as Mobility

- Create a connected system of on- and off-street multiuse facilities.
- Create a multiuse trail connection into site from Pace Street/3rd Avenue.
- Enhance 3rd Avenue to be truly multimodal and sized appropriately for future transportation needs.
- Install ped/bike bridge across St. Vrain Creek on northwest side of Ken Pratt Boulevard
- Prioritize connectivity to 1st & Main transit center and downtown.

The sugar factory community will emphasize balanced mobility and will elevate trails from a strictly recreational use to an integrated part of the overall circulation system. The system will mix on-street facilities such as bike lanes with separated and off-street facilities, such as cycletracks and multiuse

trails, to ensure a highly granular system that provides internal and external connectivity. Critical connections include the adjacent St. Vrain open space corridor, the STEAM sub-area, and the First & Main transit center. Key elements of this approach include a new trail connection into the site from the north at Pace Street and enhanced bike and pedestrian considerations on 3rd Avenue. 3rd Avenue enhancements could include leading bike-specific signals with intervals to increase bike visibility at signalized intersections; elimination of channelized right turns and prohibition of right-turn-on-red to mitigate right-hook conflicts; and median pedestrian refuges and corner bump outs to reduce crossing distance. The City has already defined the southern side of the St. Vrain Creek as primary circulation, with the northern side reserved for wildlife; to connect with existing south-side trails, community development should introduce a pedestrian and bicycle bridge across the creek, immediately west of Ken Pratt Boulevard.

Transit

- **Modify existing bus routes, introduce new routes into the site or incorporate microtransit to provide transit coverage.**
- **Plan for micromobility, allowing space for dockless bikes and bike-n-ride shelters.**

Bus connections should work in tandem with trail and micromobility improvements to provide multiple options to connect to the First & Main transit center to the greater Longmont community at-large. The development process should work with RTD and other transit operators to provide transit service internal to the community, either via route modifications/extensions, new routes. Development should also work with RTD and other transit operators to optimize transit stop locations, relative to adjacent uses and desired roadway character. In addition, streetscape should be designed to accommodate parking of dockless bike share, with additional consideration given to the potential location(s) of park-n-ride shelters that allow secure, 24-hour parking of personal bikes.



Image: Google Earth

Existing railroad crossings should be enhanced to provide pedestrian and bicycle facilities.

Railroad Crossings

- Enhance existing grade crossings on Sugar Mill and Rogers Roads.
- Pursue a new multimodal or at least ped/bike railroad crossing at or below grade at the western ‘point’ of the site.

Acknowledging the extensive coordination needed to relocate existing at-grade crossings, which may also require giving up other crossings in a 2-for-1 ratio, the existing grade crossings on Sugar Mill and Rogers Roads are considered workable for the new development. They will, however, require upgrades to allow safe bicycle and

pedestrian crossing; enhanced signals are also likely to be needed. For cyclists and pedestrians wishing to access the community from the west, the recommended route is to travel north on Martin and use the existing at-grade rail crossing just north of 1st Avenue, then proceed east along a new trail paralleling the tracks. Where the northernmost rail splits from the diagonal storage track and continues east, a new underpass will allow pedestrians and cyclists to enter the triangular area of the sugar factory area and continue on the community’s internal trail and roadway network.

Inter-Area Connectivity

- Investigate/obtain long-term options to better connect the STEAM and Sugar Factory areas.

Opportunities may arise in the future to enhance connectivity between STEAM and the Sugar Factory Area, and these opportunities should be monitored and optioned as appropriate. Notable opportunities could include relocation of the existing water treatment plan and/or modification or relocation of the rail storage track. Some combination of these options could make a direct roadway connection eastward into the Sugar Factory Area possible, in the area of Boston or 1st Avenues. Additionally, parcel redevelopment near 2nd Avenue and Martin Street and potential extension of 2nd Avenue to Rogers Road OR extension of Weaver Park Road in an ‘s’ to Martin Street could both be long-term connectivity enhancements.

3.4 ROADWAY SECTIONS

Modal Balance

All streets within the Sugar Factory Community will be Complete Streets that offer safe and comfortable mobility for people walking, biking, taking transit and driving. All streets prioritize pedestrians and cyclists, while still acknowledging that the area's main entry points – Sugar Mill Road and 119th Street – will need to move significant volumes of motorized traffic.

Flexibility

The sections that follow prioritize safety and scale; regardless of mode of travel, all residents workers and visitors should feel that they can move throughout the community without conflict, and that they **belong** on the street (as opposed to feeling 'misplaced' and vulnerable, for example, in a car-dominated environment). The sections are flexible, and can and should be adjusted to reflect the type and density of land uses on each block. Certain elements, such as bike lanes, may change position or width depending upon pedestrian conditions, demand for on-street parking, or intersection configuration (curb extensions or no curb extensions, for example).



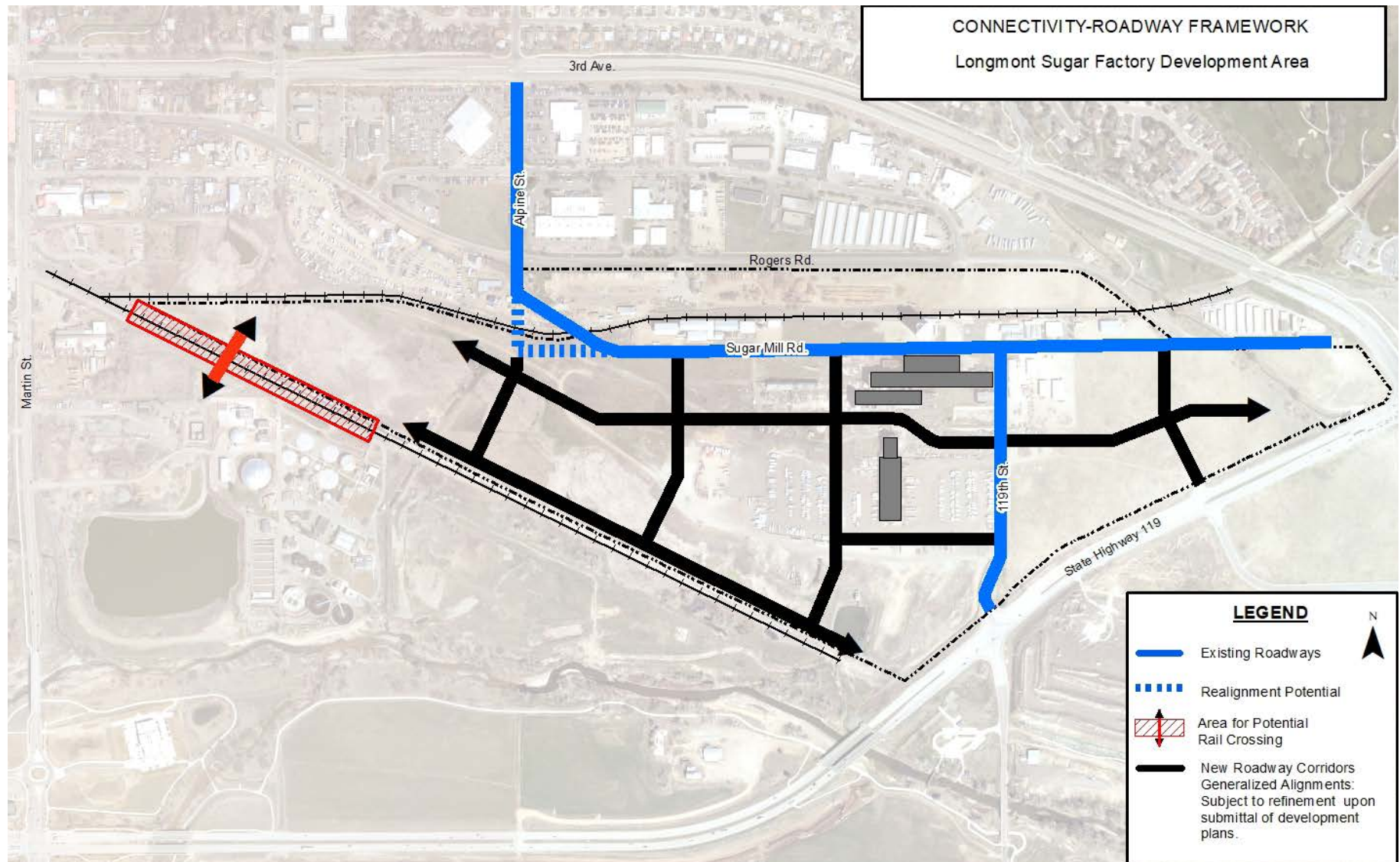
New and enhanced roadways will ensure a safe walking, biking and driving environment for all users in the Sugar Factory area.

»» The following sections are the same in Sugar Factory and STEAM. They are included in both chapters so that each chapter may act as a stand-alone guide to development in each area: Gateway Collector (small dimensional difference due to existing ROW); Neighborhood Street; Local Street; Shared Street; Alley.

New Patterns

These roadway sections differ from Longmont's current standards. Many streets within the development area will be short segments, and may provide opportunity for the City to pilot new technologies or configurations. They may also be a call for the City to reconsider their policy and equipment relative to issues such as fire services; as existing vehicles are replaced, smaller vehicles may be better matched to narrower cross-sections and allow later phases of development to use narrower sections.

As a blank slate, the sugar factory area offers a unique opportunity to depart from business-as-usual, and being to integrate new patterns of sustainability, in both physical form and policy approach.



The Sugar Factory area will require new and enhanced roadways to create a human-scaled, multimodal environment that connects to adjacent development.

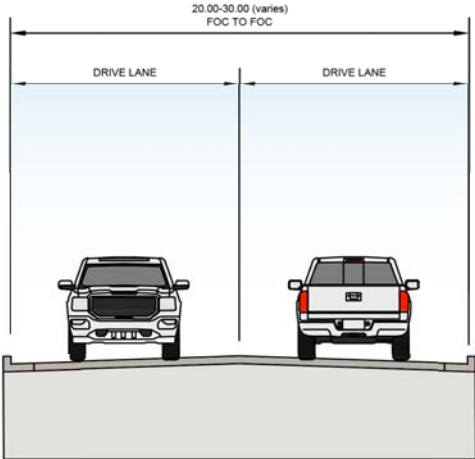
GATEWAY COLLECTOR

- Sugar Mill Road
- North 119th Street

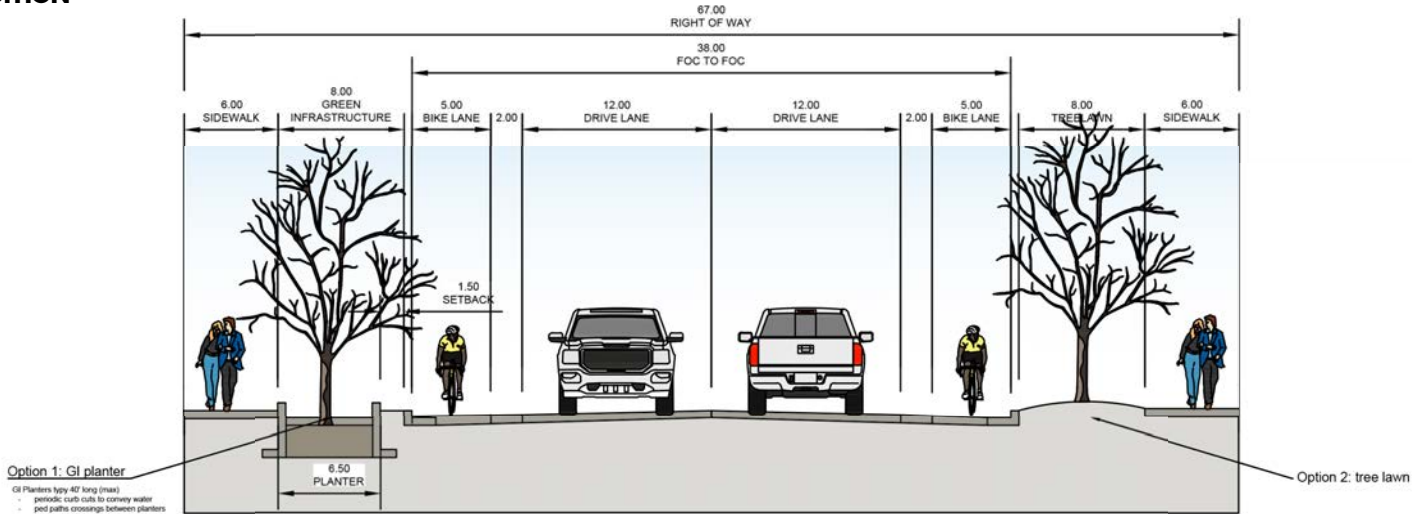
These roadways will be the ‘workhorse’ streets of the development, with an emphasis on moving vehicles in and out of the development from external access points. They will, however, still provide safe pedestrian and bicycle facilities. Roadway character will be slightly less urban than the rest of the development, and street tree plantings may be in green infrastructure planters or tree lawns; lower levels of pedestrian travel also allows for narrower sidewalks. Bicycle facilities should be included; although bike lanes are shown in the sections, depending upon design speed and levels of service/freight traffic, combining the pedestrian sidewalk and bike lane into a behind-the-curb multiuse path may be an option.

Appropriate Elements		Gateway Collector
Public Realm	Ped Lighting	x
	Street Furniture	
	Street Trees	x
	Micromobility Parking	
	Mobility Hub	
	Loading Zones	
	Parklets	
	Transit Stops	x
	Paid Parking Kiosks	
	On-Street Parking	
Safety	Crosswalks	
	Curb Bump-Outs	
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	
	Stormwater Tree Trenches	x

EXISTING CONDITION



PROPOSED CONDITION



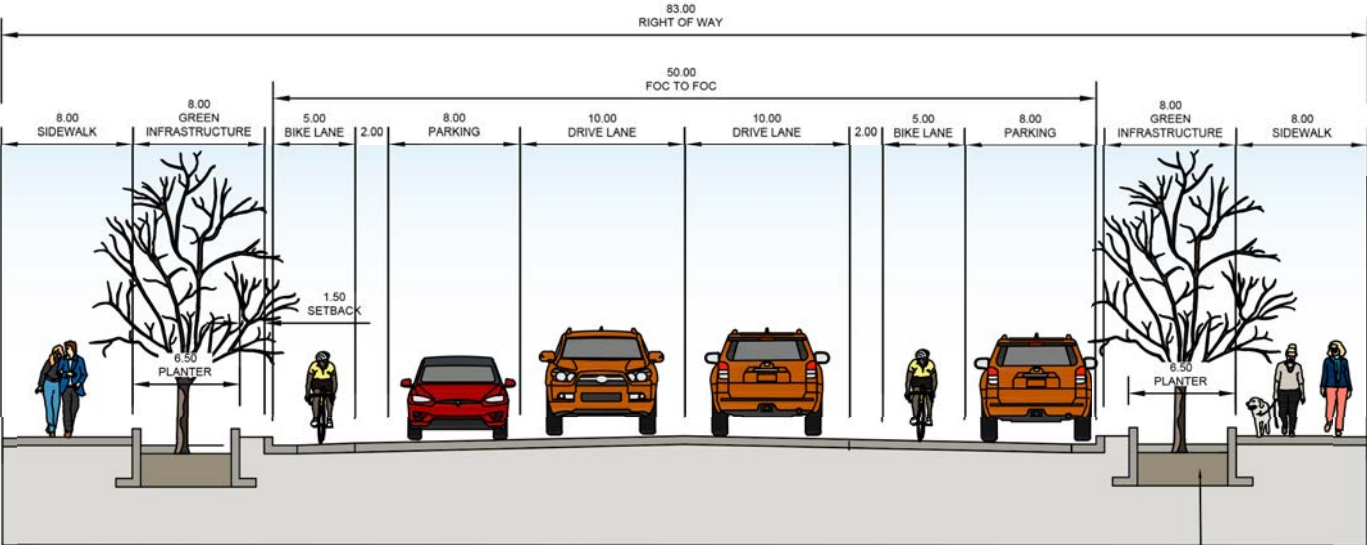
PEDESTRIAN SPINE

- To be constructed

This east-west roadway will be named when the community develops. This street will act as the community's main commercial and retail street. It offers the highest level of urban design in both materials and furnishings, and includes a generous frontage zone for café seating and outdoor retail displays. Because it prioritizes pedestrians above all other modes, curb cuts will be prohibited in core blocks (block to be identified as circulation system is designed). On-street parking should be managed to encourage frequent turnover and discourage all-day office or residential parking.

	Appropriate Elements	Pedestrian Spine
Public Realm	Ped Lighting	x
	Street Furniture	x
	Street Trees	x
	Micromobility Parking	x
	Mobility Hub	x
	Loading Zones	
	Parklets	x
	Transit Stops	
	Paid Parking Kiosks	x
	On-Street Parking	x
Safety	Crosswalks	x
	Curb Bump-Outs	x
	Driveways/ Curb Cuts	
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	
	Stormwater Tree Trenches	x

PROPOSED CONDITION



If parking against curb, can do intersection bump-outs to narrow crossing distance; planter would widen approx. 8' into space of parking lane.

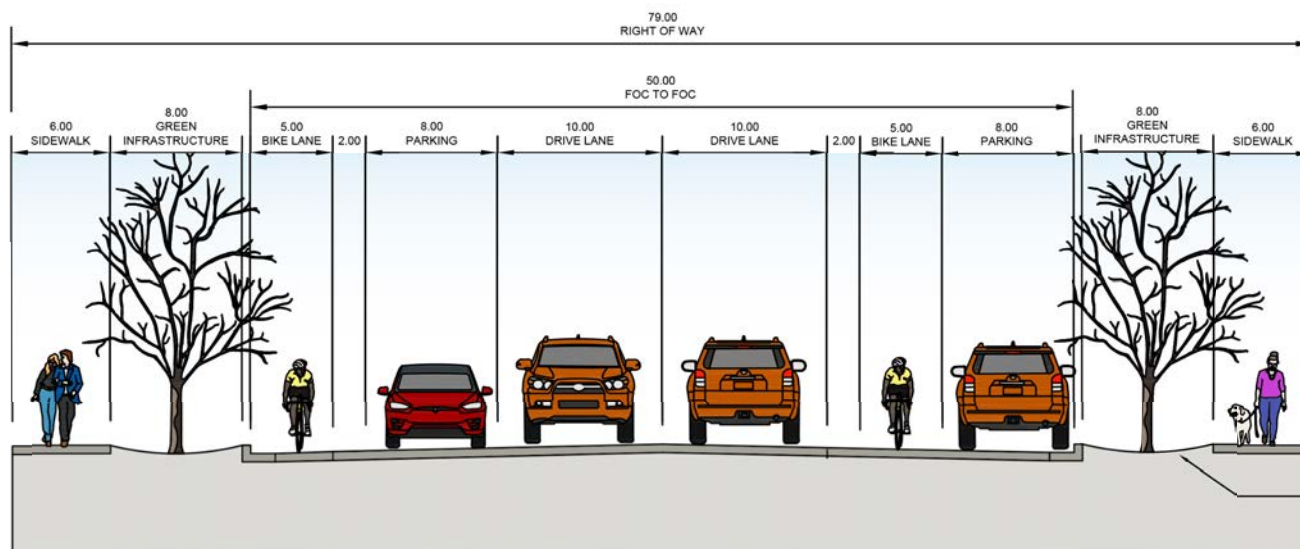
NEIGHBORHOOD STREET

- To be constructed

The street has the same curb-to-curb width and space distribution as the Pedestrian Spine, but with a more compressed sidewalk/amenity zone. This street type will be most common in high intensity residential/mixed-use areas or mini-nodes, where pedestrian and vehicular traffic is expected to be heavier than less dense, predominantly residential zones. Street trees may be planted in a variety of conditions, from stormwater planters to swales, depending upon the character and density of adjacent development. Bicycle facilities may be against the curb, or between on-street parking and travel lanes; condition will again depend on specific land use adjacency and intersection configuration.

	Appropriate Elements	Neighborhood Street
Public Realm	Ped Lighting	x
	Street Furniture	
	Street Trees	x
	Micromobility Parking	x
	Mobility Hub	
	Loading Zones	x
	Parklets	x
	Transit Stops	x
	Paid Parking Kiosks	x
	On-Street Parking	x
Safety	Crosswalks	x
	Curb Bump-Outs	x
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	x
	Stormwater Tree Trenches	

PROPOSED CONDITION



If parking against curb, can do intersection bump-outs to narrow crossing distance; planting would widen approximately 8' into space of parking lane.

SHARED STREET

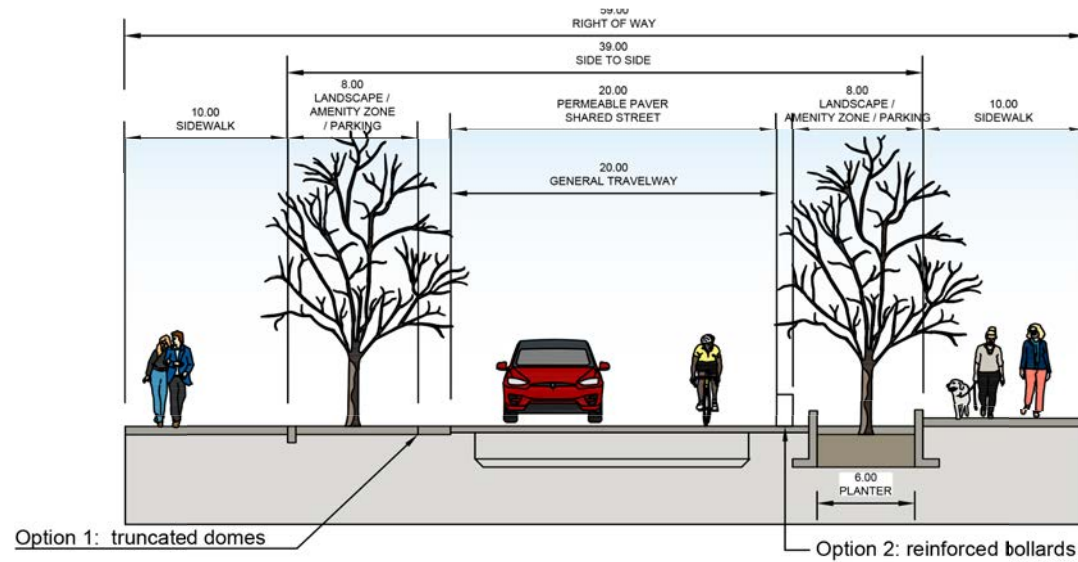
- location TBD

A shared street is intended to ‘expand’ public open space during programmed events; if included in future Sugar Factory area, it is most likely to be adjacent to a park or plaza associated with the historic Sugar Factory buildings. Due to limited circulation options within the area - particularly for east-west connections- it is also anticipated that a shared street would be a north-south street, so as not to disrupt major east-west connections.

This potentially curbless ‘pedestrian mall’ can be closed for festivals and events; It will and will use tactile paving or bollards to mark edge of drivable area. Sidewalk is shown as a minimum 10’ in width, but is anticipated to vary (potentially much wider) based on the design of adjacent structures. This type of street is sometimes called a ‘woonerf’ and is truly designed as a pedestrian mall in which motorized vehicles are sometimes allowed as ‘guests’.

	Appropriate Elements	Shared Street
Public Realm	Ped Lighting	x
	Street Furniture	x
	Street Trees	x
	Micromobility Parking	x
	Mobility Hub	
	Loading Zones	
	Parklets	x
	Transit Stops	
	Paid Parking Kiosks	x
	On-Street Parking	x
Safety	Crosswalks	x
	Curb Bump-Outs	
	Driveways/ Curb Cuts	
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	x
	Stormwater Tree Trenches	x

PROPOSED CONDITION



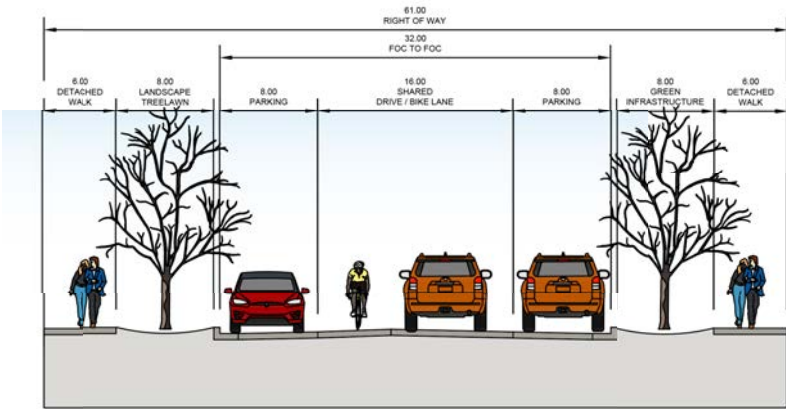
LOCAL STREET

- To be constructed

This roadway type will be found in lower density areas with less commercial/retail/ café activity. It is intended to minimize roadway width by providing a shared drive/ bike lane in place of dedicated bicycle infrastructure. When two cars meet on this type of roadway, one vehicle must typically pull aside and wait for the other to pass. Intensity of adjacent development, and the associated levels of traffic generated, should be considered when deciding between the Local and Neighborhood street typologies. Street trees on the local street will typically be planted in a less urban condition, such as a tree lawn or swale.

	Appropriate Elements	Local Street
Public Realm	Ped Lighting	x
	Street Furniture	
	Street Trees	x
	Micromobility Parking	
	Mobility Hub	
	Loading Zones	
	Parklets	x
	Transit Stops	x
	Paid Parking Kiosks	
	On-Street Parking	x
Safety	Crosswalks	x
	Curb Bump-Outs	x
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	x
	Stormwater Tree Trenches	x

PROPOSED CONDITION



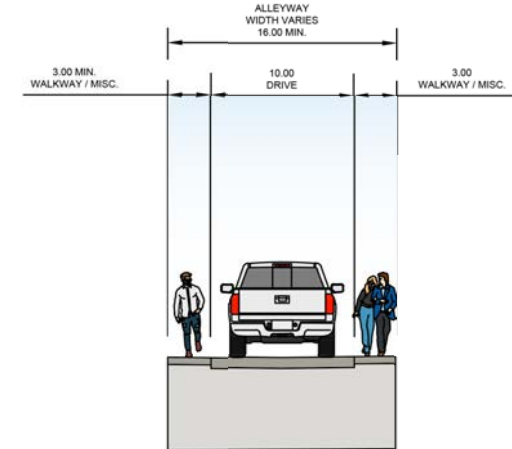
ALLEY

- To be constructed

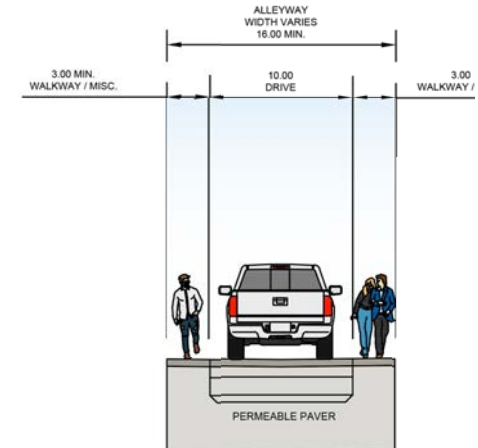
This street can be used in any part of the community; focus is on service use and fire protection. Width may vary; dimensions shown are minimums. Depending upon vehicle weight and frequency of use, alleys may be candidates for permeable paving.

	Appropriate Elements	Alley
Public Realm	Ped Lighting	
	Street Furniture	
	Street Trees	
	Micromobility Parking	
	Mobility Hub	
	Loading Zones	x
	Parklets	
	Transit Stops	
	Paid Parking Kiosks	
	On-Street Parking	
Safety	Crosswalks	
	Curb Bump-Outs	
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	
	Pervious Paving	x
	Stormwater Tree Trenches	

ALLEY



GREEN ALLEY



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4. FRAMEWORK PLAN: STEAM

- 4.1 OVERVIEW
- 4.2 OPPORTUNITIES AND
CONSTRAINTS
- 4.3 STRATEGIES
- 4.4 ROADWAY SECTIONS
- 4.5 IMPLEMENTATION

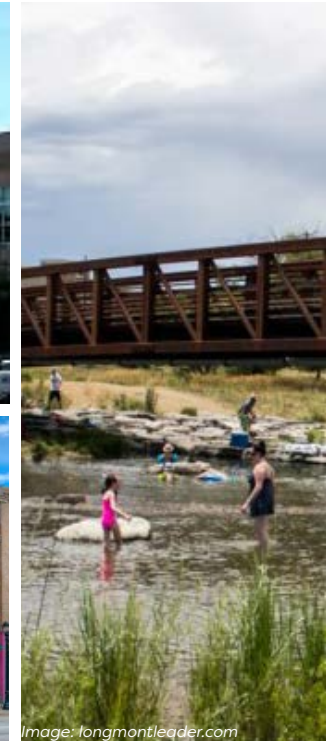
4.1 OVERVIEW

DESCRIPTION

The concept of a ‘STEAM’ district was first explored in 2019 by a Council-led advisory board and focused on the potential of promoting this portion of downtown as an area for Science, Technology, Engineering, Art and Maker uses (see the ‘Related Planning - Prior Efforts’ section in Chapter 1 of this document). The STEAM study area is roughly bounded by 3rd Avenue and Boston Avenue to the north and south respectively, and Martin Street and Main Street to the east and west. Significant portions of the area are under developed and lack interconnecting roadways and development supportive infrastructure. Significant portions of the area are undeveloped areas lacking bisecting roadways, but if the existing roadway grid were extended STEAM would comprise approximately 13 city blocks.

EXISTING USES

The area is home to a wide variety of uses that include civic, retail, restaurants, and breweries, to light industrial, outdoor storage and salvage operations, and a very limited amount of residential. The Longmont Safety & Justice Center occupies a prominent mid-block position in the northern portion of the



Scenes of the STEAM area (clockwise): new South Main Station apartments; Longmont Safety and Justice Center; Dickens Farm Nature Area; Wibby Brewing; historical commercial at Main & 3rd.

area, while a variety of restaurant, retail and commercial uses line the northern block of Main Street; additional space for these types of uses are offered on the ground floor of the new South Main Station building. South Main Station offers apartment living, with additional apartments and townhomes being planned in Phase 2. A variety of multi-tenant light industrial/commercial buildings,

and undeveloped lots are located in the southern portion of the area. The Regional Transportation District (RTD) plans for a significant multimodal transit hub on the west side of the 1st Avenue & Main Street intersection, adjacent to the STEAM district.

The STEAM area’s adjacent surroundings are important for the establishment of its character. The Dickens Farm Nature Area

abuts the district to the south and offers both formal and informal recreational opportunities, as well as access to the St. Vrain River trail corridor. Existing rail is situated adjacent to 1st Avenue and includes a large, triangular rail switching and storage area; these rail uses are expected to remain.

CURRENT DEVELOPMENT PLANNING

The area has seen recent development activity, most notably the more recent completion of the first phase of the South Main Station redevelopment – The South Main Station mixed use commercial/apartment project. Additional phases of the development are underway or awaiting plan approval, including a 20 unit townhouse/apartment project at 210 Emery Street, a 250+/- apartment project at 110 Emery Street, and an approximate 375-unit apartment project on about 12 acres of undeveloped land at 301 1st Avenue. A planned mixed-use “wrap” of the transit station at 1st and Main will add approximately 250 more units. Thus, over the next several years the STEAM area is expected to include about 1,000 new housing units that will serve as a significant catalyst to additional investment.



Current Development Projects: A: Boston Station, live/work, 371 units; B: 110 Emery, retail/restaurant/office; C: Sugar Mill Station, multifamily, 330 units; D: 121 Main Street, mixed-use, office/retail/restaurant, 175 units; E: First & Main Transit Center

TOPOGRAPHY AND VIEWS

The STEAM area has a varied topography with more significant slope from 2nd to 3rd Avenues, and being relatively flat from 2nd Avenue to the St. Vrain Creek. Thus the vast portion of the area south of 2nd Avenue does not have any significant grade issues that pose significant development challenges for individual structures or block faces. Given the anticipated higher density development west of Main Street, higher building heights will be required to capitalize on views of the Rocky Mountains.

» A full summary of existing physical conditions can be found in the Design Brief, included in the appendix of this document.

4.2 OPPORTUNITIES AND CONSTRAINTS



LAND USE

OPPORTUNITIES

- A mix of uses already exists in the STEAM area.
- The City owns a number of potential redevelopment parcels in the STEAM area.
- Large parcels offer flexibility in defining future uses.

CONSTRAINTS

- Proximity of existing, active rail tracks can limit the distribution of land uses, particularly a future performing arts center.



CHARACTER + URBAN DESIGN

OPPORTUNITIES

- Direct geographic relationship with existing Main Street may provide framework for area character.
- Proximity to the St. Vrain Creek bridge offers the potential to define a gateway for STEAM and Longmont as a whole.

CONSTRAINTS

- It may be challenging to integrate recent and anticipated developments into a cohesive identity.



MOBILITY

OPPORTUNITIES

- New roadways can introduce a walkable, human scale to existing large parcels.
- New trail connections to downtown/1st & Main Transit Center/St. Vrain Greenway offer enhanced connectivity.

CONSTRAINTS

- The scale of existing arterials – 3rd Avenue, 1st Avenue, Boston Avenue, Martin Street – will require extensive enhancement to create pedestrian scale.
- Existing railroad alignments limit internal connectivity.

4.3 STRATEGIES



LAND USE

Mixed Use

- Discourage single family detached housing development.
- Create density around - or with easy access to - transit.
- Create higher density multifamily residential, for both ownership and rental.
- Promote integrated housing and amenities that appeal to a spectrum of life stages and family compositions.
- Promote multiple scales of development, through changes in height, density and building coverage.
- Provide space for flexible multi-use buildings that can evolve or adapt to fit a variety of both residential and/or commercial uses.

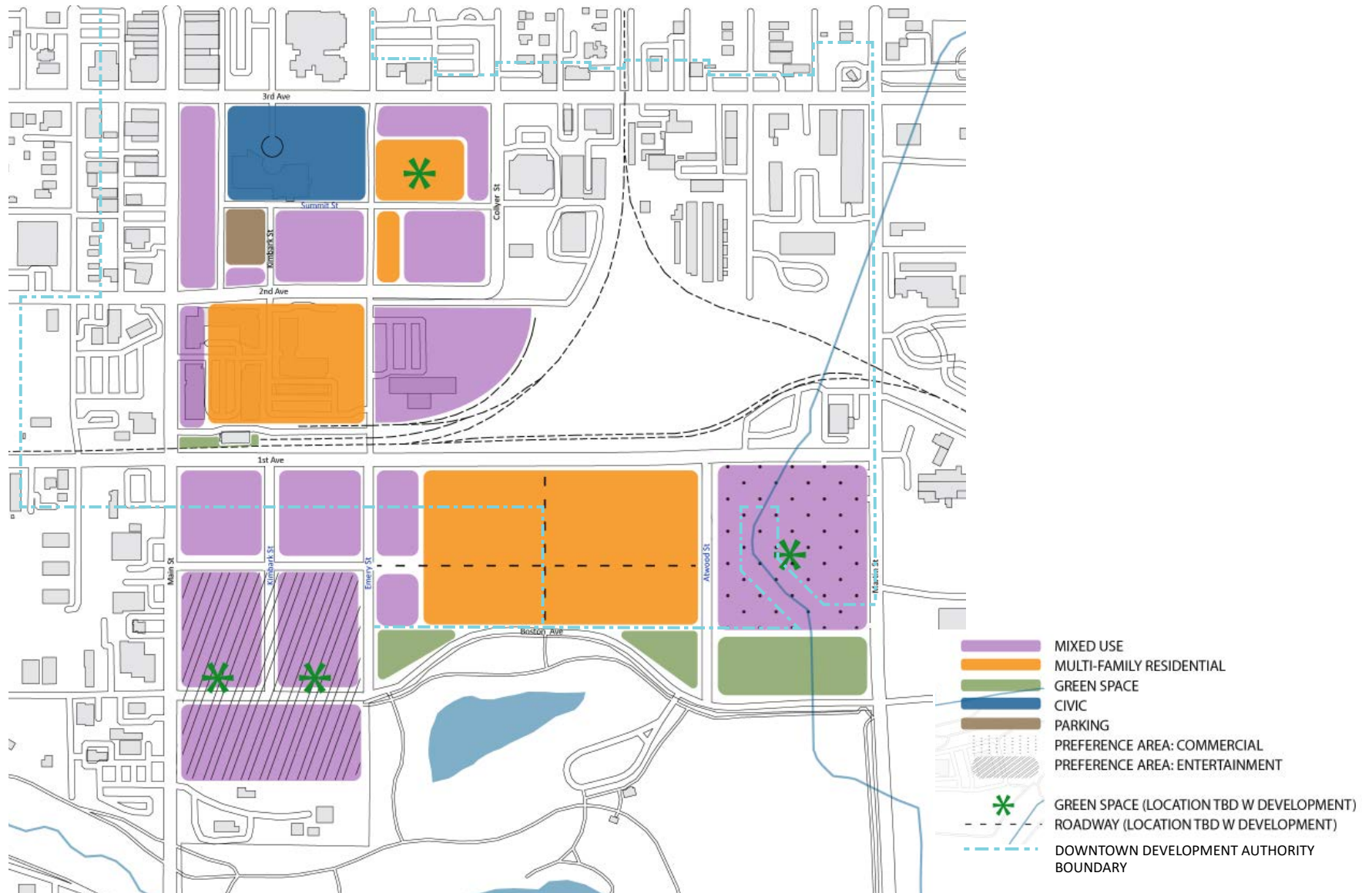
- Provide space for employment uses that promote decreased personal-vehicle commuting.
- Create a mixed-use community that allows people to walk or bike to dining, retail, and employment.

All areas within the STEAM redevelopment area provide opportunity for horizontally and vertically mixed uses. A rich mix of uses decreases the time and expense of traveling from home to separate locations for employment, dining, and recreation. It further serves to meet City goals relative to climate change, quality of life, access to employment, and contributing to the continued success and viability of downtown.

There are specific uses contemplated for STEAM which, if built, should be directed to specific locations within the sub-area; these preference areas (described on the pages that follow) are intended to create nodes

of synergistic uses, which also relate to the existing uses that are expected to remain.

All areas should focus on an urban character. Higher residential density and intensity of activity are encouraged in the areas close to the future 1st & Main transit hub. It is particularly important that these transit-proximate areas offer a variety of housing types that accommodate a wide variety of household compositions with varying needs and preferences. Housing should be limited to attached-dwellings and multi-family buildings. Multi-family residential development should be a minimum height of 4-stories with townhouse-type projects being no less than 2-stories. Exceptions for affordable housing may be given consideration. Residential parking should be premised on a maximum of one space per unit. Single-family detached housing is not encouraged and supported.



Key land use and preference areas within the STEAM redevelopment area.

Preference Area: Cultural

- Identify a location for cultural facilities that reflect Longmont's history and culture.
- Locate sensitive uses such as performance facilities and/or technical equipment to minimize noise and vibration impacts from freight rail.
- Orient uses and service entrances to minimize visual impacts to important roadways.

The parcel south of Boston Avenue is an appropriate location for the performance and/or conference facility along with a hotel. This location would require three primary façades, facing Main Street, Boston Avenue and the Dickens Farm Nature Area. A cultural facility in this location should strive to include an indoor/outdoor space, which could take advantage of and integrates with the adjacent Dickens Farm Nature Area.

Preference Area: Commercial/Institutional Cluster

- Capitalize on visibility of Martin Street frontage.
- Work with Public Safety to identify options for relocating the Fire Training Facility.
- Identify opportunities for public private partnerships to redevelop the City-owned property adjacent to Martin Street.
- Integrate public space into preference zone design.

This preference area is intended to capitalize on Martin Street frontage and its associated visibility, as well as the availability of a large amount of land that can be aggregated if

necessary. In addition, partial City ownership of this area could provide an advantage in incentivizing development. This area provides opportunity for a multi-building development, such as an office or innovation park, or an institutional campus such as education or research. If developed as a single project, this area should include publicly-accessible internal open space. If developed as separate parcels, this area should include open space either fronting on or with access from Atwood Street.

Preference Area: Industrial/Maker

- Identify appropriate locations and configurations for public spaces, such as plazas, pocket parks, and similar to serve existing and future uses.

This preference designation supports the character of existing, well-established uses and current development proposals. Also of note in this area is the sensitivity of the Safety & Justice Center as a land use; replacement/relocation of the current parking and conversion to another use should recognize the security and viewshed concerns of this use. Compatible adjacent uses could be public space for new residential or appropriately oriented residential.

Open Space

- Design the east-west utility easement to offer different kinds of amenities than those offered by Dickens Farm Nature Area.
- Maintain public access to and integrate the north-south drainage near Martin Street with redevelopment.

STEAM enjoys excellent proximity to Dickens Farm Nature Area and the St. Vrain Greenway. The nature area provides a natural open space along with a small improved passive park area along with access to the greenway and creek-based recreational activities such as tubing. Additional public spaces will complement this resource by providing an expanded spectrum of experiences. A new space could run parallel with the sanitary sewer easement running east-west along the north side of Boston Avenue. A performance and/or conference facility could incorporate this



New open spaces should be designed to provide a more urban experience that contrasts with the naturalized/recreational experience available at nearby Dickens Farm Nature Area.

space into its design and programming if it were to be pursued on the north side of Boston Avenue. Adjacent to the proposed Boston Station development, the easement offers two triangles which offer opportunity for street character enhancements and public usage such as community gardens, pocket parks or plazas.



CHARACTER AND URBAN DESIGN



A civic building provides a large 'lantern' to mark the building's entrance and elevate the public realm.

Gateway Features

- Use streetscape elements and public art to connect the St. Vrain Creek bridge gateway through STEAM and to the existing downtown core.
- Provide iconic architectural treatments at key Main Street locations.
- Extend the existing historic downtown streetscape enhancements between 1st and 3rd Avenues.

STEAM will extend Longmont's already successful, pedestrian-focused Main Street several more blocks south, linking downtown with existing gateway treatments on the bridge. As STEAM's section of Main Street is enhanced, it should continue these themes; new artwork or streetscape need not be a literal reproduction of the pillars-and-art on the bridge, but should provide a similar and continuous character.

New buildings at high-visibility gateway locations, particularly cultural or civic buildings, should be designed to provide enhanced architecture or public art at these critical locations: Boston Avenue & Main Street is a gateway for all of downtown, while Boston Avenue & Martin Street is an important STEAM gateway.

Heights and Views

- Eliminate maximum building height restrictions.
- Calibrate building heights to roadway width.

STEAM's goals of increasing access to the 1st & Main transit hub and of promoting an amenity-rich urban environment suggest that higher heights than what are currently found in Longmont may be appropriate.

Shared Street

- Design and construct a north-south street as a pedestrian-oriented festival street.
- Locate service and deliveries out of view of shared street.
- Design adjacent buildings to have primary façades facing shared street.
- Extend design elements from shared street into the public open space.

The Cultural Preference Area will include a flexible multi-use street, which can be closed for events. The street will be designed with a high degree of pedestrian interest, in both materials and street elements. The street will include street trees as well as ornamental annual and perennial plantings, either in pots or integrated planters; this design aesthetic should be continued into the

new public plaza/park spaces at the south end of the block. To maintain the integrity of materials and elevate the aesthetics of the adjacent building façades, no service functions should take place along this street; service entrances/loading docks and trash receptacles should be located on adjacent east-west streets.

Low Impact/Green Infrastructure and Sustainability

- Create incentives for green infrastructure and sustainable development.
- Integrate renewable energy throughout the study area whenever possible.

The development of new buildings offers opportunities for use of sustainable energy and product/materials technologies at scale. Both passive and active energy technologies should be encouraged, but implemented in a manner that enhances or is complimentary to design character.



Shared streets: Bell Street Park in Seattle (top) is temporarily closed to vehicles for an event; a shared street in Madison WI features a curbsless cross-section.



MOBILITY



The sub-area plan will accommodate and balance all mobility modes: pedestrians, cyclists, transit riders, emergency services and individual drivers.

Complete Streets

- Update existing roadways, including 3rd Avenue, to offer pleasant and safe bike and pedestrian utilization, including, as viable, reducing the number of vehicular travel lanes.
- Provide strong physical and visual connections between new housing areas and the opportunities and culture within the core of Longmont. All roadways should be designed and constructed to a standard acceptable for conveyance to the City as a public street.
- Install facilities accommodating both pedestrians and bicycles on both sides of Martin Street.
- Prepare design plans for and implement streetscape improvements between the railroad tracks and the St. Vrain Creek.

Modal choice is a priority in STEAM, and people should feel comfortable walking and biking on all streets within the sub-area. Major existing roadways such as 3rd Avenue, 1st Avenue, Boston Avenue and Martin Street should be upgraded to introduce pedestrian and bicycle facilities where they don't exist, increase the comfort and safety of existing facilities and provide traffic calming as appropriate. Multi-block redevelopments, such as 301 1st Avenue and potentially the fire training area, should design all internal roadways – whether publicly dedicated or private – as full roadways rather than drives through parking; these roadways should provide multimodal facilities as appropriate and follow the street cross-sections in this document. Wayfinding and urban design enhancements should create a physical and visual, character connection with the existing Main Street core north of the STEAM area running through downtown.

Transit

- Plan space on walkways for active mobility sharing companies to operate visibly and conveniently.
- Plan for and manage curb space appropriately for motorized micro-mobility options using the street.
- Coordinate route optimization with RTD and St. Vrain Valley School District (SVVSD).
- Ensure safe active mode connections to the 1st & Main Transit Station.
- Consider future opportunities for extending rail and bus transit to the STEAM and Sugar Factory areas.
- Consider microtransit to help fill transit gaps from traditional transit options.

As roadways and the public realm is upgraded, design should accommodate bike share parking, with additional consideration given to the best use of curb space for future on-demand or microtransit, carshare, and rideshare access to the site. In addition, indoor and outdoor secured bike parking should be encouraged during the development review process as an amenity for both residential and commercial/office buildings. Finally, most of STEAM is immediately adjacent to the planned transit station, with over half the area within a ¼ mile of the facility. It is also currently immediately served by or within a half-mile of a bus route; but, routes and stop locations should be reconsidered as density and population patterns become clear, and as the potential for new or rerouted service to Sugar Factory are explored.

Trails as Mobility

- Enhance pedestrian/bike connections to the St. Vrain Greenway by integrating facilities into the proposed street network as applicable.
- Provide intuitive wayfinding to reinforce STEAM connectivity with larger regional systems.

By integrating the historic street grid as an element of the STEAM urban design context, bicycle and pedestrian connections will be incorporated into the public street system. These facilities should pay particular attention to connecting on-street, buffered or separated bike/ped systems to the existing St. Vrain Greenway, which acts as the spine of the Longmont trail system. Crosswalks, street narrowing, signalization, and appropriate signage will be particularly important for safe active mode crossings of Boston Avenue.

Inter-Area Connectivity

- Investigate/obtain long-term options to provide more direct connection between the STEAM and Sugar Factory areas.
- Enhance pedestrian/bicycle connections to the 1st & Main Transit Station, including crossing improvements on Main Street.

Opportunities may arise in the future to enhance connectivity between the STEAM and Sugar Factory areas, and these opportunities should be monitored and pursued as appropriate. Notable opportunities could include negotiations with BNSF railroad relative to crossing of the rail storage tracks. Such a roadway and/or pedestrian connection eastward into the Sugar Factory area with connectivity to Boston Avenue or 1st Avenue would significantly enhance east-west travel. Additionally, the potential extension of 2nd Avenue to Rogers Road and/or the extension of Weaver Park Road to Martin Street could both be long-term connectivity enhancements.

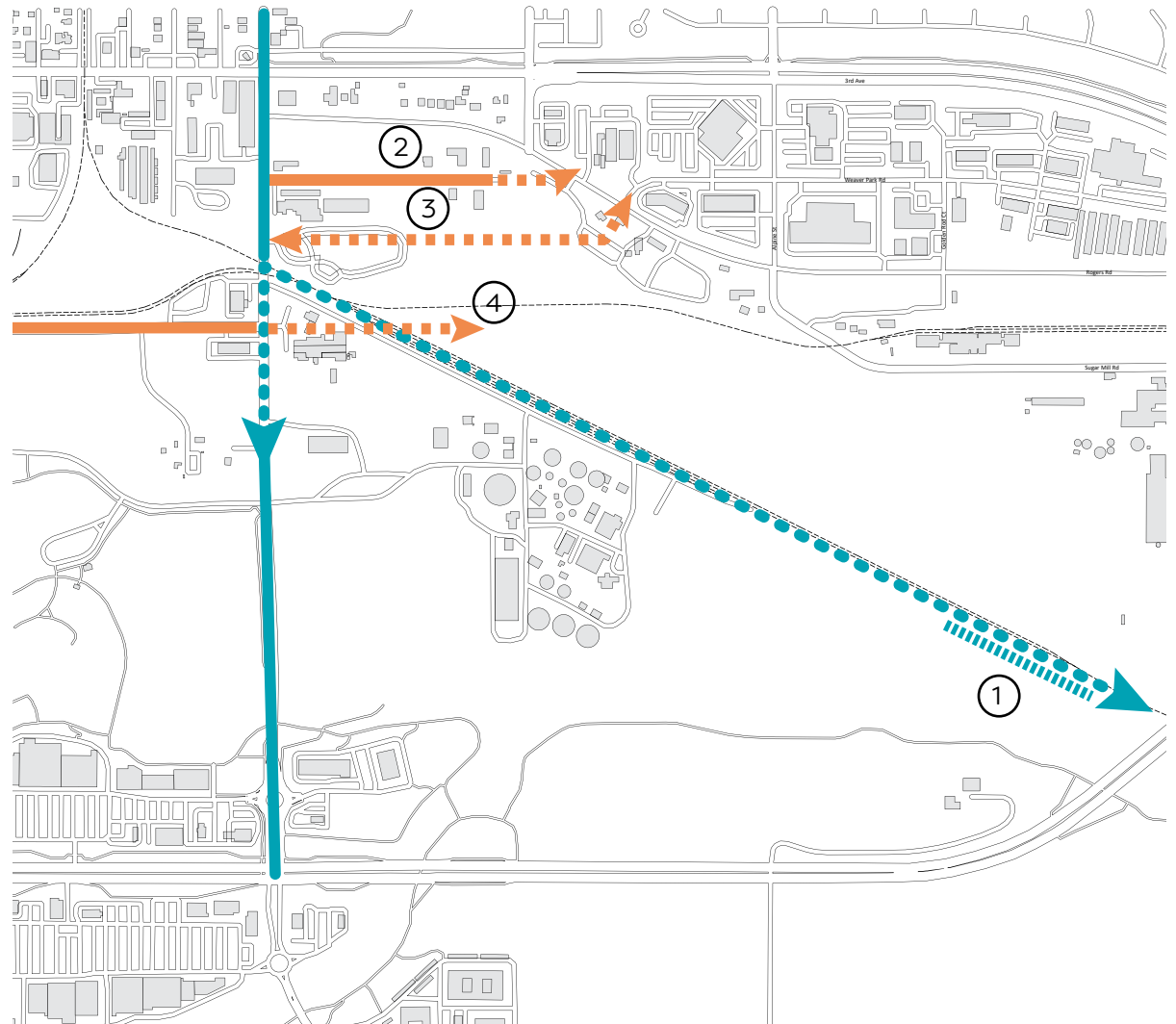
Parking

- Promote and incentivize structured parking in place of surface lots.
- Coordinate with Public Safety to construct shared structured parking that meet community needs, while still providing safe, secure parking for activities of the Safety & Justice Center.
- Introduce district-wide shared parking.

A new parking structure in the lower downtown would assist in reducing the amount of land needed for vehicle parking, preserving more available land for development in the vicinity of Main Street and 2nd Avenue. The reduction of surface parking lots will also promote STEAM priorities by contributing to a more pedestrian-friendly, human scaled urban environment. The structure should provide parking for commercial and retail uses, as well as the nearby justice center.

Potential Inter-Area Connectivity

- ① Connect multi-use trail on north side of rail storage track, connecting Sugar Factory area and adjacent open space. (Near-Term)
- ② Pursue the possible extension of 2nd Avenue to Rogers Road as development of properties in vicinity of alignment occurs. (Long-term/Oppportunistic)
- ③ Potential connection of Weaver Park Road and Martin Street (Long-term/Oppportunistic) and/or
- ④ Pursuit of crossing of rail storage track, to extend 1st Avenue from Martin Street into Sugar Factory area. (Long-term/Oppportunistic)

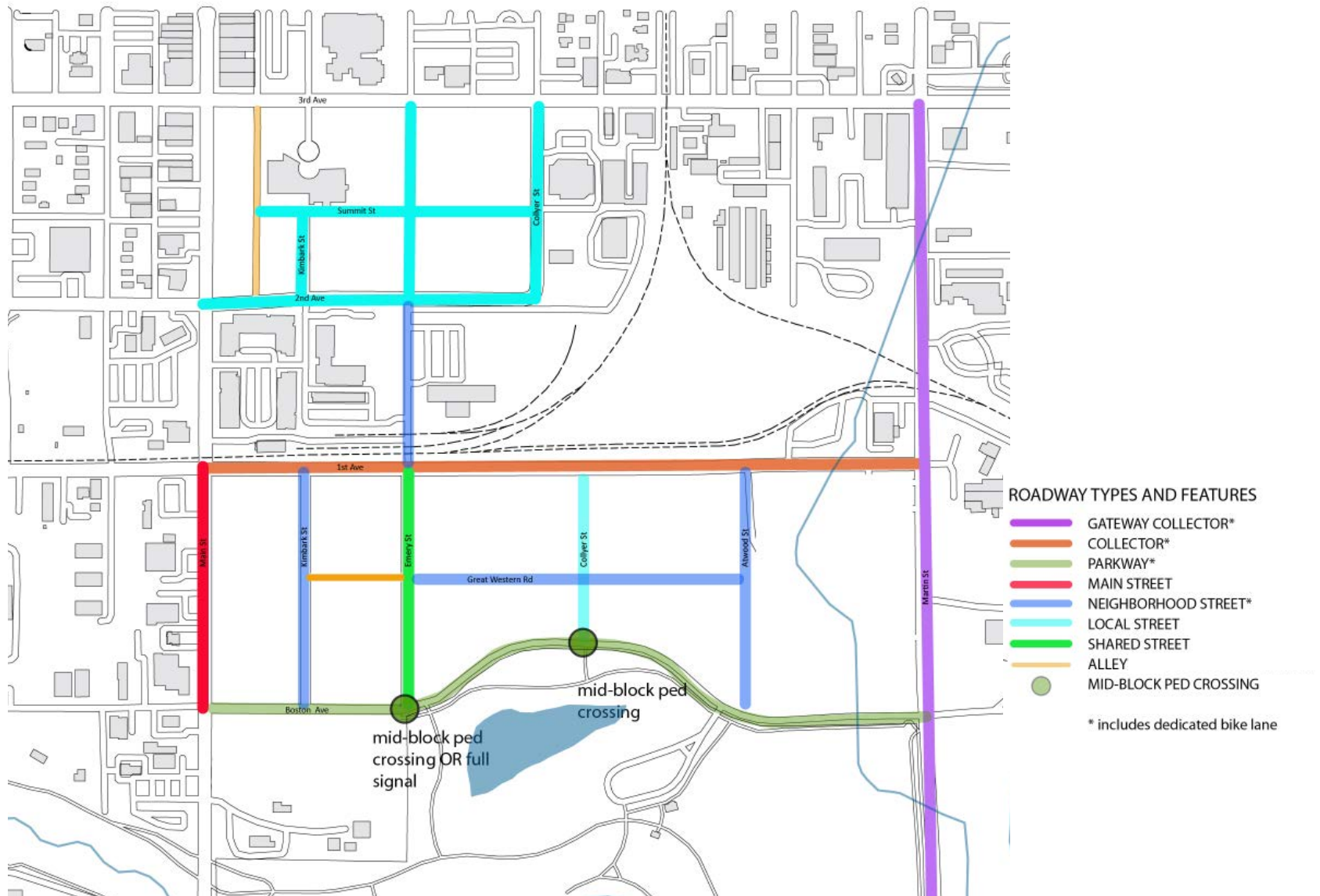


4.4 ROADWAY SECTIONS

Many but not all of the roadway cross-sections shown in this part of the document are retrofits of existing roadways, with a focus on enhancing comfort and safety for pedestrians and cyclists. Exceptions are the shared street, extensions of existing roadways that break large parcels into smaller blocks, and new roadways within the Boston Station development area; these roadways maintain focus on pedestrians and cyclists but are new roadways.

Regardless of mode of travel, all residents, workers and visitors should feel that they can move throughout the STEAM area without conflict, and that they *belong* on the street. These sections should be considered ideal designs, and have some degree of flexibility to adjust to the type and density of uses on each block; while certain elements may be widened or relocated within the cross-section (choosing to provide a multi-use path behind the curb on Martin Street, for example, instead of having a sidewalk plus and on-street bike lane), the element shown should always be provided.

» The following sections are the same in Sugar Factory and STEAM. They are included in both chapters so that each chapter may act as a stand-alone guide to development in each area: Gateway Collector (small dimensional difference due to existing ROW); Neighborhood Street; Local Street; Shared Street; Alley.



Roadway types within the STEAM redevelopment area.

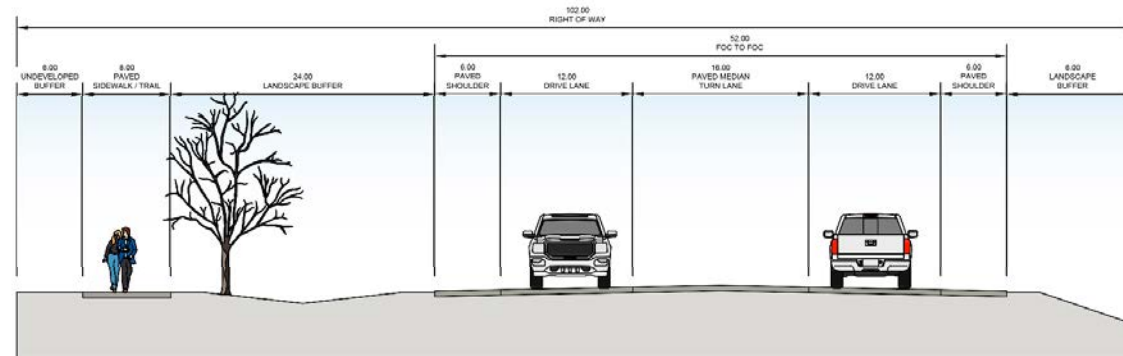
GATEWAY COLLECTOR

- Martin Street

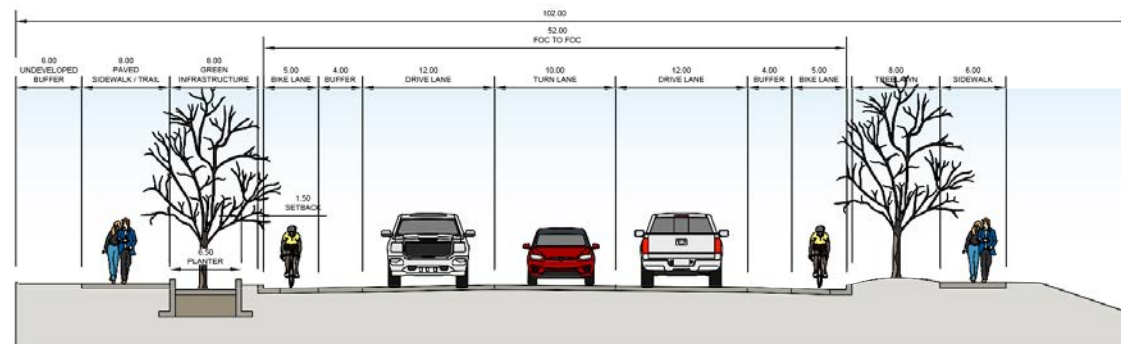
This cross-section recognizes Martin Street as a major through-street collector that connects existing neighborhoods north of 3rd Avenue to the retail centers south of the creek along Ken Pratt Boulevard. Existing geometry, traffic control and posted speed limit make this a challenging corridor for pedestrians and bicyclists; for this reason, a generous buffer should be included between bike lanes and adjacent traffic; tree plantings will also provide visual separation and a degree of traffic calming. Existing development projects and projects currently under construction/development exhibit widely varied character; for this reason, street trees may be planted in green infrastructure planters, tree lawn or bioswales depending upon the character of adjacent redevelopment. Tree planting may vary by block and by size of the road. Alternately, although not shown in the section, the on-road buffered bike lane and sidewalk could be jointly replaced with a 10 to 12-foot wide, detached multiuse path behind the curb.

	Appropriate Elements	Gateway Collector
Public Realm	Ped Lighting	x
	Street Furniture	
	Street Trees	x
	Micromobility Parking	
	Mobility Hub	x
	Loading Zones	
	Parklets	
	Transit Stops	x
	Paid Parking Kiosks	
	On-Street Parking	
Safety	Crosswalks	x
	Curb Bump-Outs	x
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	
	Stormwater Tree Trenches	x

EXISTING CONDITION



PROPOSED CONDITION



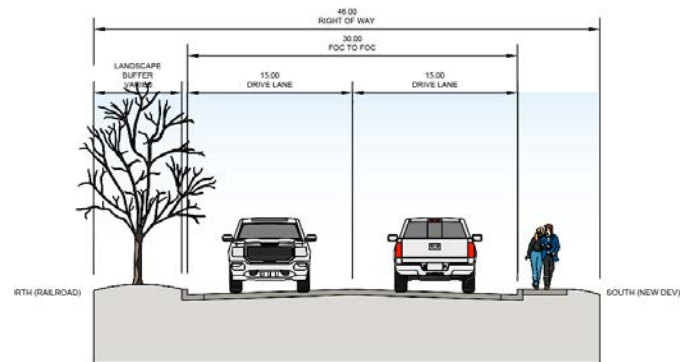
COLLECTOR

- 1st Avenue

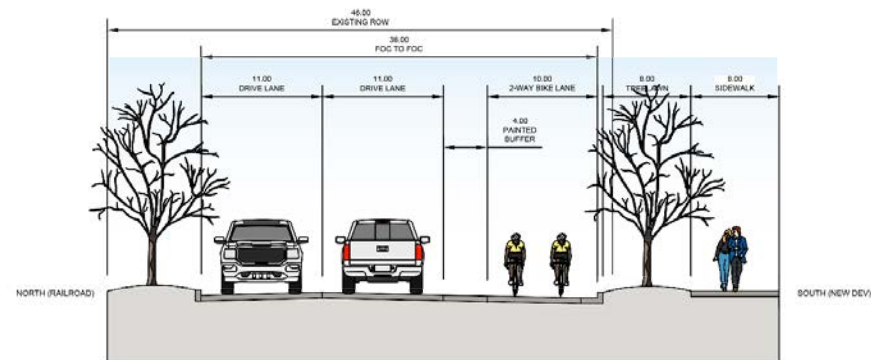
This cross-section recognizes the continued presence of the railroad track and rail storage yard on the north side of the street. Pedestrian and bicycle facilities are aggregated on the south side of the street, allowing additional space for planted buffering of the rail uses on the north side. Due to higher traffic levels and straight geometry that allows potentially greater speeds, the bike lane should also be buffered.

Appropriate Elements		Collector
Public Realm	Ped Lighting	x
	Street Furniture	
	Street Trees	x
	Micromobility Parking	
	Mobility Hub	
	Loading Zones	
	Parklets	
	Transit Stops	x
	Paid Parking Kiosks	
	On-Street Parking	
Safety	Crosswalks	x
	Curb Bump-Outs	x
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	
	Stormwater Tree Trenches	x

EXISTING CONDITION



PROPOSED CONDITION



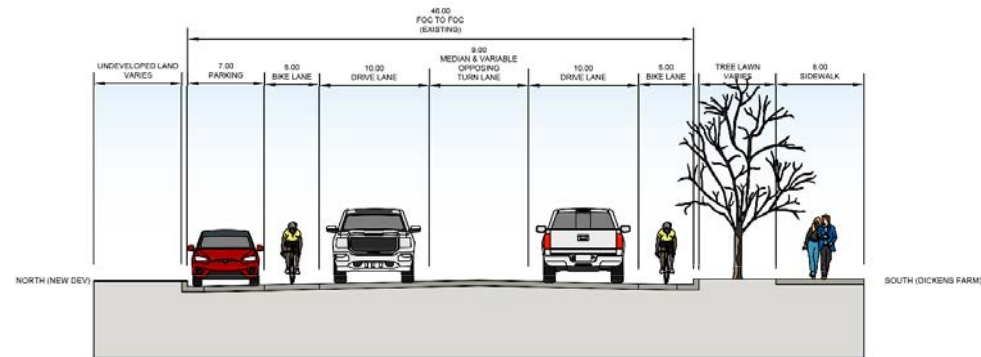
PARKWAY

- Boston Avenue

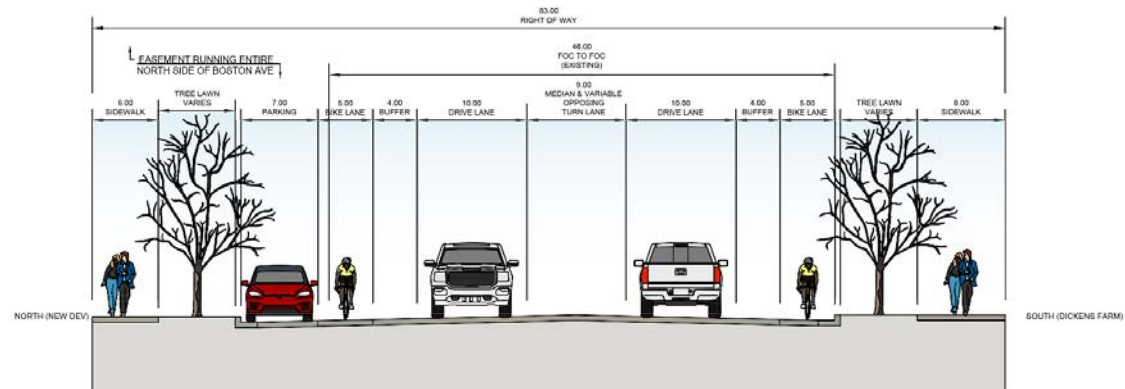
This cross-section anticipates a significant amount of residential development on the north side of the roadway between the Emery and Atwood extensions. This change is expected to bring an increased demand for on-street parking, as well as upticks in both pedestrian and bicycle traffic including crossing movements to access the Dickens Farm Nature Area. The cross-section is predicated on re-allocation of existing curb-to-curb space via re-striping. The center median/turn lane is retained where necessary; at locations with curb cuts into the Boston Station development, the non-turning side of the median should be raised to create a protected pedestrian refuge paired with a marked pedestrian crosswalk.

	Appropriate Elements	Parkway
Public Realm	Ped Lighting	x
	Street Furniture	
	Street Trees	x
	Micromobility Parking	x
	Mobility Hub	x
	Loading Zones	
	Parklets	
	Transit Stops	x
	Paid Parking Kiosks	x
	On-Street Parking	x
Safety	Crosswalks	x
	Curb Bump-Outs	x
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	
	Stormwater Tree Trenches	x

EXISTING CONDITION



PROPOSED CONDITION



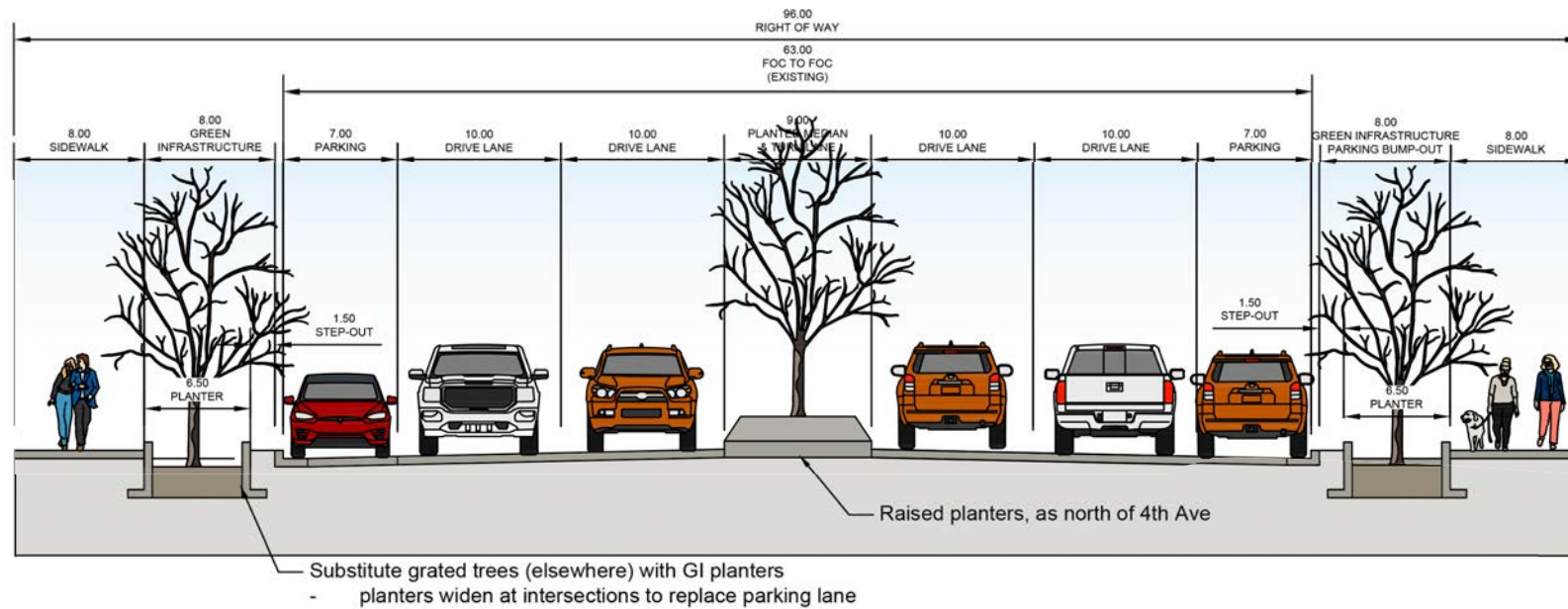
MAIN STREET

- Main Street

In contrast to the Sugar Factory area where ‘main street’ is a placeholder designation for a street to be named later, this section is truly Longmont’s named Main Street and US Highway 287. Existing lanes and dimensions are maintained, with recommendations to add tree plantings within a new center median. This cross section also emphasizes regular street trees and widened sidewalk on both sides of the street. This improvement has already been made in front of the new South Main Station, and for consistency, this new construction should serve as the model for Main Street upgrade along STEAM’s frontage.

	Appropriate Elements	Main Street
Public Realm	Ped Lighting	x
	Street Furniture	x
	Street Trees	x
	Micromobility Parking	x
	Mobility Hub	x
	Loading Zones	
	Parklets	
	Transit Stops	x
	Paid Parking Kiosks	x
	On-Street Parking	x
Safety	Crosswalks	x
	Curb Bump-Outs	x
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	
	Stormwater Tree Trenches	x

PROPOSED CONDITION



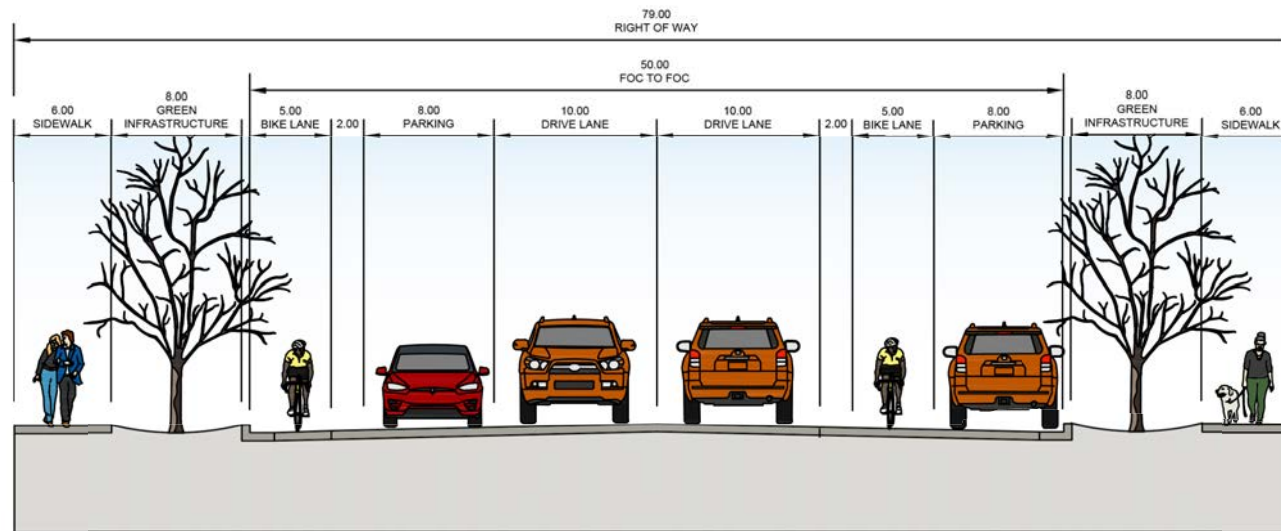
NEIGHBORHOOD STREET

- Atwood Street
- Great Western Street (name placeholder for east-west street through 301 1st Street)

This is a new roadway that provides optimal multimodal facilities. Two bike lane configurations are shown, and should be selected based on adjacent use and intersection design: on the left side of the section, the curbside, buffered bike lane moves bicyclists from the door zone of parked cars. On the right side of the section, the buffered bike lane is located between the parking and drive lanes; this configuration works better with intersections that have curb extensions to reduce pedestrian crossing distance.

	Appropriate Elements	Neighborhood Street
Public Realm	Ped Lighting	x
	Street Furniture	
	Street Trees	x
	Micromobility Parking	x
	Mobility Hub	
	Loading Zones	x
	Parklets	x
	Transit Stops	x
	Paid Parking Kiosks	x
	On-Street Parking	x
Safety	Crosswalks	x
	Curb Bump-Outs	x
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	x
	Stormwater Tree Trenches	

PROPOSED CONDITION



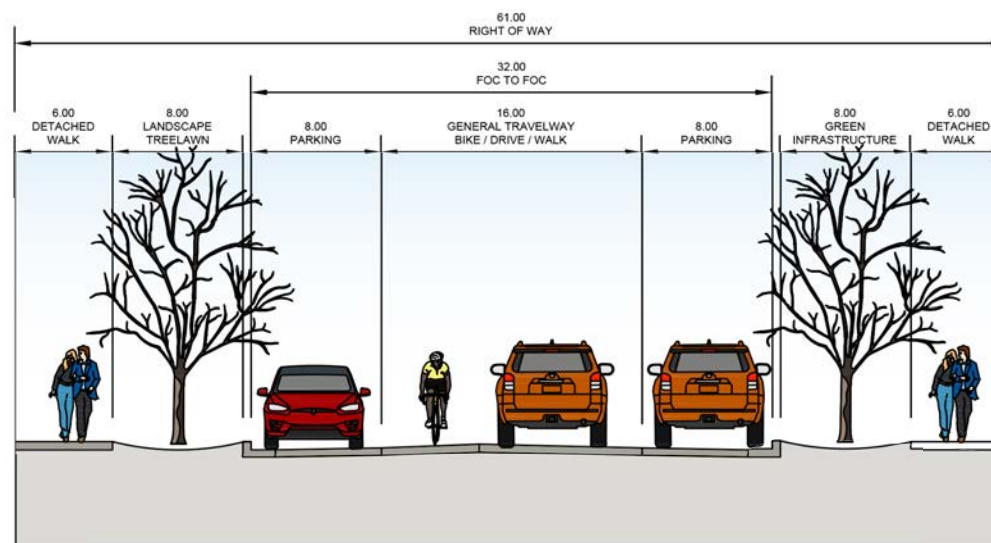
LOCAL STREET

- Summit Street
- 2nd Avenue
- Collyer Street

This roadway reduces street width by providing a shared drive/bike lane in place of dedicated bicycle infrastructure. Due to the narrow width of the travelway when two cars meet, one vehicle must typically pull aside and wait for the other to pass. Intensity of adjacent development, and the associated levels of traffic generated, should be considered when deciding between the Local and Neighborhood street typologies, with the Local typology chosen for lower traffic/less intensive development areas. Street trees on the local street will typically be planted in a less urban condition, such as a tree lawn or swale.

	Appropriate Elements	Local Street
Public Realm	Ped Lighting	x
	Street Furniture	
	Street Trees	x
	Micromobility Parking	
	Mobility Hub	
	Loading Zones	
	Parklets	
	Transit Stops	
	Paid Parking Kiosks	
	On-Street Parking	
Safety	Crosswalks	
	Curb Bump-Outs	
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	
	Pervious Paving	x
	Stormwater Tree Trenches	

PROPOSED CONDITION



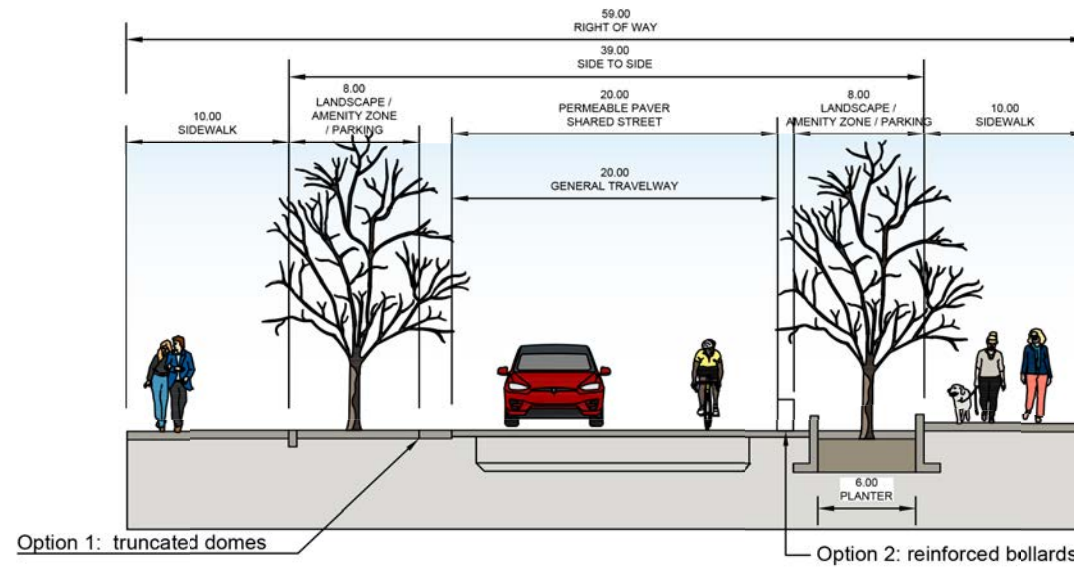
SHARED STREET

- Emery Street

This one-block long street will be located within the Cultural Preference Area, offering flexible public open space that can serve as spillover and pre-event space for adjacent Arts & Event Center and Music Venue. This potentially curbless ‘pedestrian mall’ can be closed for festivals and events, and will use tactile paving or bollards to mark edge of drivable area. Sidewalk is shown as a minimum 10’ in width, but is anticipated to vary (potentially much wider) based on the design of adjacent structures. This type of street is sometimes called a ‘woonerf’ and is truly designed as a pedestrian mall in which motorized vehicles are sometimes allowed as ‘guests’.

	Appropriate Elements	Shared Street
Public Realm	Ped Lighting	x
	Street Furniture	x
	Street Trees	x
	Micromobility Parking	x
	Mobility Hub	
	Loading Zones	
	Parklets	x
	Transit Stops	
	Paid Parking Kiosks	x
	On-Street Parking	x
Safety	Crosswalks	x
	Curb Bump-Outs	
	Driveways/ Curb Cuts	
Green Infra-structure	Bioretention Facilities	x
	Pervious Paving	x
	Stormwater Tree Trenches	x

PROPOSED CONDITION

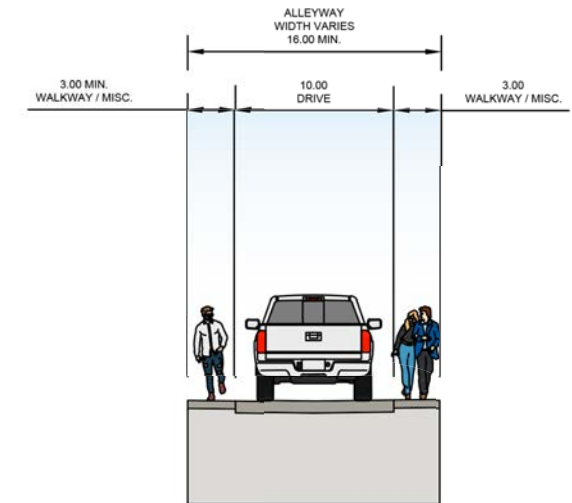


ALLEY

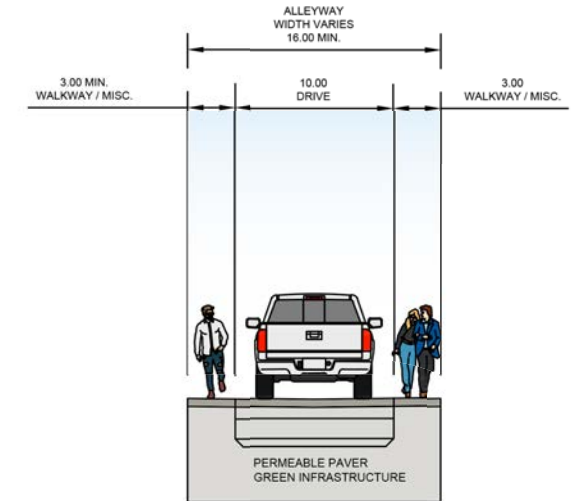
This street can be used in any part of the community; focus is on service use and fire protection. Width may vary; dimensions shown are minimums. Depending upon vehicle weight and frequency of use, alleys may be candidates for permeable paving.

	Appropriate Elements	Alley
Public Realm	Ped Lighting	
	Street Furniture	
	Street Trees	
	Micromobility Parking	
	Mobility Hub	
	Loading Zones	x
	Parklets	
	Transit Stops	
	Paid Parking Kiosks	
	On-Street Parking	
Safety	Crosswalks	
	Curb Bump-Outs	
	Driveways/ Curb Cuts	x
Green Infra-structure	Bioretention Facilities	
	Pervious Paving	x
	Stormwater Tree Trenches	

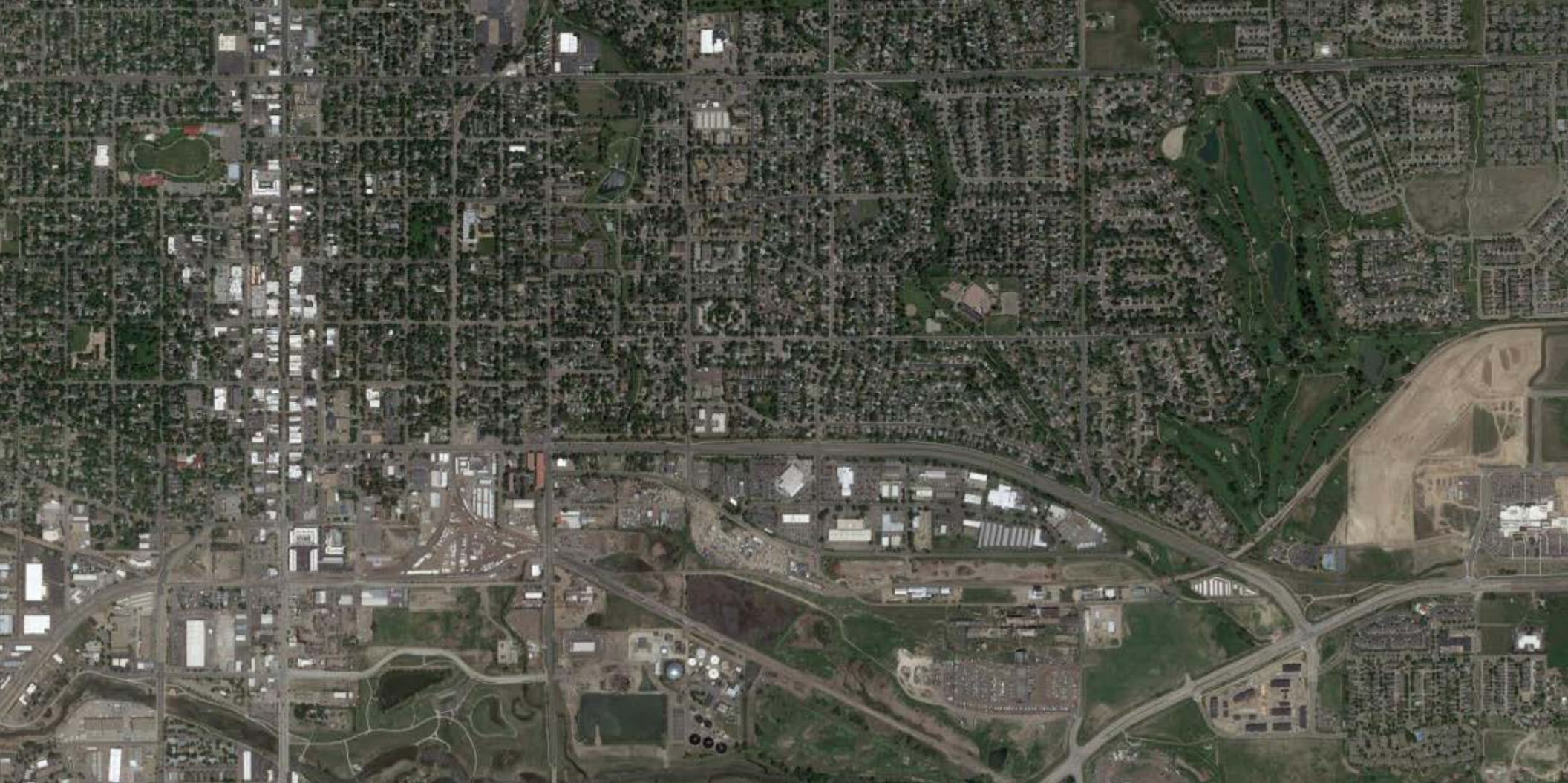
ALLEY



GREEN ALLEY



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SUGAR FACTORY + STEAM: **SUB AREA PLAN**

APPENDICES

APRIL 2023



5. **APPENDICES**

- 5.1 DESIGN BRIEF
- 5.2 PLAN ALTERNATIVES
- 5.3 PUBLIC ENGAGEMENT
- 5.4 MARKET STUDY UPDATE
- 5.5 GREEN INFRASTRUCTURE
STRATEGIES

5.1 DESIGN BRIEF

Design Brief starts on next page.



SUGAR MILL + STEAM SUB AREA PLANNING

DESIGN BRIEF
SUMMARY MEMO OF EXISTING CONDITIONS
FEBRUARY 2022



TABLE OF CONTENTS

DESIGN BRIEF

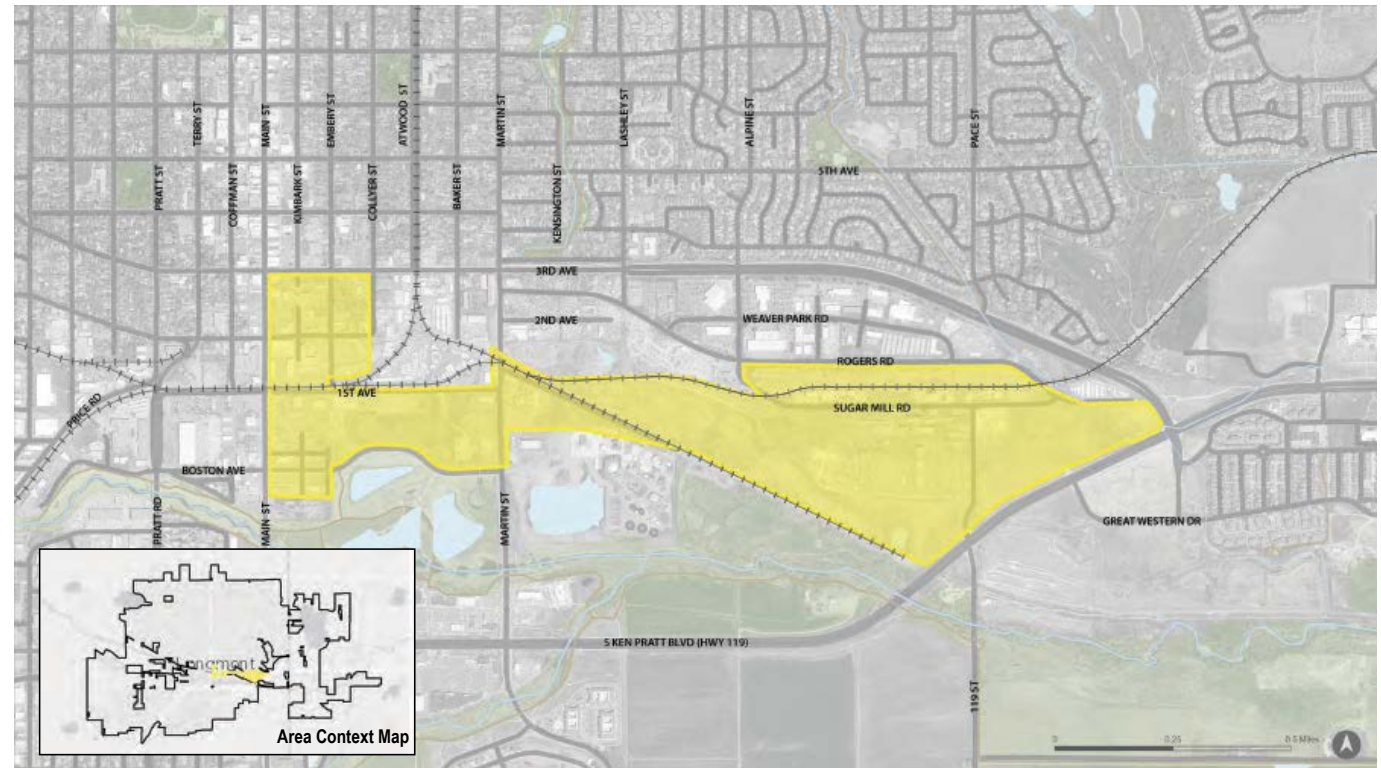
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Initial Analysis: Opportunities & Challenges.....	33
Draft Project Goals.....	34



Baseline Conditions

Study Area

The study area encompasses both the historic Sugar Mill area and the STEAM area along south Main Street, between 1st Avenue and the St. Vrain river. This sub-area plan will focus on development opportunities and challenges, urban design, multimodal connectivity, and an assessment of the development timing of public and private lands around key corridors. These corridors include: St. Vrain River Corridor, the Main Street Corridor, Martin Street, S. Ken Pratt Parkway and the surrounding the Sugar Mill.





Baseline Conditions

PLANNING CONTEXT: MAJOR CHANGES

Major upcoming changes to Longmont's transportation network are shown on the map to the right and include:

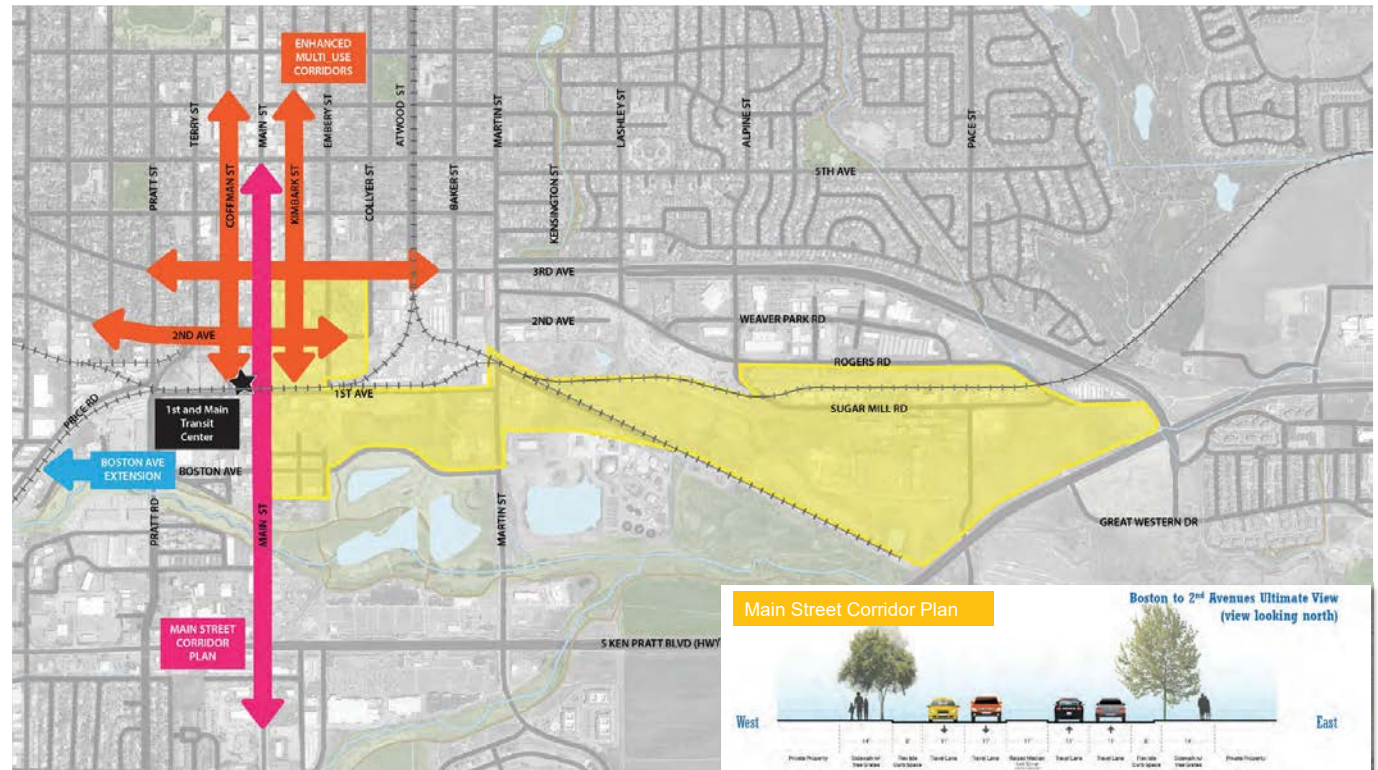
- Boston Ave Extension
- Main Street Corridor Plan
- 1st and Main Transit Center
- Enhanced Multi-Use Corridors

ISSUES:

- Planned corridor changes only access the study area on the western edges.
- Planned transit investments are outside of the study area.

OPPORTUNITIES:

- 1st and Main Street Transit Center is adjacent to the western portion of the site, creating limited access to transit within the study area.





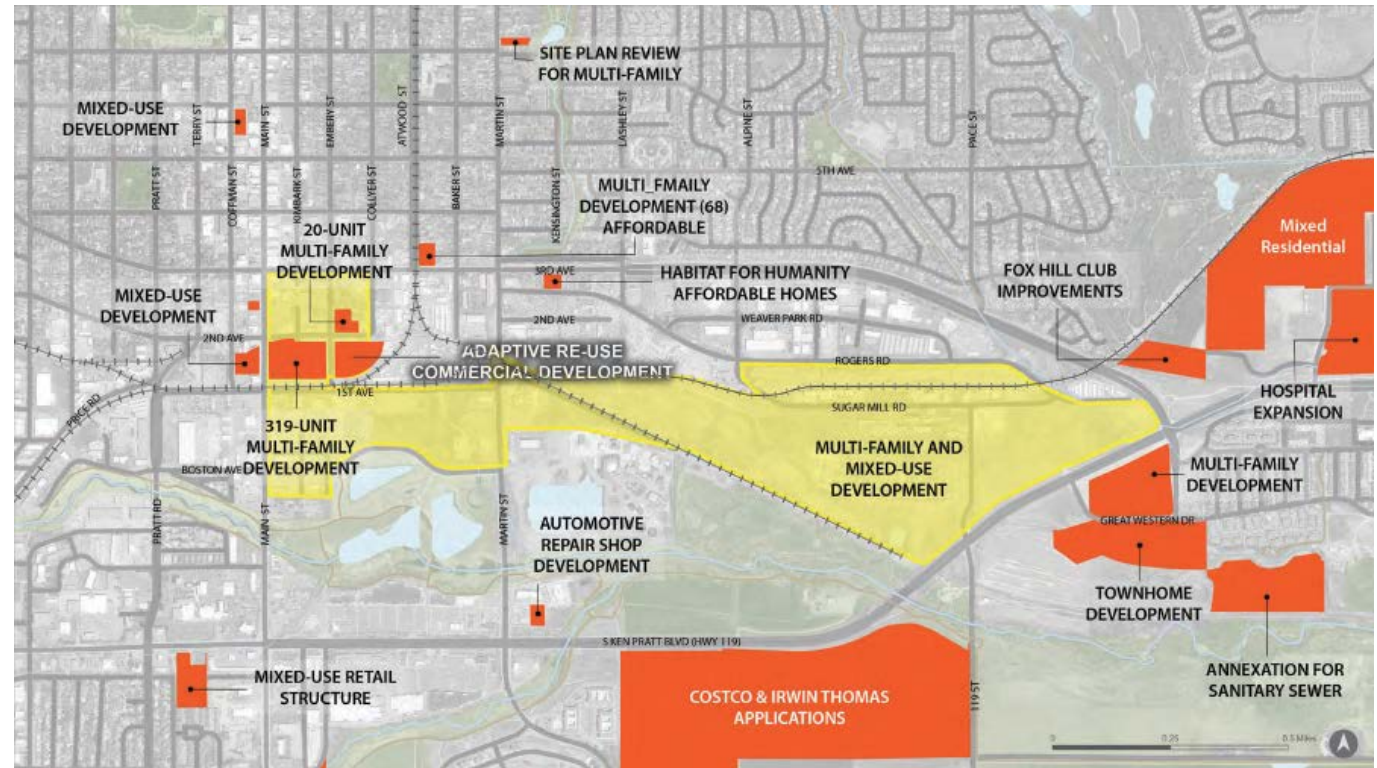
AREA DEVELOPMENTS (as of 11/21)

ISSUES:

- Significant amount of new development along Ken Pratt corridor – a high-speed, high-volume roadway.

OPPORTUNITIES:

- The site is surrounded by development on all sides. This may present an opportunity for coordinated planning.
- Development at the site can connect to the multi-family development across Ken Pratt (east of site).





RAILROADS AND OWNERSHIP

ISSUES:

- BNSF Railway cuts through the site (used for train storage).
- Railroad crossings are limited (~0.5 miles apart, or a 10-minute walk).
- Railway often requires that two existing crossings be closed in exchange for creating a new crossing.

OPPORTUNITIES:

- Use grids to disperse traffic between railroad crossings.
- Build railroad character into future development.





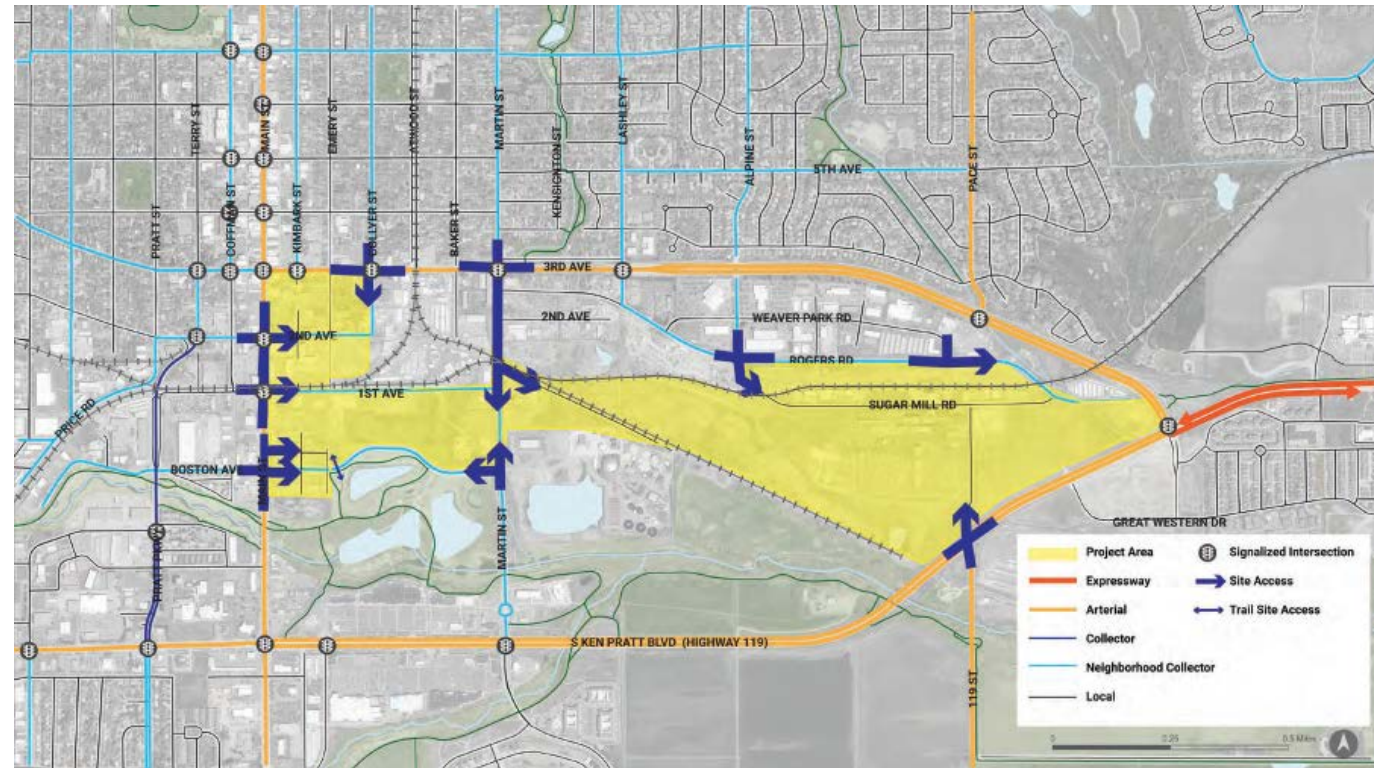
SITE CONNECTIVITY

ISSUES:

- Very limited connectivity to south and east.
- Lashley Street is signalized but does not go south to site. Alpine Street is not signalized but does link through to site.
- Railroads and wetlands act as barriers for roads.
- Currently only one connection to trail system.

OPPORTUNITIES:

- Trails provide non-motorized connectivity.
- Good connectivity to west of site, which is close to downtown.





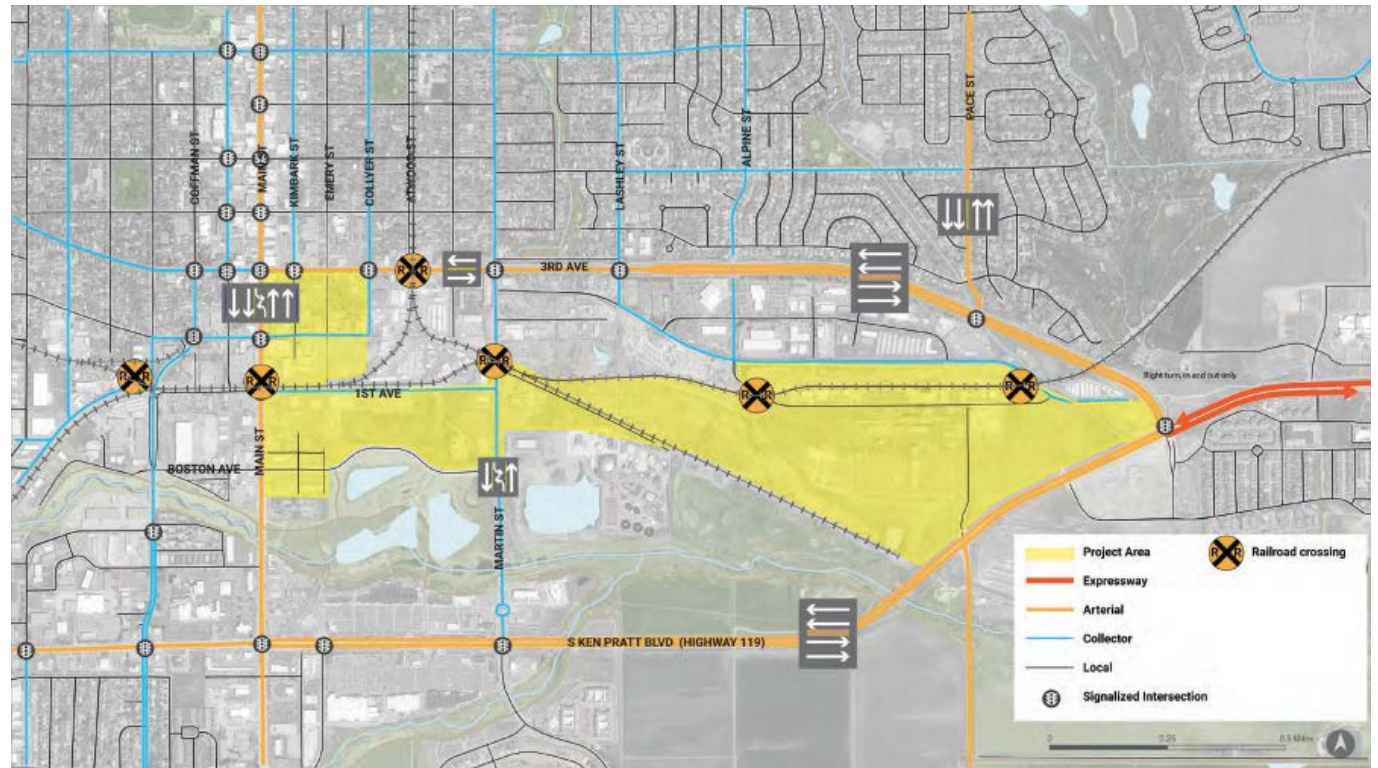
ROADWAY CLASSIFICATIONS AND CROSS SECTIONS

ISSUES:

- Existing roadway network does not extend to the north of the project site.
- Railyard acts as a barrier to site from existing Downtown.
- Street network to south and east limited.
- Ken Pratt is a 4-lane, separated highway without opportunity for multi-modal travel.
- No roads cut east-west through site.

OPPORTUNITIES:

- Carry Downtown Longmont road grid onto the site, particularly near western edge.





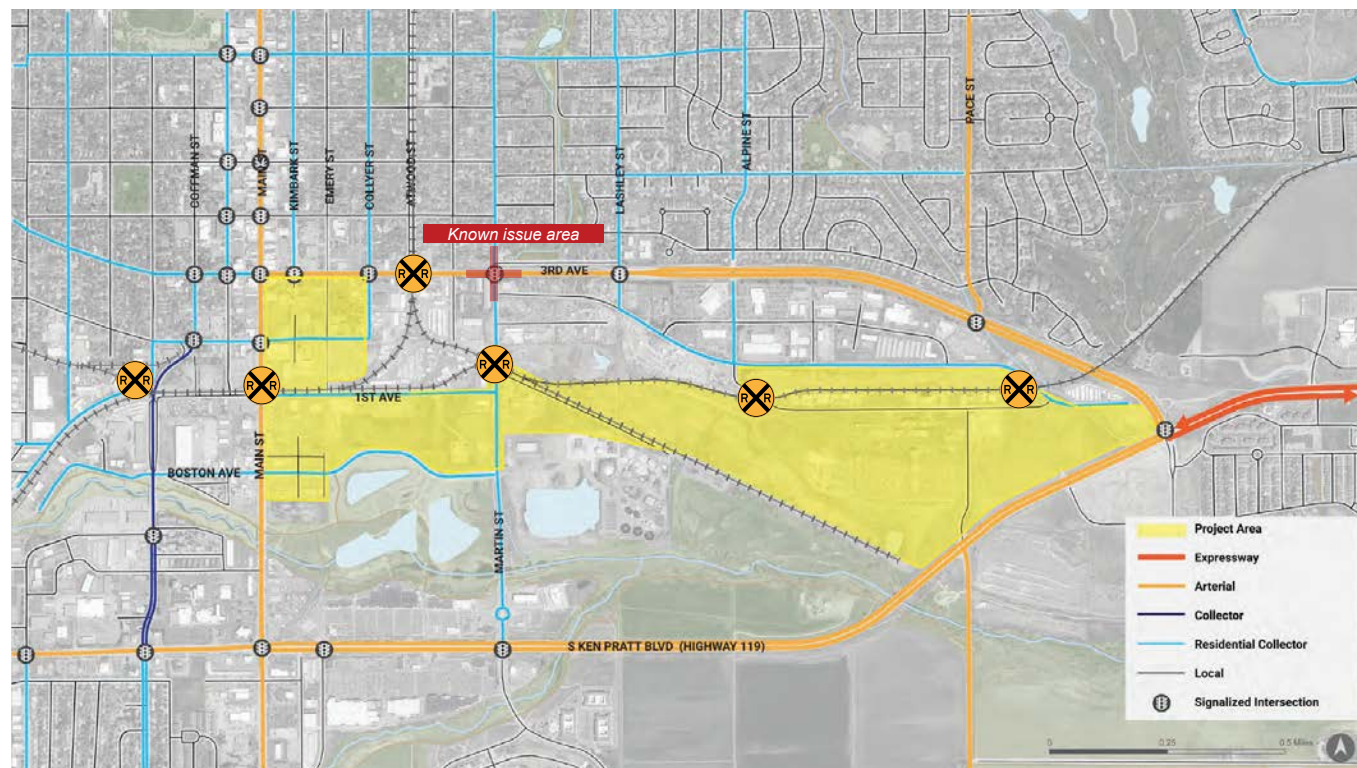
NEW TRAFFIC SIGNALS AND ROADWAY PROJECTS

ISSUES:

- No signal planned for Alpine Street.
- Martin Street at 3rd Street is a known congestion point.

OPPORTUNITIES:

- New traffic signals may facilitate additional access to/from site.





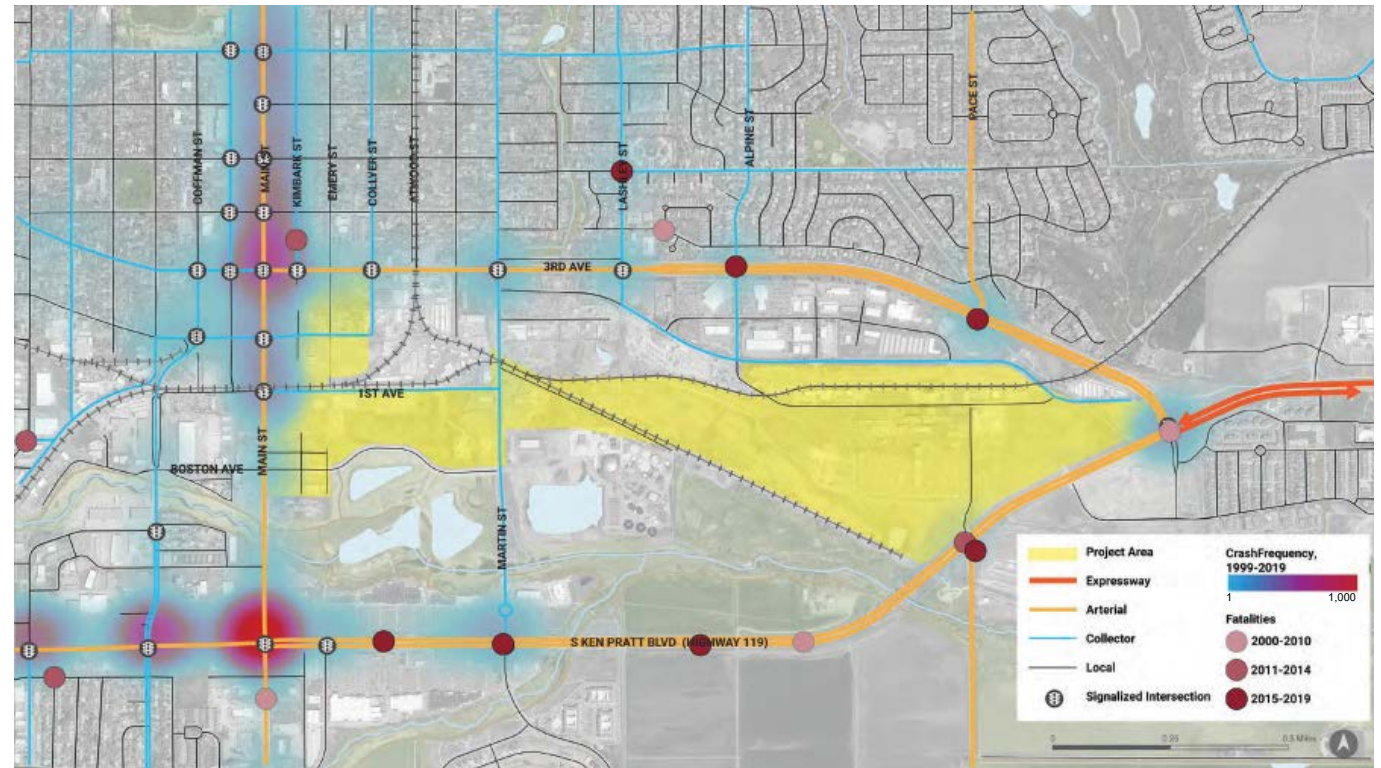
CRASHES

ISSUES:

- Crashes around site edges reveal safety issues.
- There are currently no plans to improve safety at the 119/Main Street intersection.

OPPORTUNITIES:

- Future developments can ameliorate safety issues.





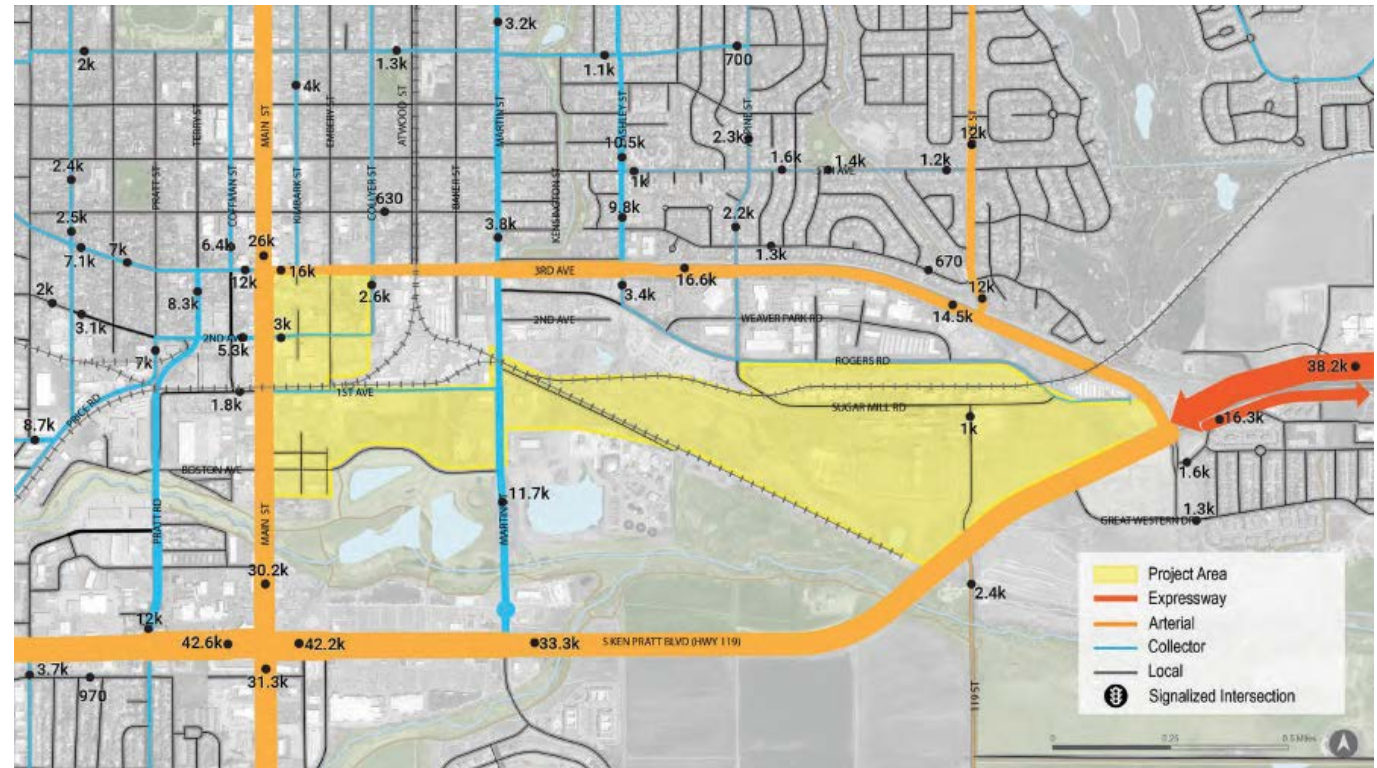
VEHICLE VOLUMES

ISSUES:

- Vehicle volumes are heaviest along Ken Pratt Boulevard and Main Street.
- Ken Pratt Boulevard is a 4-lane, separated highway without opportunity for multi-modal travel.
- No roads cut east-west through site.
- Relatively high volumes on Martin Street, Pace Street.
- RTD Feeder Plan does not cover the area well, but does provide coverage along southeast boundary on 119.
- Most CIP projects focused on the North Character areas.
- Site improvements are limited to project sites, do not extend beyond for area-wide improvements.
- No plans on Great Western Drive.
- Business owners concerned about the lack of pedestrian infrastructure on north side of 2nd Street between Main Street and Emery Boulevard where the existing side path is on southern curb.

OPPORTUNITIES:

- Main Street and Ken Pratt Boulevard provide access to Boulder and Downtown, and will act as primary access roads to site.
- City to have updated roadway and bicycle design standards by June 2022.
- Possibility of removing a lane in each direction once Coffman Street is complete. Intent would be to put in improvements between Boston Avenue and first as part of the transit project.
- Plans to make 3rd Avenue a more complete street.
- New signals at:
 - 1st Avenue/Emery Boulevard – installed
 - 119th / East Ken Pratt Boulevard – not installed yet
 - Costco – not installed yet
- Coffman Street will be the main corridor for BRT service and bicycle access because Main Street is congested and has on-street parking. All bus service will shift to Coffman Street, except 324 which will remain on Main Street.



Source: Sugar Mill Geodatabase



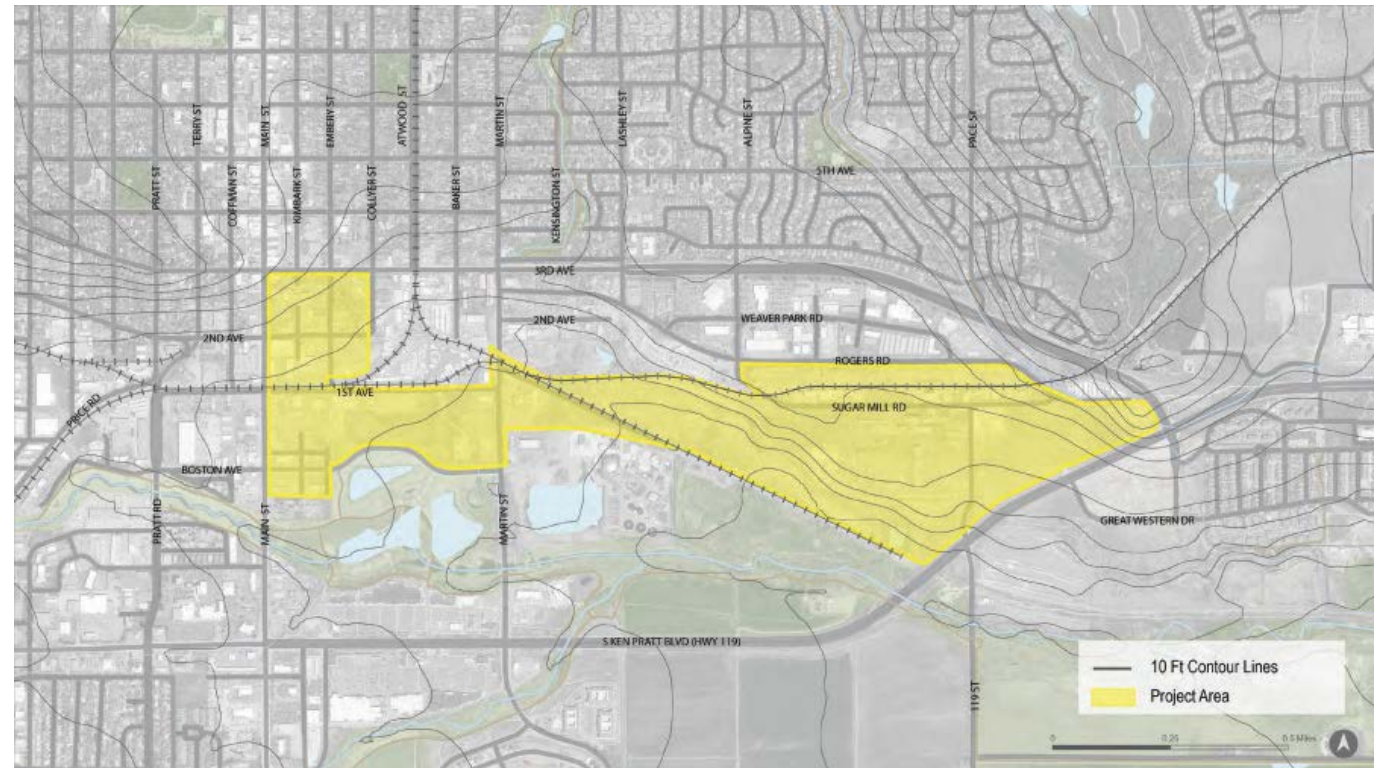
TOPOGRAPHY

ISSUES:

- Relatively steep slope around where Pace Street comes in from the north.
- Relatively steep slope on Ken Pratt Boulevard Boulevard at 3rd Street.
- Both make sight lines and bicycle access a challenge.

OPPORTUNITIES:

- Ebikes can help with grade issues, providing secure storage can encourage their use.



Source: Sugar Mill Geodatabase



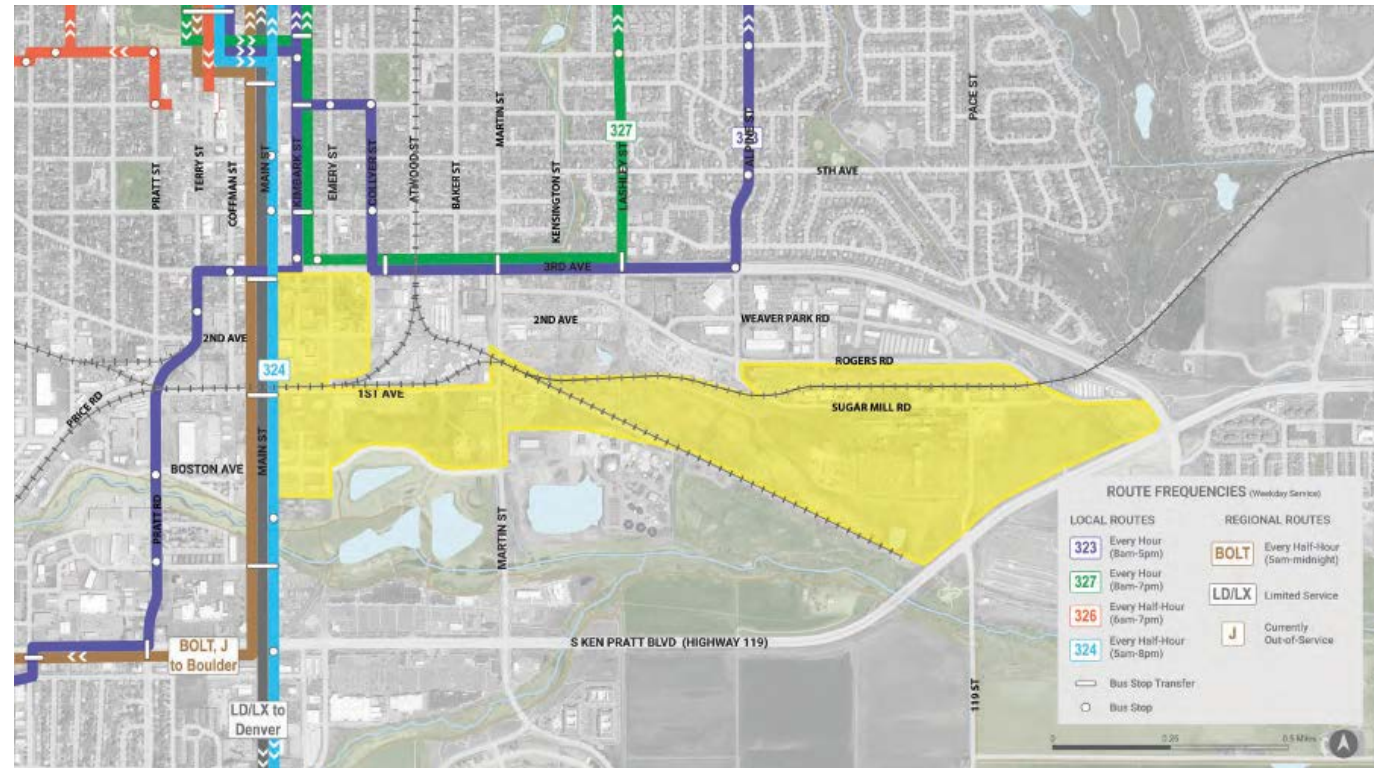
EXISTING TRANSIT: LOCAL

ISSUES:

- Eastern edge of site is almost 2 miles from the new commuter rail station.
- No transit service on the east or southern edges of project site.
- RTD runs all services, except FLEX.

OPPORTUNITIES:

- Strong Downtown transit network that can connect to site (4 buses/hr on Main Street).
- Bring bus service on site with the 323 or 327.
- Private developers can fund a shuttle to the new BRT / Commuter rail stop.



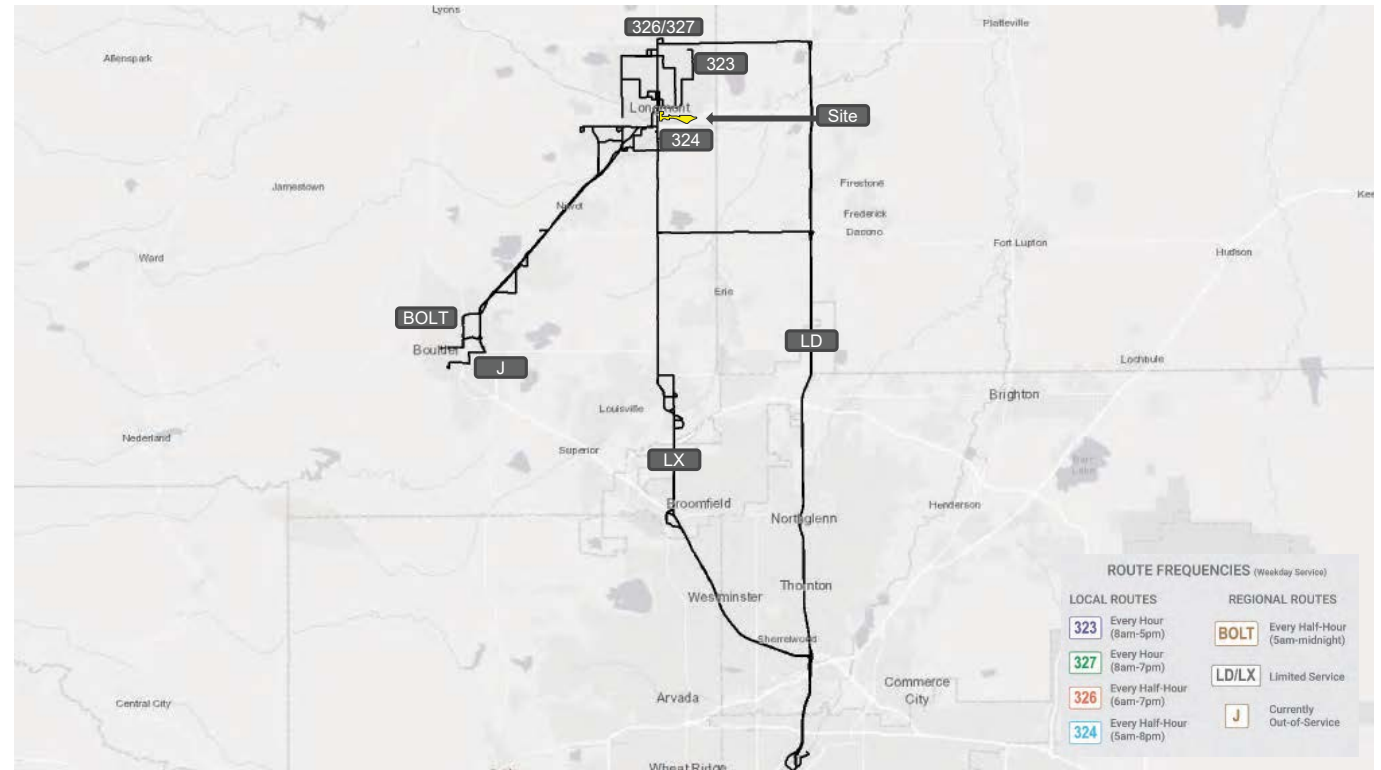
Source: City of Longmont



EXISTING TRANSIT - REGIONAL CONTEXT

OPPORTUNITIES:

Transit services to the site are generally local, but there are regional/commuter connections to Boulder, Denver, and Broomfield.



Source: ESRI, HERE, GARMIN, OpenStreetMap contributors, and the GIS user community



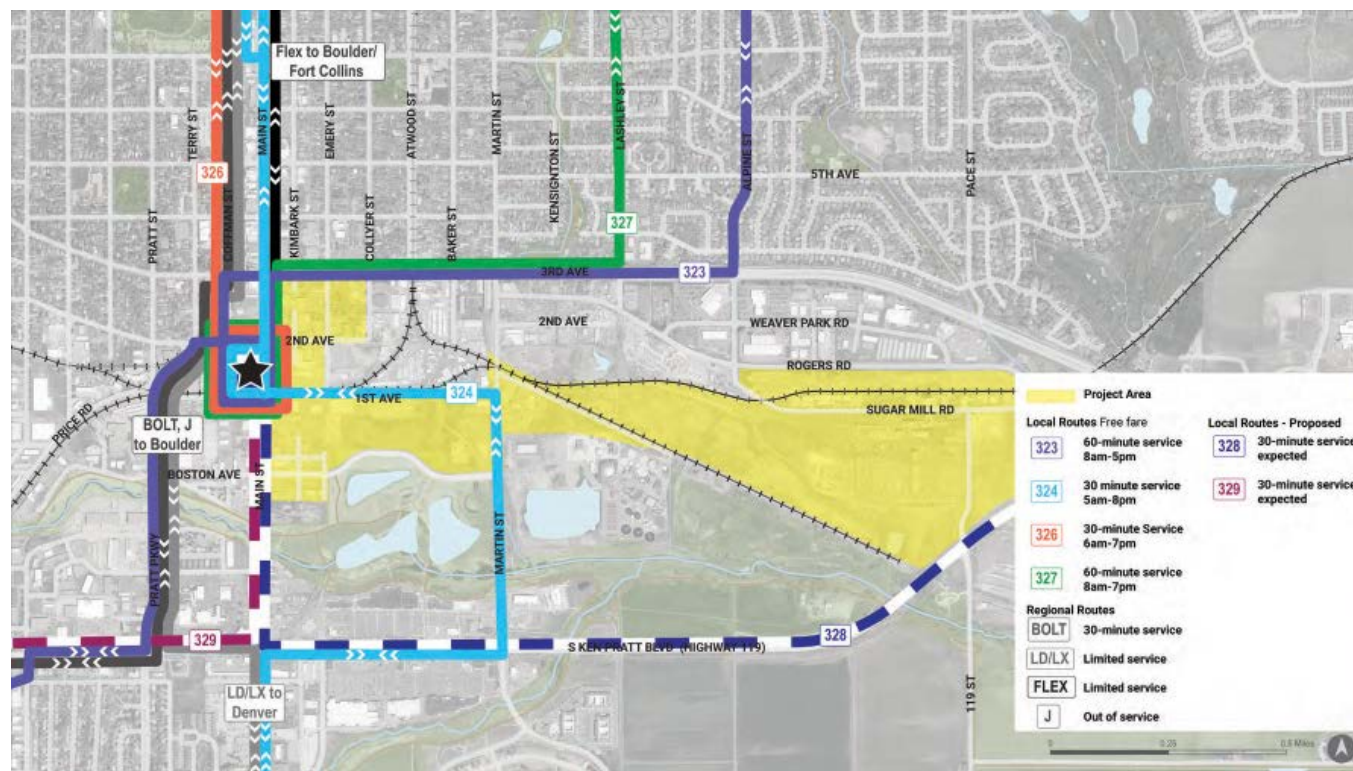
FUTURE TRANSIT - LOCAL

ISSUES:

- Last-mile connections to future BRT station from site are challenging.

OPPORTUNITIES:

- Future BRT/Commuter Rail link to Boulder (see next page)



Source: City of Longmont



FUTURE TRANSIT - REGIONAL

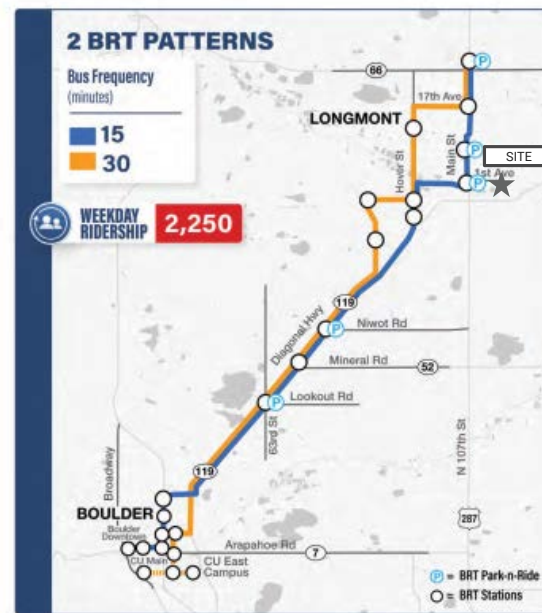
OPPORTUNITIES:

There are two proposals for BRT that would connect to close to the site. These include:

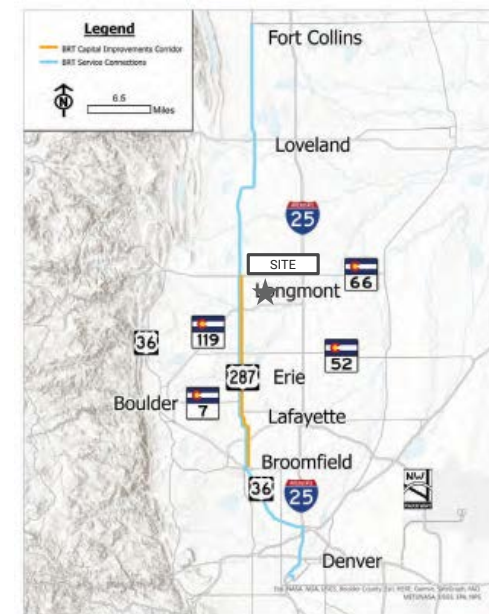
- 119 BRT Study (RTD)
- 287 BRT Study (RTD)

119 BRT Study (RTD)

PROPOSED ROUTES



287 BRT Study (RTD)





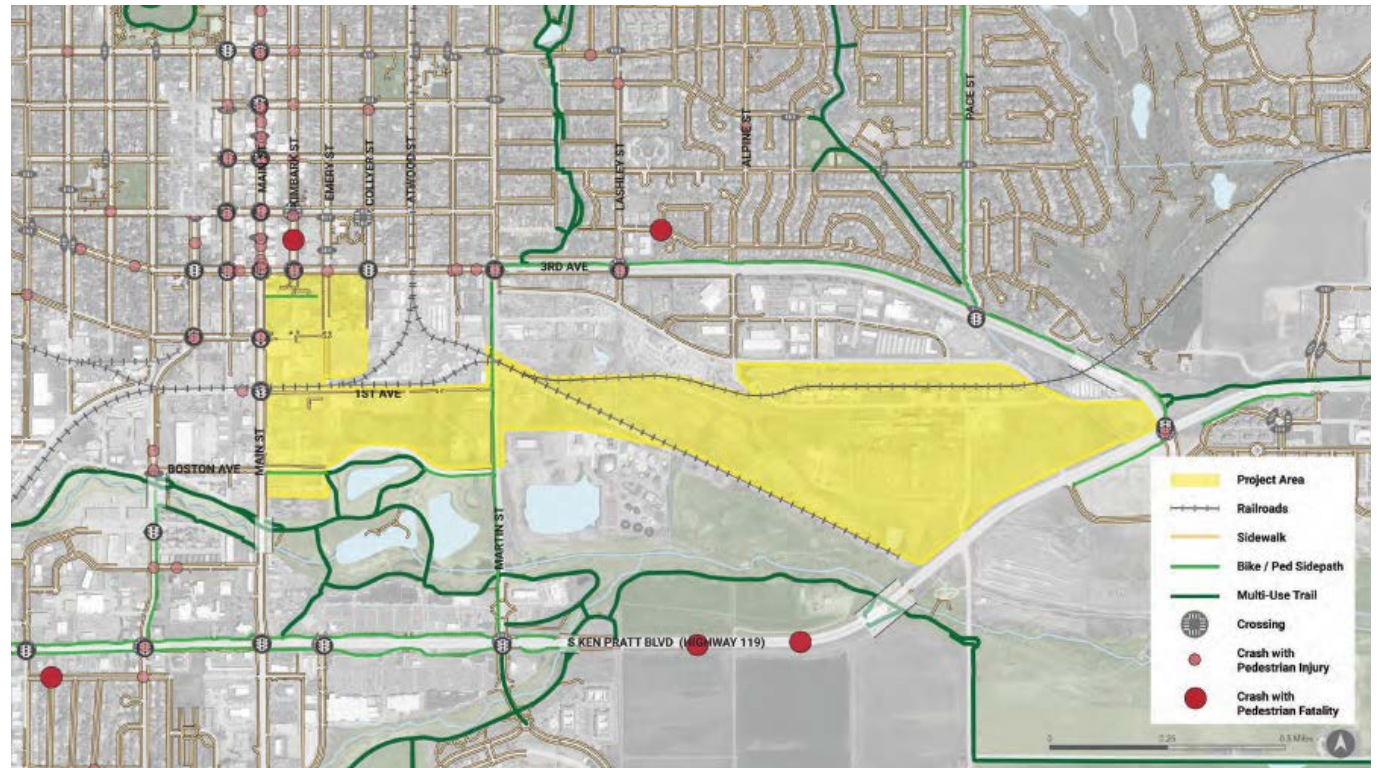
EXISTING PEDESTIAN NETWORK

ISSUES:

- Four pedestrian fatalities near site, and many injuries along Main Street.
- Sidewalk network and crossing opportunities are limited on 3rd Avenue.
- 3rd Avenue is essentially a barrier for people walking from the neighborhood to the north/east to the site.
- Limited crosswalks to bus stops.

OPPORTUNITIES:

- Design roadways and crossings to protect pedestrians .
- Trail network is an asset to the area and provides an additional east-west connection.
- Connection to sidepath on north side of Ken Pratt Boulevard would drastically increase pedestrian access in that area.



Source: Sugar Mill Geodatabase; 1999-2019 crash data



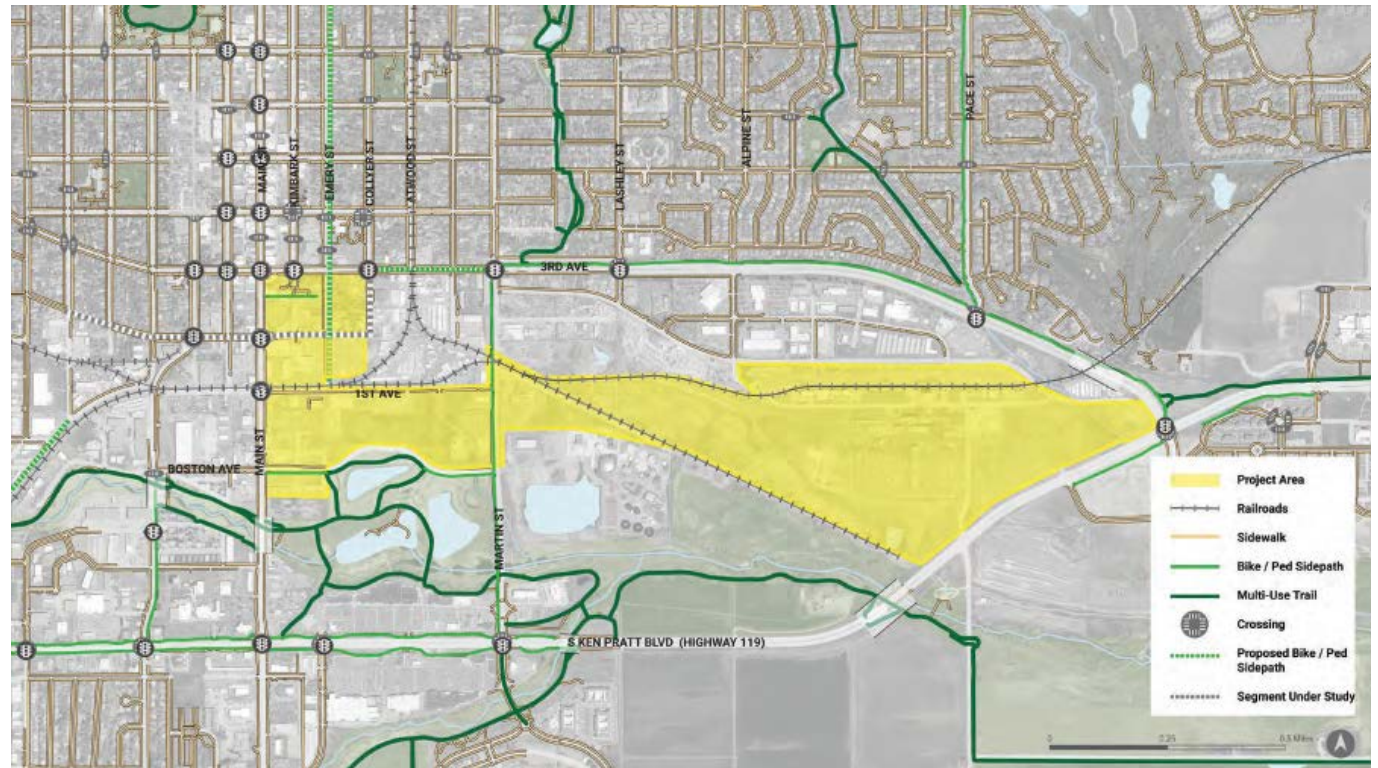
FUTURE PEDESTRIAN NETWORK

ISSUES:

- Site is two miles wide, too long for a realistic walk from the existing commercial corridor along Main Street.

OPPORTUNITIES:

- Carry Downtown Longmont road grid onto the site, particularly near western edge.
- Pedestrian environment along Main Street is comfortable, with wide sidewalks, street trees and furniture, and active storefronts.
- Carry this culture and feel onto site.



Source: Sugar Mill Geodatabase; 1999-2019 crash data



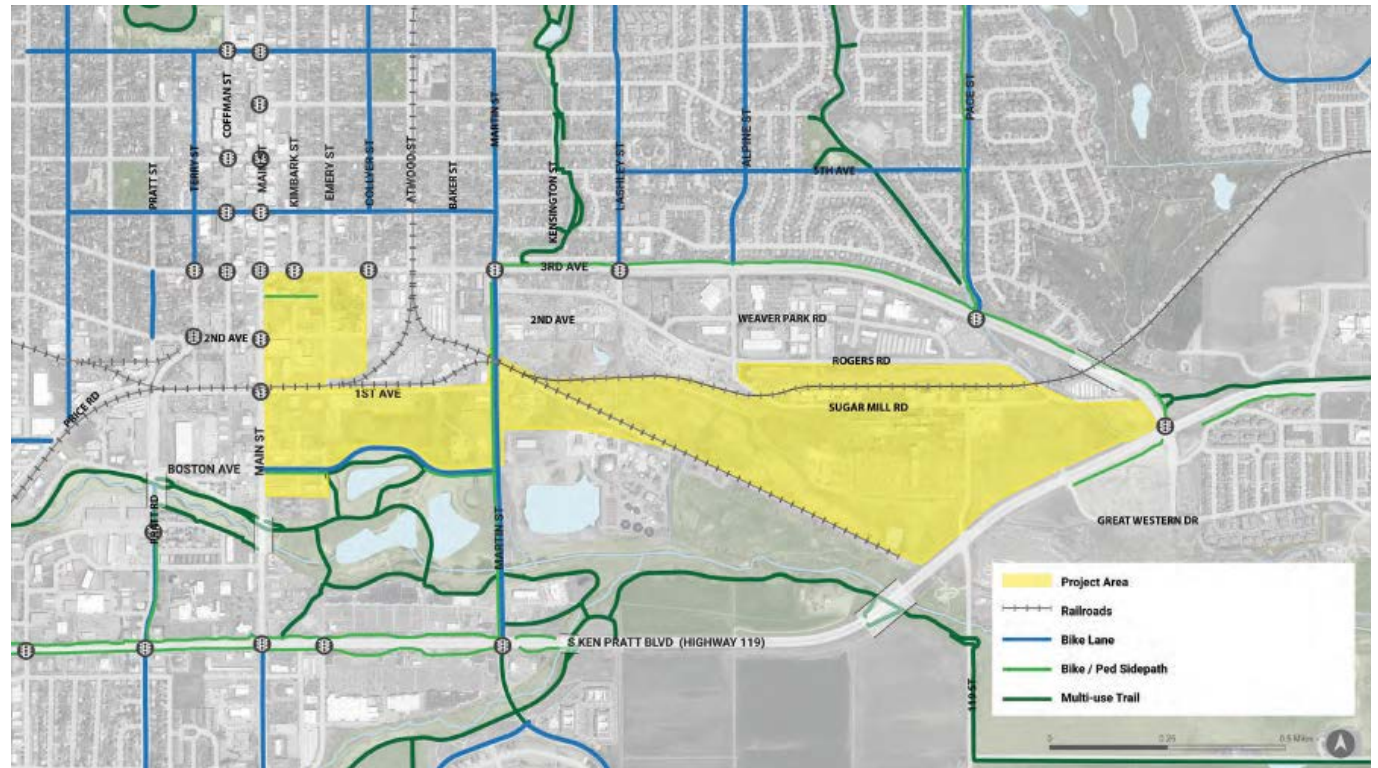
EXISTING BICYCLE NETWORK

ISSUES:

- No access to 3rd Avenue sidepath from site.
- Disconnected sidepath along Ken Pratt Boulevard.
- Pace bikeshare recently discontinued.

OPPORTUNITIES:

- Carry Downtown Longmont road grid onto the site, particularly near western edge.
- Trail network to south provides east-west connectivity.
- Martin Street provides a north-south bicycle link north to downtown and south to trail system.



Source: Multi-use Corridor Plan



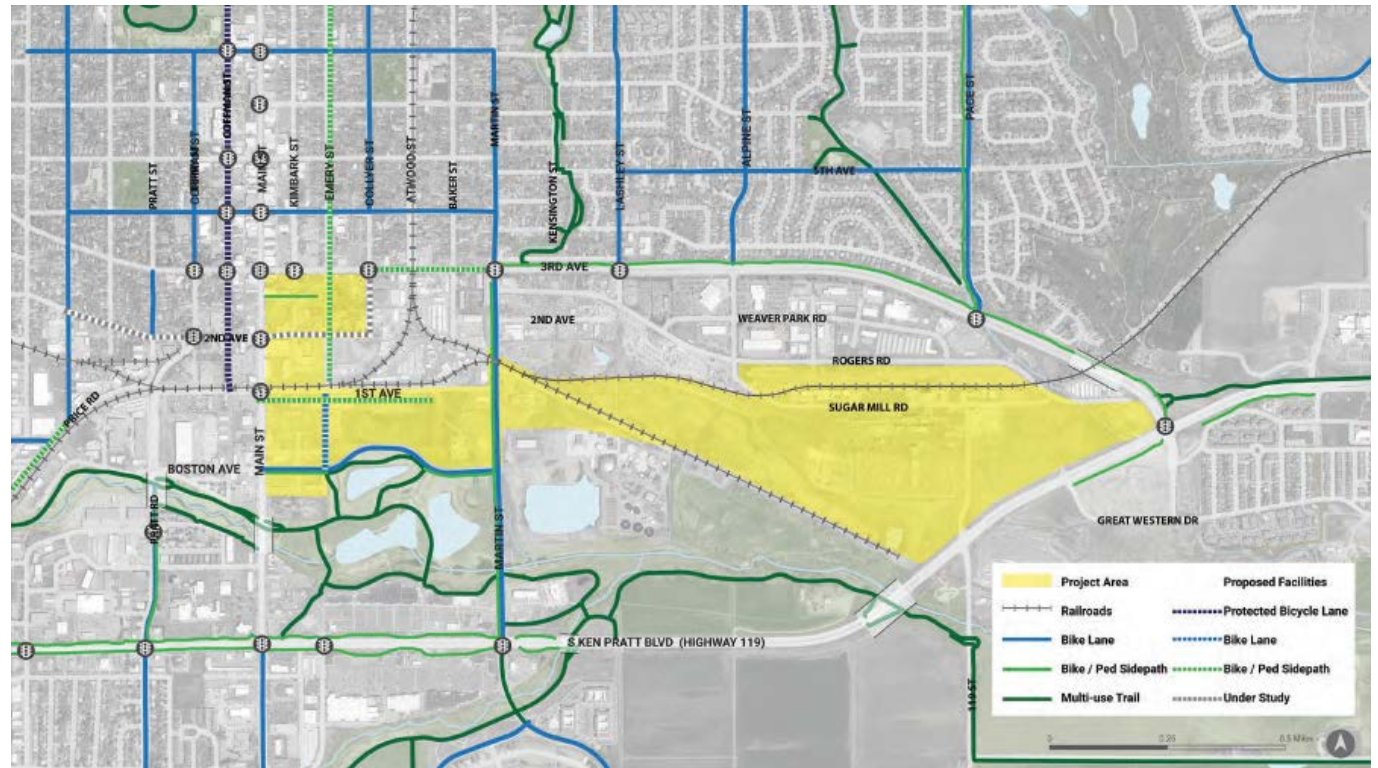
FUTURE BICYCLE NETWORK

ISSUES:

- Multimodal future corridor plan does not extend to site.
- No bicycle connections from future BRT station to site.

OPPORTUNITIES:

- Emery Street connection will connect between site and downtown.



Source: Multi-use Corridor Plan



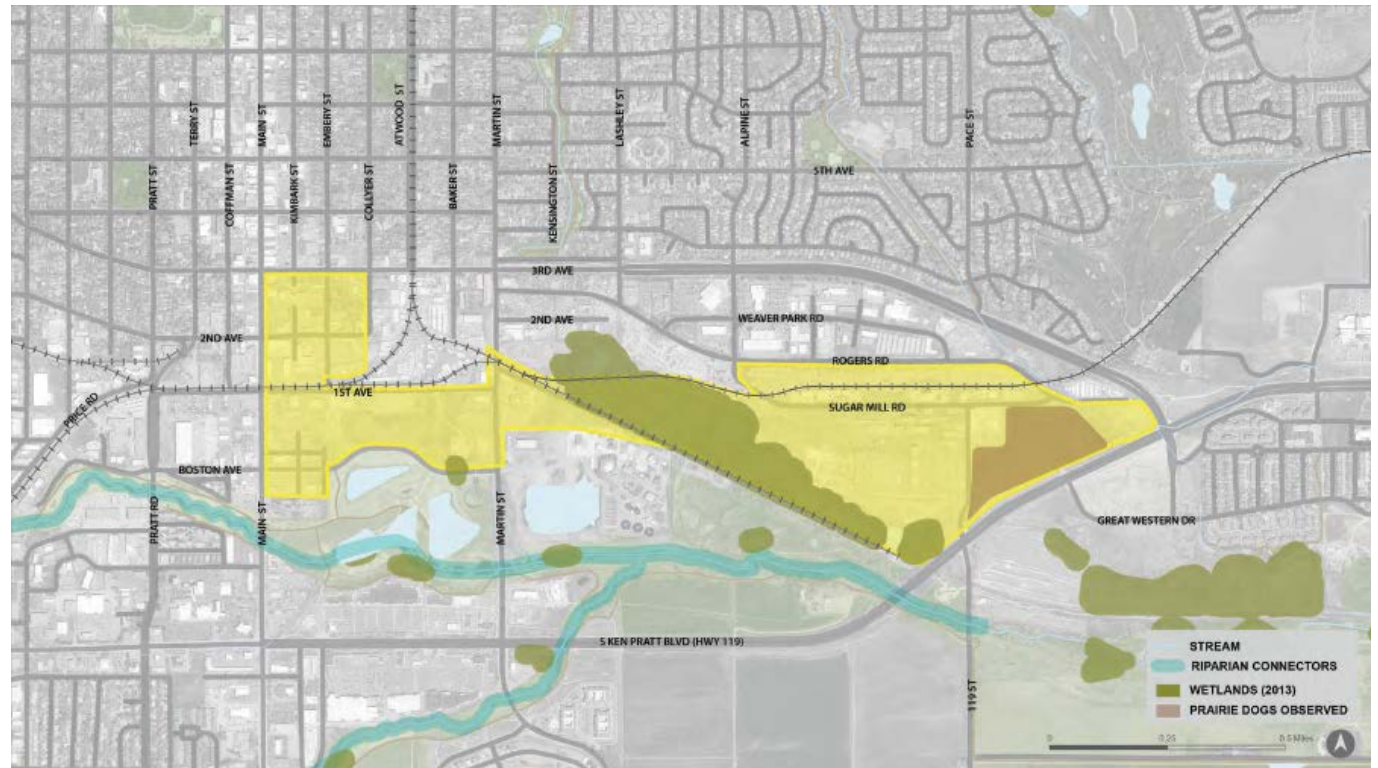
ENVIRONMENTAL RESOURCES

ISSUES:

- The study area is bisected by wetlands that make future development more difficult.
- Permissible development (occupied uses, roadways) within wetland areas is unclear at this time.

OPPORTUNITIES:

- Much of the natural habitat has been protected.
- There are many natural resources near the site making opportunities to interact with nature possible.





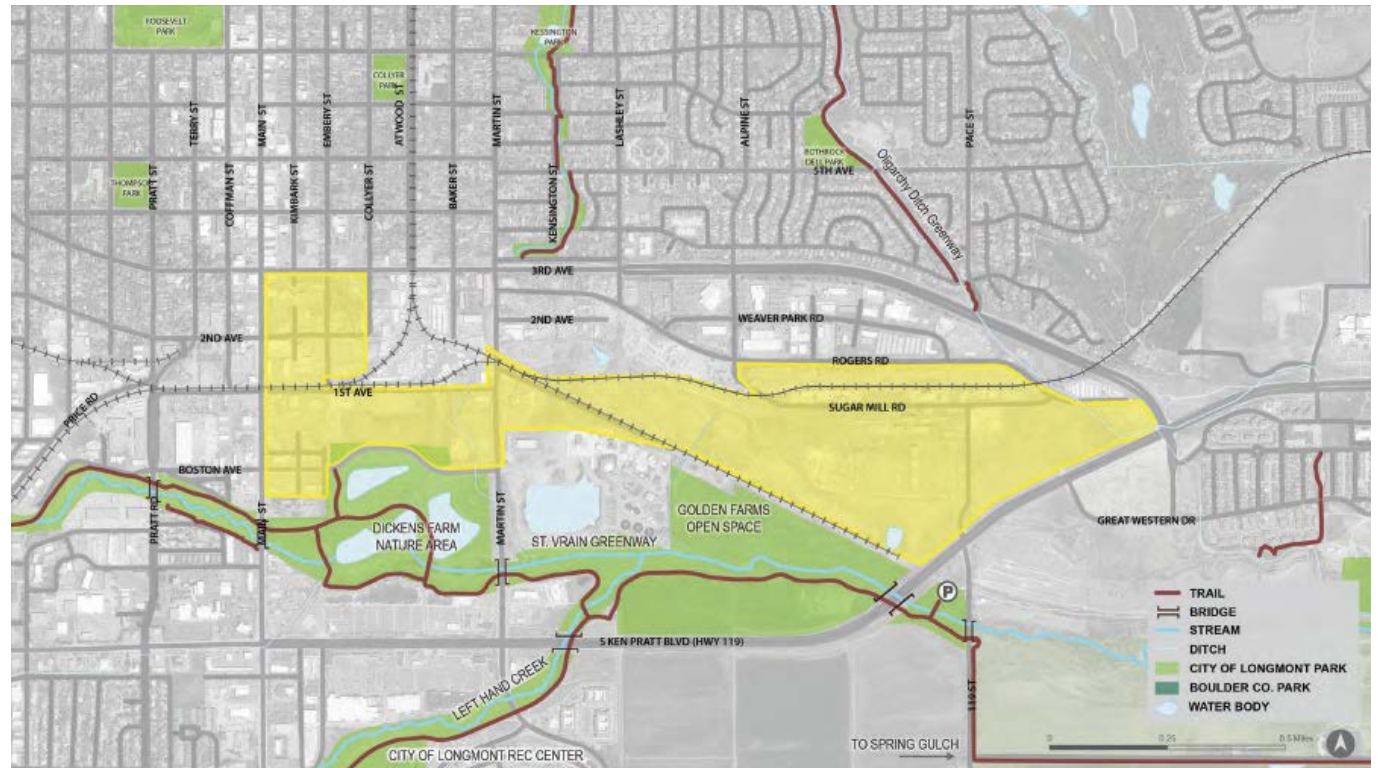
OPEN SPACES

ISSUES:

- There is very little open greenspace located within the study area.
- The eastern portion of the site has limited access to open space, due to the presence of the rail corridor.
- Trails are largely inaccessible to the study area.

OPPORTUNITIES:

- Future trail connections could be used to help get people to open spaces.





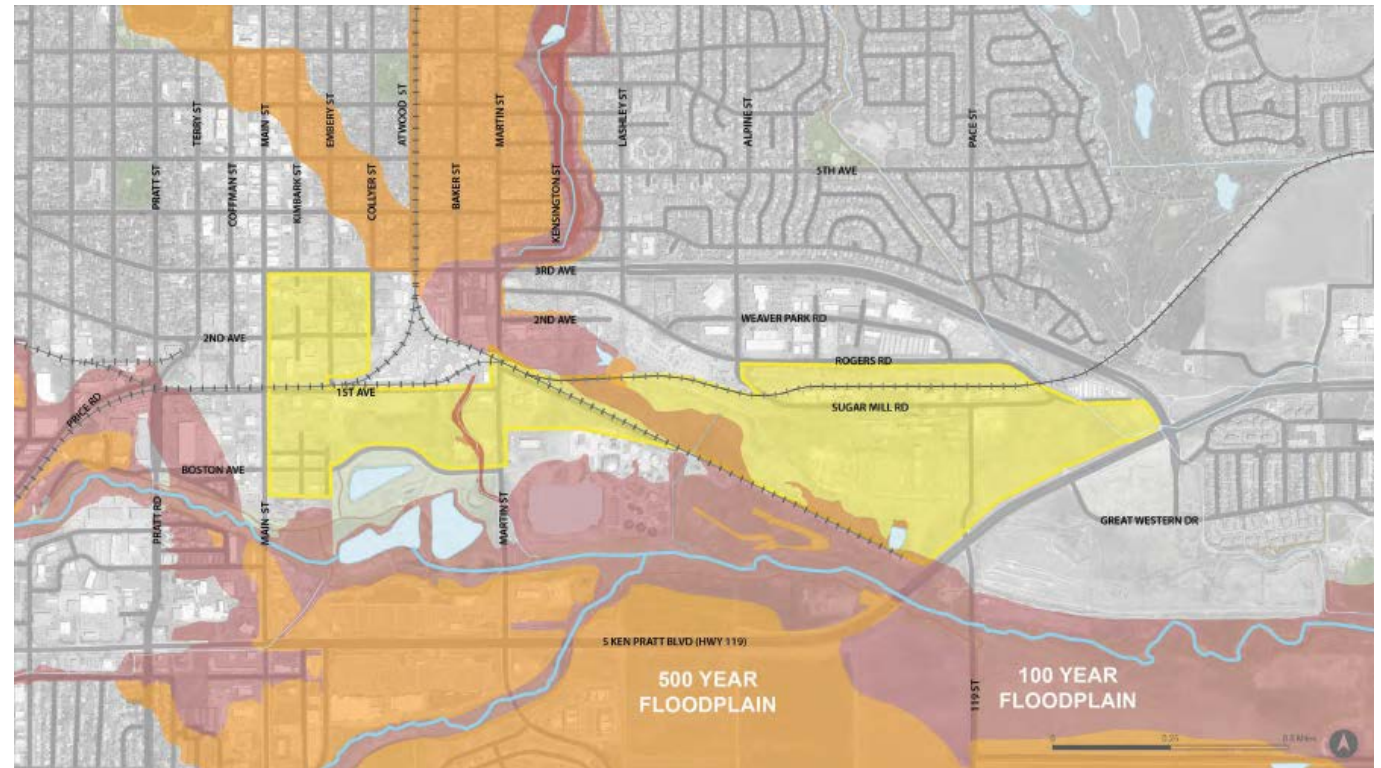
FLOODPLAIN

ISSUES:

- The 100-year floodplain bisects the site making efforts to connect the site more difficult.

OPPORTUNITIES:

- Low Impact Development could be used in areas where the floodplain limits development intensity.





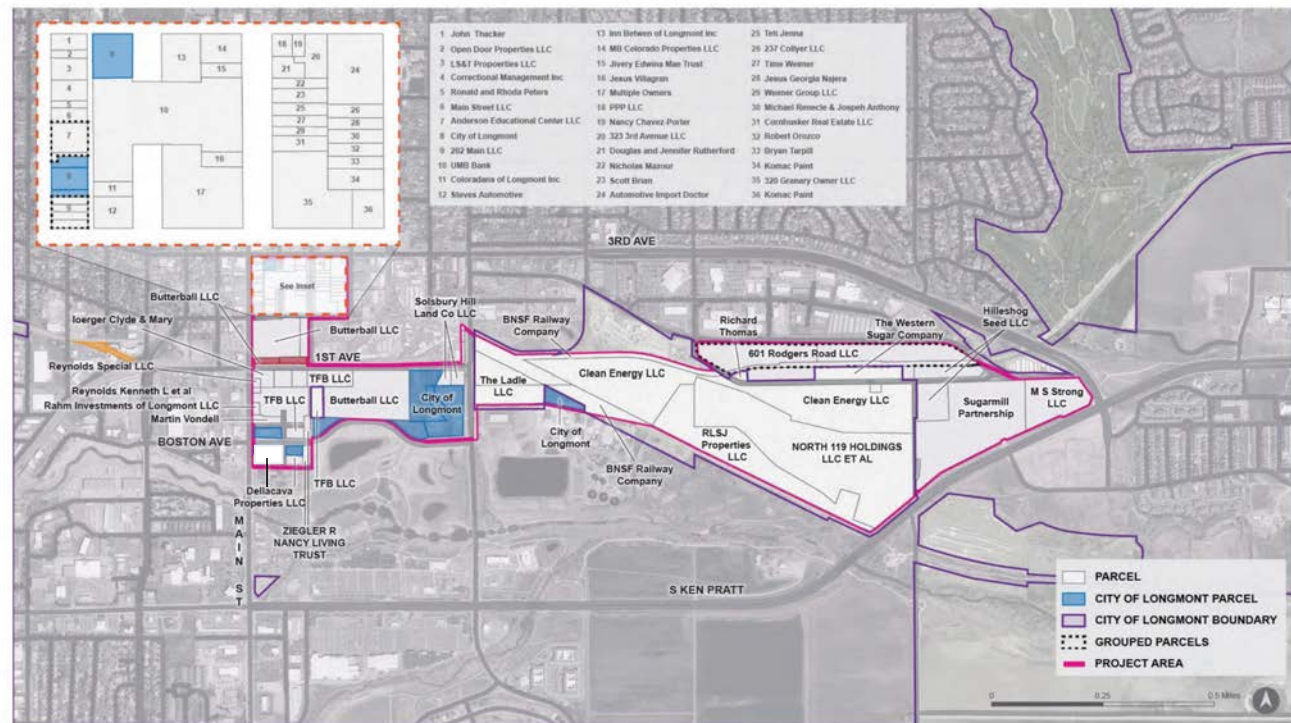
LAND OWNERSHIP

ISSUES:

- The study area has over 60 land owners, presenting challenges for both future development coordination and phasing.

OPPORTUNITIES:

- City owned/controlled sites could be developed strategically to spur future development and attract desired land uses.
- City owned sites could be planned to accompany future corridor and transit improvements.





Summary of Guiding Documents

Arts & Event Center Hotel Feasibility Study (2021)

Scope

This document evaluates the market to support the proposed type of facility, as well as potentially competing facilities, potential partner organizations, and trends. The report also advises on size and configuration, as well as future funding and operational options.

Facility

- Phase I facility 'bare minimum' can be supported now
 - 138K GSF, \$104M
 - Multipurpose hall: 25K SF
 - Auditorium: 1000-1500 seats
 - Meeting rooms/classrooms/rehearsal rooms: 10K SF (8-10 rooms)
 - Back of house: kitchen, IT, office, etc; 2-3 truck bays
- Phase II cannot be supported at this time but likely viable once economic recovery is clearer
 - 216 GSF, \$159M (both figures include Phase I)
 - Recital hall: 500 seats
 - : 10K SF
 - Meeting rooms: 10K SF
 - Back of house
- Additional, future
 - Hotel: 200 key, full-service
 - Amphitheater (outdoor)

Location

- Compared 5 locations, on 12 factors:
 - Boulder Co Fairgrounds
 - Former Plaza Convention Center
 - Building STEAM
 - Quail Campus
 - Old Sugar Mill
- STEAM most favorable: only negative was train noise
- Sugar Mill had 4 pros and 5 cons (3 of which can be expected to mitigated with development): proximity to Main St, Transit Accessibility, Walkability and bikeability, within city limits, and train noise

	A. Boulder County Fairgrounds	B. Former Plaza Convention Center	C. Building STEAM	D. Quail Campus	E. Old Sugar Mill
Proximity to Main Street					
Proximity to I-25					
Transit Accessibility					
Traffic Flow & Parking					
Walkability & Bikeability					
Existing & Future Land Use					
Location, Zone Potential					
Floodplain & Environmental					
Proximity to Existing Hotels					
Land & Facility Missing					
Within City Limits					
Train Noise					
OVERALL					

Legend:

Not Reviewed

Neutral

Favorable

Source: MRG, DPR, Johnson Consulting

Comparative merits of five areas evaluated in the Arts & Event Center Hotel Feasibility Study.

ULI Technical Advisory Panel (TAP) Report: Revitalizing Longmont's Great Western Sugar Mill (2020)

Scope

This report was produced from a two-day virtual charrette in Aug 2020 to brainstorm ideas and financing options for Sugar Mill property; it did not include the STEAM area. Included were 6 contiguous parcels totaling 125 acres; 1/3 of site is within Longmont limits, while the rest is located within unincorporated Boulder County.

Land Use

- Notes Comp Plan (2016) priority to redevelop, preserve and re-use Sugar Mill buildings
- Site could serve as an example of sustainability
- Challenge: large amount of land relative to absorption capacity
- Potential for range of uses: housing, employment, commerce, recreation and culture
 - Concept 1: mixed industrial, employment surrounding historic properties, residential on both sides, retail at 'point' w/ 119 and Ken Pratt; site access via Pace, Alpine and 119
 - Concept 2: similar mix and locations, includes second access off Ken Pratt
 - Concept 3: similar mix and locations, smaller retail, includes second access off Ken Pratt
- Proposes agri-hub concept (need feasibility study): grow and sell produce, also food and drink; retail related to agri-hub; light manufacturing/processing; artisan makerspace
- Residential would be limited
 - Comp plan doesn't allow detached SF housing; has density requirements for this site
 - Suggest MF housing w mixed use; townhomes and cottage style housing on perimeter
 - Also senior housing; spectrum of care options
 - Recommend site master plan with clear sustainability goals
 - District energy approach

Urban Design

- Assets: mountain views, proximity to open space
- Challenge: poor condition of historic structures
- Suggests design guidelines rather than strict standards, to promote sense of place but not damped development



Two of the conceptual land use distributions identified in the ULI TAP report.

Mobility

- Asset: regional transportation access, proximity to regional bike/ped trail
- Challenges: rail tracks, lack of infrastructure, lack of MM connectivity
- Proposed Network
 - Multimodal streets interior to site
 - Bike and ped on East Rogers Rd and Sugar Mill Rd
 - Connections to Pace St
 - Local Connection with Boston Ave at Martin St and Pace St
 - Regional access at East 3rd Ave and Ken Pratt



Longmont Main Street Corridor Plan (2019)

Scope

The plan focuses on the 5-mile long Main St corridor, from SH 66 to Plateau Rd and 1-3 blocks on each side.

Land Use

Identifies 4-character areas: North Main, Midtown, Downtown, South Main

- STEAM and Sugar Mill fall into Downtown (11th Ave to St. Vrain Creek)

Corridor-wide recommendations include:

- Adding residential and mixed-use throughout the corridor
- Preserve historic buildings
- Utilize strategic infill to diversify economic base
- Ensure neighborhood transitions
- Add public parks, plaza and greenspaces
- Create a sense of place

Location

Study Area Falls into the following:

- The Longmont Downtown Development Authority (LDDA) boundary
- The Census Tract 134.01 Opportunity Zone
- The North Metro Enterprise Zone
- The Southeast Urban Renewal Area

Urban Design

Character Area Recommendations

- Extend Main St streetscape N and S
- Extend Main St streetscape to east and west
- 1st & Main a catalyst site

Mobility

Corridor-wide recommendations include:

- Optimize intersection width to reduce crossing distance on main
- Provide additional storage for heavy turn intersections
- Provide queue ump lanes for buses
- New bike facilities, enhanced alleys
- Separate off-street bike facilities from peds with buffers or other treatments
- Enhanced multiuse corridors on 21st Ave, Mountain View Ave and 2nd Ave as key E-W routes

Corridor-wide recommendations include:

- Flexible curb space
- Shared parking structures
- Shared parking



Extents (dark blue/gray) of the Main Street Corridor Plan.

Building STEAM Visioning Process (2019)

Scope

- Year-long engagement process focused on Visualization.
- STEAM area bounded by South Pratt Pkwy (w); St Vrain Creek (s); 1st Ave (n); S Martin St (e)
- Includes 7 Narratives on:
 - Transportation
 - Boston & Main hub for all transportation in Longmont
 - Move state highway traffic to Martin/Hover
 - 5-minute N/S bus service
 - Building Type
 - Hotel, convention, higher ed close to Main St; Res near curved section of Boston
 - Buffer sewer treatment plan
 - Need 'visual candy'
 - Land Use
 - St Vrain crossing is visual gateway into DT
 - Mixed use throughout, not necessarily vertically
 - Parking structures
 - Arts and Culture
 - Attract local, regional, national talent
 - Natural Environment
 - Low impact development (LID)
 - Renewable energy
 - Reused and sustainable materials
 - Education
 - P3s to create innovation district
 - Longmont Education Collaborative for Entrepreneurship and Solutions (ECOLES) rivals Stanford and MIT innovation laboratories

Land Use

Divided into 2 sub areas: https://engage.longmontcolorado.gov/building-steam/news_feed/project-update

- Transit sub-area: west side, Pratt Pkwy to Main St
 - Office and parking structure
 - Transit center
 - MU residential w ground floor retail (TX Donuts)
 - Townhomes
 - Apt/Condos
 - Affordable Housing (standalone project)
 - Office w shared parking structure w high-density res
 - Conference Center and Performing Arts Center (Boston Ave, behind hotel)
 - Hotel



STEAM area as defined by the visioning process (no formal document produced).

- Dickens sub-area: east side, Main St to Martin St
 - Surface parked med density MF
 - Higher education / innovation campus
 - Innovation maker space

Urban Design

Largely silent on urban design

Mobility

Mentions multimodal mobility; bikeable and walkability come up frequently



Longmont Main Street Corridor Plan (2018)

Scope

- City of Longmont document
- Builds on Parks, Recreation, and Trails Master Plan (2014) and Envision Longmont Multi-modal and Comprehensive Plan (2016)
- Planning-level document that provides designs for a network of Enhanced Multi-Use Corridors. Designs minimize impacts to ROW/traffic

Land Use

- Not mentioned, except to note that streets should respond to their context

Urban Design

- Streetscape recommendations

Mobility

- Key corridors include:
 - 3rd Avenue: Collyer Street to Martin Street
 - 21st Avenue: Hover Street to Alpine Street
 - Dry Creek Drive: S Fordham Street to Sidepath at SH 119
 - Emery Street: Mountain View to St. Vrain Greenway
 - Fordam Street: Mountain View Avenue to Dry Creek Drive
 - Gay Street: Highway 66 to Price Road
 - Mountain View Avenue: Airport Road to Deerwood Drive
 - Price Road: Gay Street to Nelson Road
 - Sunset Street: Creekside Drive to Plateau Road
 - 2nd Avenue: Gay Street to Collyer Street
 - Coffman Street (called out separately) - bus lanes



Area covered by the Main Street Corridor Plan.

Downtown Longmont Master Plan of Development (2017 Update)

Scope

- Documents by/for Longmont Downtown Development Authority (LDDA)
- Examines elements/projects/goals within LDDA boundary: roughly Longs Peak Ave (N); just south of 1st Ave (S); Pratt St (W); Emery St/Martin St (E)
- Covers significant portions of STEAM and Sugar Mill
- Six main topics: economics, connectivity, urban design, land use, creative district, leadership

Land Use

- Identifies opportunity development sites in western portion of study area, particularly both sides of Main (2nd to 3rd) and both alleys directly behind this segment
- Promote downtown to tech talent, makers and creative arts/ culinary
- Create additional public plaza and flexible public gathering spaces
- Ensure public spaces are incorporated into redevelopment south of 3rd ave
- Prioritize projects that fill a gap in use mix: housing, modern office, trade/high ed institution, community market hotel, larger event venue (would this be performing arts center?)
- Design large format buildings or parking structures to be easily adapted to other uses in response to future market conditions
- Encourage live-work spaces that support Creative District
- Prioritize redevelopment along and south of 3rd ave and along Coffman St

Urban Design

- Promote an 18-hour/7 day activity zone
- Showcase local creativity and culture
- DT is a certified Colorado Creative District (2014)—enhance existing arts and cultural assets (not create new?)
- Downtown area S of 3rd lacks strong identity
- Comprehensive placemaking to create unified areas
- Rehab and reuse historic buildings
- Enhance gateways to distinguish downtown
- Embrace tech-based wayfinding innovations
- Incorporate green infrastructure

Mobility

- Safety and comfort for all ages
- Intersection enhancements at: Emery/3rd; Emery/2nd; Emery/1st (rail)
- Complete streets enhancements on 2nd (Coffman to Emery); Coffman (entire length in study area); Emery (entire length in study area)



Some sites identified as opportunity sites in the Downtown Longmont Master Plan fall within the current sub-area plan boundary.

- 1st & Main station will be Northwest rail (long-term), consolidated Park & Ride for local/regional bus (near term)
- Sufficient parking now, forecast potential shortfall in 5 years; location/demand mismatch in some locations
 - Development/redevelopment may increase demand; autonomous vehicles (AVs) may reduce demand
- Promote a street grid for new areas, specifically south of 3rd Ave
- Development walk/bike connection to 1st & Main Transit Station, Roosevelt Park, St Vrain Greenway, Dickens Farm Park
- More bicycle infrastructure: lanes, protected lanes, routes, contraflow alleys
- Convenient, attractive transit waiting areas
- Utilize transportation demand management techniques, such as transit use incentives
- Integrate parking structures into redevelopment projects



Envision Longmont: Multimodal & Comprehensive Plan (2016)

Scope

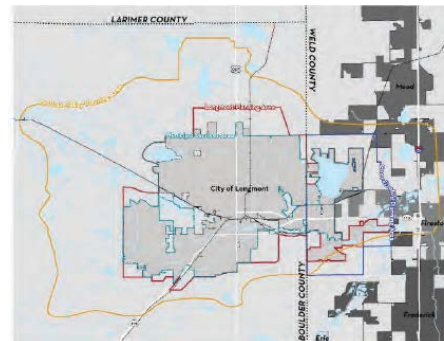
- City of Longmont document
- Serves as both the Comprehensive Plan and Multimodal Transportation Plan)
- Advisory document that outlines Longmont's aspirations for the future, specifically:
 - Provides policy guidance
 - Helps inform residents, property and business owners, developers, and others invested in or considering investing in Longmont's plans for the future
 - Promotes continued coordination and collaboration with neighboring jurisdictions and the many public, private, and nonprofit agencies that play a significant role

Vision and Guiding Principles

- Fundamentals:
 - SUSTAINABLE Longmont, by taking care of the needs of the present generation without compromising the ability to meet the needs of future generations; and
 - RESILIENT Longmont, by increasing the capability of the City and its partners to anticipate risk, limit impact and bounce forward rapidly by adapting and learning in the face of disruptive shocks and stresses.
- Guiding Principles:
 - Livable centers, corridors, and neighborhoods
 - A complete, balanced and connected transportation system
 - Housing, services, amenities, and opportunities for all
 - A safe, healthy, and adaptable community
 - Responsible stewardship of our resources.
 - Job growth and economic vitality through innovation and collaboration.

Land Use

- Sugar Mill/Highway 119 Gateway called out as a focus area with:
 - Vibrant, mixed employment area with high density residential and supporting service
 - Adaptive reuse of mill building as a neighborhood center
 - Pace street extension
 - Ped/Bike linkages to Greenway and Mill Village neighborhood
- Future Land Use Plan (p.98)
 - Study area is Mixed Use Employment (jobs, housing, supporting retail)
- 1-2: Update development review processes to make additional types possible



Three-Tier Planning Area map included in Envision Longmont.

- 1-5: Explore changes to the Southeast Longmont urban renewal plan
- 1-8: Detailed planning for Sugar Mill area
- 1-13: Develop TOD at 1st and Main
- 1-20: Work with property owners/developers in focus areas
- 3: Strategies to support housing, services, amenities, and opportunities for all
- 5-5: Revise Land Development Code provision relatively to local food production

Urban Design

- 4-8: Update public improvements design standards to implement resilient design performance standards

Mobility

- Future multimodal connections (p.124)
 - Rapid transit connecting E-W through site
 - Pace Street connection
- 2-1: Adjust codes to encourage transit supportive development where feasible
- 2-2: Create a prioritized hierarchy of bus stops
- 2-3: First/last mile connections
- 2-4: Connected bike/ped facilities citywide
- 2-8: Activity participate in BRT planning
- 2-11: Pilot projects
- 2-13: Study the feasibility of additional grade separated railway crossings specifically for emergency vehicles
- 2-15: Work with transit providers to expand local transit service within Longmont

Parks, Recreation & Trails Master Plan (2014)

Scope

Addresses project priorities, costs and funding and stewardship for existing and future facilities. Includes inventory of existing facilities and identifies priorities/locations to expand system.

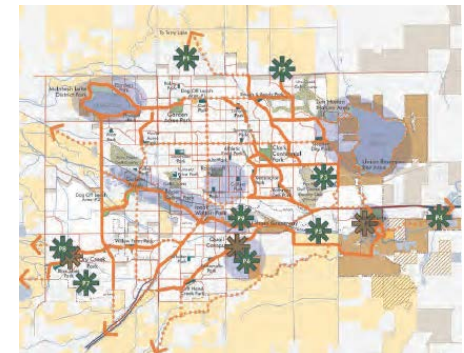
Goals

- Renew: reinvest in existing assets
- Complete: fill service gaps and create new opportunities
- Connect: integrate active living; update definition of what a trail can be
- Distinguish: highlight unique natural, historical, cultural elements
- Design large format buildings or parking structures to be easily adapted to other uses in response to future market conditions
- Sustain: protect long-term system health (financial, maintenance, operations, planning & design)

Future Facilities

Map 6 shows System Concept

- In STEAM/Sugar Mill area:
 - Shows Future Park Development in STEAM area
 - Described as Future District Park, Dickens Farm Park Site; master plan for the site approved in July 2013 (this has since been completed)
 - Shows enhanced recreation connection immediately east of Main St
 - Nothing shown in Sugar Mill area



Since it is currently not within the City of Longmont, the Sugar Mill portion of the sub-area plan study area does not have any specific facilities shown in Parks, Recreation & Trails Master plan.



Longmont 1st & Main Station Transit & Revitalization Plan (June 2012)

Scope

Develop a vision for the 1st & Main Station area, including market analysis of economic development potential to be used for short- and long-term land use concepts. Provides development concept scenarios for 0-5 years, 5-10 years, 10-25 years.

Note that this is an older policy document, much of which has been superseded by more recent STEAM (2019) and Performing Arts Feasibility Study (2021) efforts.

Land Use

- Guiding Principles:
 - Respond to changing demographics – more ped-oriented communities with mixed-use and low maintenance
 - Ped-oriented and bike-friendly
 - People-intensive uses – housing, retail, dining, employment, recreation, community facilities, educational facilities, hotels
 - Compactness
 - Manage parking and access – shared parking, center of blocks
 - Be successful without transit – specifically mentions preliminary use as bus facility before rail comes
- 3 zoning areas: Transit core, commercial core, mixed use transition area
- Transit facilities center w of First Ave, on north and south sides of roadway
- Shows arts district wrapping transit to the north and west (portion of transit N of First Ave); this recommendation largely superseded by more current arts and convention study (2021)
- Includes arts incubator and live/work uses

Urban Design

- Gateway bridges on Main and Pratt Pkwy
- Shows linear green strip on both sides of historic depot, north of First Ave and east of Main St
- Main St streetscape extended south to river
- Zero lot lines on Main St
- Continue DT grid system



The 1st & Main Revitalization Plan identifies the majority of the sub-area plan study area as an Urban Renewal Area.

Mobility

- Discusses proposed transit service concepts: main street service, city circulator, modified crosstown route, flexible fixed route, checkpoint service
- Recommends focusing on Main St as main transit spine
- Focus on east-west connector (Route 323)
- Enhanced Call n Ride



Initial Stakeholder Input

Developer / Property Owner Meeting #1 (East)

Scope

The project team, led by the City and Stantec, met with property owners, developers, and prospective developers to better understand current and future plans. This first meeting generally discussed the portion of the site east of Martin St and around the historic Sugar Mill property. This working meeting was intended specifically to discuss development plans for this area, rather than a more general community meeting. Comments ranged across a variety of issues and are included below in an informal format.

Stakeholder Attendees

- Fairfield Residential project:
 - Lenne Viehlehr, Norris Design. Planning and Landscape for Fairfield Residential
 - Lee Busse, Fairfield Residential. 23 acres at NE Corner of Ken Pratt. Horizontal mixed use
- Remington Homes - St Charles Town project:
 - Guillaume Pouchot, _____, Remington Homes
 - Jesse Adkins, Chris Shears: SA+R - St Charles
 - Charles Woolley II St Charles Town Company affordable housing, MF
- Golden Family
 - Barb Brunk, Resource Conservation Partners (north of RR trucks and reps Golden Family), DGM Ilc
 - Regge Golden, DGM Ilc
 - Drew Golden, DGM Ilc
- Additional Affiliations
 - Andy Welch, Sun Construction
 - Brad Slott, Civil Engineer
 - Alisa Romero, KTG. Architects
 - Wendel Pickett, Frontier, 16 acres Rogers Road to RR tracks. Do flex industrial and workforce housing
 - Brad Slott, Kimley Horn
 - Torry Willis

Stakeholder Comments

Market Comments

Unlimited demand for affordable housing in the region. Building at 60\$ Median Housing. Also preservationists and would like to repurpose buildings into commercial uses.

Overall Comments

City has done extensive study of the RR crossing issues. There is a lot of information for this area.

Costco site is only a portion of the PUD. The remainder is for mining and reclamation. There is a deeded access to _____ and N 119th Street and Sugar Mill Road. Railroad crossing that gets you to the city's open space on the south side of the tracks. Couple connections or integrated connectivity. The mining area will make beautiful open space once reclaimed. This piece of ground could contribute to the open space network for the neighborhood to allow density on other pieces and use this as community open space.

What is the status of the Fairfield development? Answer: We are preparing the prelim plat (had pre-hearing) to go in early January. Developer requested the plat to align curb cuts.

Really helpful to see all of the plans. Before we get into questions, is there a timeline or goals for when the subarea is meant to be formalized?

Planning on working through Q1 and having something to Council Q2. Hope to have it wrapped up during second quarter of 2022 with Council.

When will extraction happen? How does this time with the development and potential to have residential next to mining operations. Answer: In a perfect world, it would be done in the next 5 years. Mining is only extraction (not processing). Planned access for gravel mining is south on 119 and east.

Any and all of the land could be available for open space. There was an idea to allow increased density on the other parcels and utilize the open space in the reclamation area. Everyone wouldn't have to provide their on site open space. That is the best use for this, adjacent to city open space.

Are these wetlands of value? Answer: Not high quality wetlands. Would require 1:1 replacement, but nothing more. Army Corps has evaluated.

There are two flood plains. One from the north, Spring Gulch. This is where the flood plain. As part of this process, we would like to remedy the floodplain from Spring Gulch.

Mining and reclamation is a great way to resolve this floodplain issue.

Residential developers don't get the same treatment as removing gravel.

Not allowed to touch the wetlands. Have gotten a determination that they are not allowed to touch them. Does mining have different rights than developers.

Not exempt from 404 permit. The mining has always been a solution/opportunity to clean up that piece of ground. They are old wash basins covered with cattails. Because it is such a low quality, mining can get 404 permits and replace at 1:1 on site as part of the reclamation. Off site credits are hard to manage and expensive to manage. Cheaper to go pay someone else's bank than find your own. Will go into a deed restricted area through a conservation easement and open space - available to the public. Not buildable. Difference is that the wetlands will be removed, restored and replaced as part of mining process.

What are Challenges and Opportunities

Concerned about aligning access to adjacent properties. Coordination with CDOT for primary entrance. Is there a desire to take the local street and make it a wider arterial street that borders the sugar mill north?

Does the city anticipate street improvements (Sugar Mill and 119)? That would influence our planning. What is the feasibility of extending Lashley south to western bow of bowtie? Curb cut alignment with Fairfield.

Goes back to what other person said and the realistic mixed use demand. How do we confidently as a group figure out what is viable. A lot of things have changed in the last 5 years. How this relates to access. What is realistic?

Reuse of historic buildings could help with placemaking for the entire community. Whatever connectivity you can make to those buildings as a core placemaking for the region, that would be great. Looking at saving the big tin shed. What we don't want to have is roads dead ending into buildings. Need a mobility grid that works for everyone.

Appreciate this. Open to any and all conversations about connectivity and connecting roads. Have done a lot of this in a vacuum. None of this is solidified or in place. Have worked with the City for a year, but didn't have info on adjacent parcels. More than happy to discuss how to connect, and make roads work.

Pretty new at this too. Through City's efforts working through the 2nd phase of environmental studies to figure out what it will cost to do environmental

mitigation on the buildings and soil issues that may be encountered. Won't be finished until early part of 2022. This sets up the feasibility of saving buildings, remediation, funding, financing, etc. Very committed to making that happen.

There is a Phase 2 on the property that was shared with ULI. Charlie - we have all the historic stuff.

Blank old metal building that is a blank canvas. Paint the roof. It is the gateway to Longmont. Could be an extraordinary piece of art.

Building is structurally sound. Exterior needs help. Could be an incredible gathering space. That itself could be a special spot for people to come and gather. Farmer's market, etc. Could work well with restoration and preservation that Charlie talked about to the north.

There are a lot of agricultural facilities there and the ULI Tap talked about it. What are your thoughts on that?

Will look at exploring. Don't know how many more seed labs or ag uses are out there that could be attracted and create synergy of ag business. Willing to explore.

Rogers Road could be a more transitional piece with 5-10,000 sf buildings for industrial/flex space to complement what we are doing.

Renewable energy? Solar panels on metal roof.

Did 2 proposals. One is biowaste for 8 acres (did not move forward). One was a recycler of electronic goods.

Last 3 projects did solar and use green building standards. Clean energy LLC has tried to see how to make boilers into biomass generator

One opportunity that may be possible is energy that is produced off of wastewater treatment plant. Generates heat from processing. Can developments utilize heat? Agricultural grow houses? Small scale version is Stanley Marketplace. Greenhouses grow produce used by restaurants.

If you are interested in solar, sustainability or community solar, reach out to her to coordinate with LPC. Has to be uniform credit with net metered. Don't have community solar billing at this point. Community solar is hard.



Timeline for Development

NW corner of intersection. Land will be a for sale piece of ground. Most likely multi family with some mixed use. Probably going to come forward quickly. Want to coordinate with Fairfield.

601 Rogers Road - At least 10 years out.

Long road to go with environmental issues on both projects. Will continue to work with staff. Want to get through annexation in early 2022. Looking for shared opportunities on roads, utilities, open space, detention, etc.

Started out as SF developer, but shifting towards more attainable housing. Don't want to create the same product over and over. Mostly fee simple projects. Have partnered in Broomfield and other communities to be affordable at 80%. Remain very flexible. Don't see this as a single family project

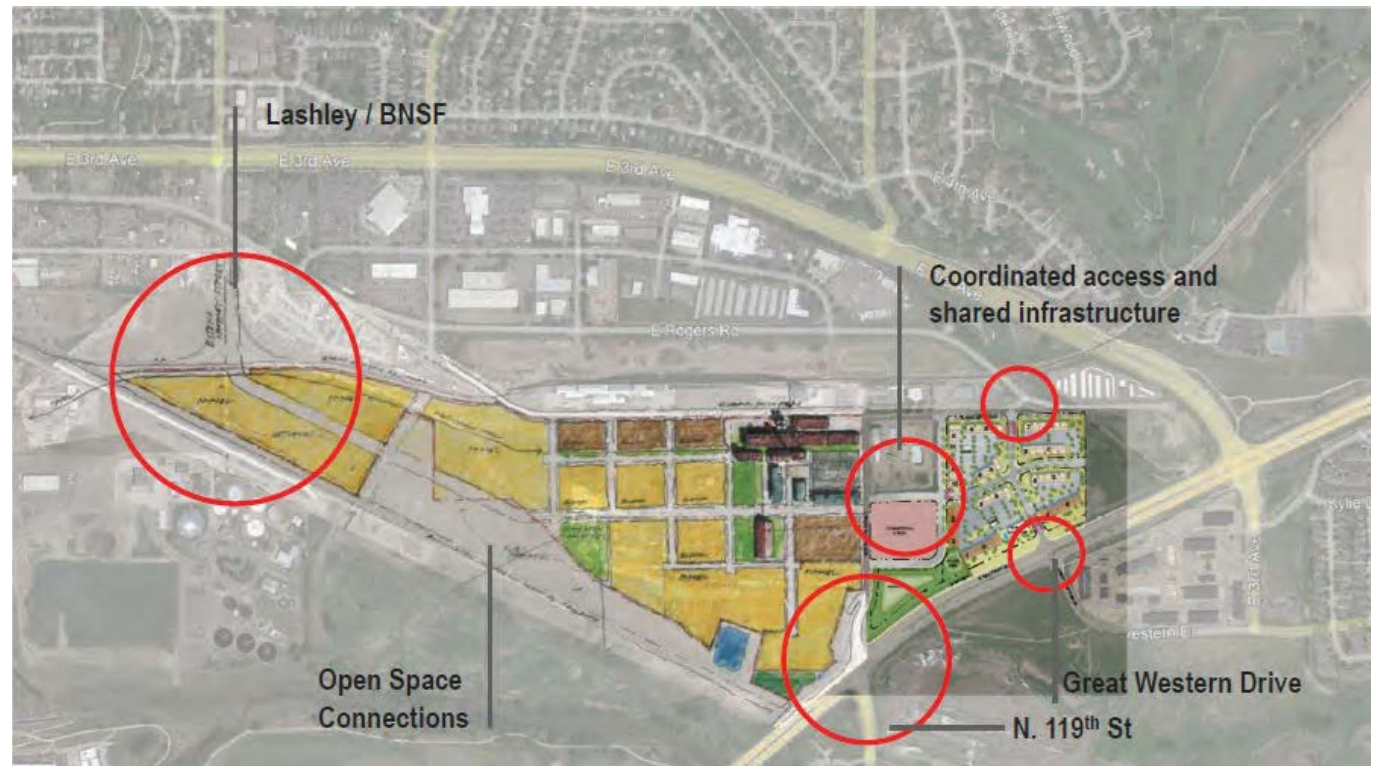
Won't be the end developer. Interest is exclusively for rent multi family. With some mix. Comparable to what Lee is seeing.

What does the acronym STEAM stand for? Focus on sugar mill building and elevate "arts" pieces of it. Answer: Science Technology Engineering Arts Math/Makers

Example <https://www.youtube.com/watch?v=e0C1kRtVYAY>

RTD - is looking for 5-10 acre sites west of Lashley alignment.

Excited to look at this as a gateway project comprehensively Getting to critical juncture where looking at architecture. Overall conceptual direction conversations with the city. Does it play off Sugar Mill or something completely different? Will start to share info with neighbors. Gateway project. Have looked at Agritopia in Gilbert AZ. How could we take best practices from projects and apply them?



Source: A composite graphic made from plans submitted by St. Charles Company, Remington Homes and Fairfield



Developer / Property Owner Meeting #2 (West)

Scope

The project team, led by the City and Stantec, met with property owners, developers, and prospective developers in order to better understand current and future plans. An agenda was sent out questions for consideration as we get closer prior to this event.

While there was a public outreach component of this planning effort and involvement of other project stakeholders and community members, this was a working meeting to specifically discuss development plans for this area (rather than a more general community meeting).

Stakeholder Attendees

- City of Longmont:
 - Glen Van Nimwegen
 - Erin Fosdick
 - Hannah Mulroy, City of Longmont Environmental
 - Todd Fleming, Business Analyst
 - Tony Chacon, Exec Director for Urban Renewal
 - Tyler Stamey, Traffic Engineer
 - Ben Ortiz
 - Kimberly McKee
- Stantec / Trestle Strategy:
 - Rhonda Bell
 - Nancy Locke
 - Marine Siohan
 - David Starnes
- Property Owners / Developers / Stakeholders
 - Erica and Jon Eggers: TFB LLC, with Jim Cobb, 1st and Main and 10 South Main
 - JD Parker
 - Jim Cobb: owns 11.5 acres south of 1st & Main. Had developer under contract, walked away after due diligence because of high costs and ways land would be divided up.
 - Michael Tignanelli with Global Pacific: developer who was under contract with Jim Cobb
 - Brian Bair: bought Butterball property in 2015 (2nd & Emery, 1st & Main). Almost 12 acres. Redeveloping area, finished 230 units, commercial, 61 units under construction right now. Starting additional construction as well. Wibby brewery

- included.
- Andrew Dye: Corecivic, 236 Main St, purchased whole portfolio in 2016-2017. Helping folks getting out of prison to get them back on their feet.

Stakeholder Comments

Overall Comments

What are you contemplating for the area?

Need for townhomes in the market; we are contemplating multiple apartment buildings, some of which are mixed use, market rate apartments. We're seeing declining growth around the nation in housing, migration plays a big role in Colorado. We were evaluating this for apartments primarily and also some townhomes.

Just a note that we have over 60 different property owners in the study area (45 in the STEAM area)

Challenges

On your 2019 diagram, my property is showing as multifamily. I'm a little confused about whether you are planning for multifamily housing or STEAM uses.

This 2019 diagram is not meant to guide any development and is completely flexible and open for discussion.

This drawing was not done as part as this planning effort, it was done as a brief visioning process done in 2019.

One of the first things the City should look is improving the infrastructure. When you load one development with all the initial infrastructure costs, it stops the development. Maybe the City could make all the improvements with a fee system on the different properties.

It's a great comment and a very real concern, and we can look at that.

In 2013 we started developing in RiNo; none of that would have happened if the City hadn't put in infrastructure along Brighton. It is a real concern, how to deal with infrastructure, who pays for it, how does it get allocated over time, etc. I'd like to have a roadmap for that piece, and then be less

prescriptive on what the verticals are. I am almost 100% leased at Emery, Main St is a little more challenging.

Is there a physical difference between how the spaces are arranged on Main St and at Emery?

No, they are very similar.

How to mitigate taxes is another challenge that's in the mix. Costs and land costs are high. All of these things are limiting the ability to develop.

Mitigation of flood plain seems to be dragging on.

How possible do you see providing for the missing middle is?

We're doing it. Institutional capital is pretty siloed making true mixed use, mixed type developments very difficult to implement. Have to solve for larger properties.

All the requirements from the City don't pencil down on the proforma. No large institutional capital group will fund public projects for zero or negative returns.

The hope with a sub area plan is to get a cohesive vision for the area and recommendations to the City.

Are there items specifically that you would like to see implemented going forward?

Predictability and consistency is key. Planning timeline is very slow compared to development timeline. It would be nice to create a plan so everybody can develop into it. I love the concept of the whole town connecting down to the river, we would need to get that set in stone so to speak.

Flexibility within the zoning is very important as things can change over time. Being able to work out changes with staff is very important. Put the framework in place and let the market do the rest.

Are there still things specifically that we ought to look at in the code?

I'm not a code expert, but I would say things like parking regulations, mix of uses, etc.

Housing Gaps in Longmont

We're really interested in attainable housing.

All of it is needed. But it's hard to start thinking about this when costs and fees are prohibitive. We don't do affordable housing at all, so we approach projects on the market rate standpoint.

Missing middle and density is a little bit of a cat and mouse issue.

Amenities

Are there any kind of amenities you would see in the area?

I haven't read the performing art center report.

Our group purchased a property in Canada with a performing art center, but it wasn't profitable. A more flexible event space may be easier to manage.

Public services more data driven. We've always thought that the proximity of downtown is the community amenity. Connecting STEAM and Sugar Mill to the Old Town would be a great win where everything is walkable and accessible.

What about connecting south to the green space?

Next Steps

Send out the slides. Please share any additional thoughts on any of the questions or anything else, we'd like to continue the conversation.

We'll invite you to conversations we'll have with the DDA as well.



Initial Analysis

Key Findings - Land Use

Opportunities

1. **Urban Living + Natural Environment** – STEAM is situated in a way that it can leverage and physically connect to both existing open space assets (St. Vrain River corridor, Dickens Farm Open Space) and the already successful Main Street corridor.
2. **Residential Spectrum** – as a blank slate (albeit brownfield), the Sugar Mill area offers opportunity to offer a wide variety of housing options, including those that might not fit into the downtown block structure already defined in the STEAM area.
3. **Cultural Venue** – the sub-area plan is tasked with identifying potential locations for a performing arts/cultural venue as described in the 2021 study. In the STEAM area, this type of venue may be able to create an indoor-outdoor setting that is physical connected to existing open spaces. In the Sugar Mill area, this type of venue could have a different character and capitalize on character and style of the sugar mill or the tin shed.
4. **Transition** – new uses between Rogers Rd and Sugar Mill Rd should transition between new development around the Sugar Mill and existing stable uses north of Rogers Rd.

Challenges

1. **Competition** – many of the innovation/maker uses suggested for STEAM could also be good fits (and to a degree already exist) in the Riverfront area. The sub-area plan should provide flexibility to accommodate these uses, but not be diminished if these uses go elsewhere in near- and/or long-term.
2. **Competition, pt 2** – finding a land use mix that complements but doesn't compete/cannibalize existing main street uses north of the study area. As signified by the STEAM concept, there is the potential to define the area as distinct from but an extension of existing Main Street.
3. **Residential Density and Variety** - Push the envelope on the density when current development thinking – expressed in pre-app plans and developer discussion – trends toward lower density and more traditional products.
4. **Wetland** – acceptability of development or roadways on or through identified wetland areas is unclear at this time.
5. **Transit Oriented Community (TOC)** – STEAM's location offers direct transit adjacency to near-term (bus) and long-term (rail) transit improvements at the 1st and Main area; due to the long-term nature of rail, some improvements in this area may be delayed by waiting for full transit build-out, which carries some degree of uncertainty. STEAM should strive to provide a feeling of 'completeness' even before/if rail comes.

Key Findings - Urban Design

Opportunities

1. **Historic Aspect** – the historic Sugar Mill building offers a unique character and history that new development can build upon and celebrate
2. **Gateway** – both STEAM and Sugar Mill offer own unique opportunities to act as gateway. The Sugar Mill area can act as a visual gateway to Longmont from the East. The STEAM area can act as a visual – or even physical - gateway for Main Street corridor as a whole.
3. **Identity and view** – the site's high point at the northeast portion of the Sugar Mill area provides an outstanding view of the Sugar Mill buildings and smokestacks, as well as the Rocky Mountains beyond. This view can be celebrated as part of the place-identity of Longmont in general and the Sugar Mill area in particular.
4. **Community Enthusiasm** – initial community outreach has shown a largely positive view toward developing the Sugar Mill area, and toward adaptive re-use/historic preservation of the Sugar Mill buildings themselves.

Challenges

1. **Historic Aspect** – finding a use that is financially viable for some or all of the historic Sugar Mill buildings, in terms of market demand and the considerable costs associated with mitigation/restoration is anticipated to be challenging. This aspect of redevelopment will ultimately not be solved by the sub-area plan, although it can discuss a framework of uses that would be compatible with uses imagined for the adjacent land.

Key Findings - Mobility

Opportunities

1. **3rd Ave Complete Street** - pedestrian and bicycle upgrades to 3rd Ave, including traffic calming measures, could benefit existing and future development, in terms of speed and aesthetics.
2. **Pace Connection** – although grades are not challenging for a vehicular connection, a non-motorized connection at this location would be less fraught, and would have the added advantage of underlining non-motorized connectivity as a priority in its own right, rather than an add-on to vehicular systems.

Challenges

1. **Limited options for connectivity** - both portions of the study area offer limited options for connectivity to the surrounding city fabric, due to the river corridor (both), railroad (both), existing land uses (Sugar Mill).
2. **Railroad crossings** – additional crossings unlikely; freight's position on relocation or even re-alignment of existing crossings is unknown. Grade-separated crossing may be possible, but would require significant ramping to reach required clearance; such crossings would bear significant expense.



Draft Project Goals

To start a conversation on community vision and priorities, the project team created draft project goals in five categories. The goals were put on EnvisionLongmont to ask community stakeholders what these goals mean to them. Draft goals and community input are reflected below.

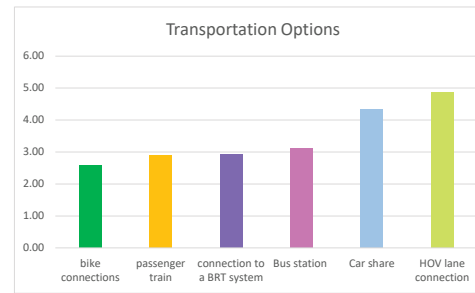
Housing: Encourage meaningful housing options with a range of housing types and price points.

Input Question: What type of housing do you think would be appropriate in the STEAM + Sugar Mill area?



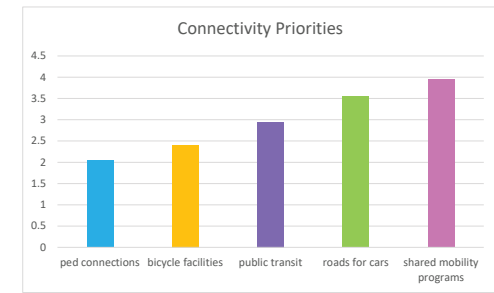
Transportation: Provide for both regional and local transportation connections.

Input Question: What facilities could the site integrate to encourage regional connections? (Rank from least (1) to most (6) important)



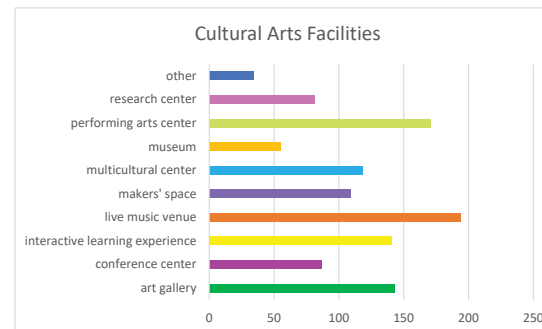
Development: Ensure connectivity between parcels and to the broader Longmont community.

Input Question: How should the study area connect to the broader Longmont community? (Rank from least (1) to most (5) important)



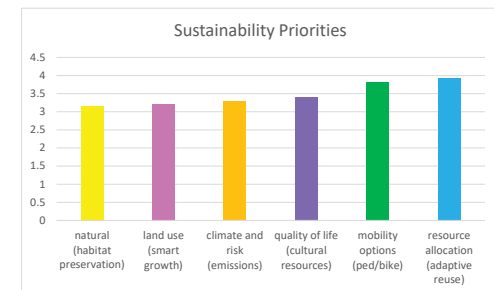
Community: Incentivize and encourage cultural arts facilities and community hubs for creativity and innovation.

Input Question: What kind of facilities or uses do you envision as being the most appropriate for a cultural hub or center for creativity and innovation in the study area?



Sustainability: Focus long term efforts on sustainable building and development practices.

Input Question: What does sustainability mean to you as it relates to development in this area? (Rank from least (1) to most (6) important)

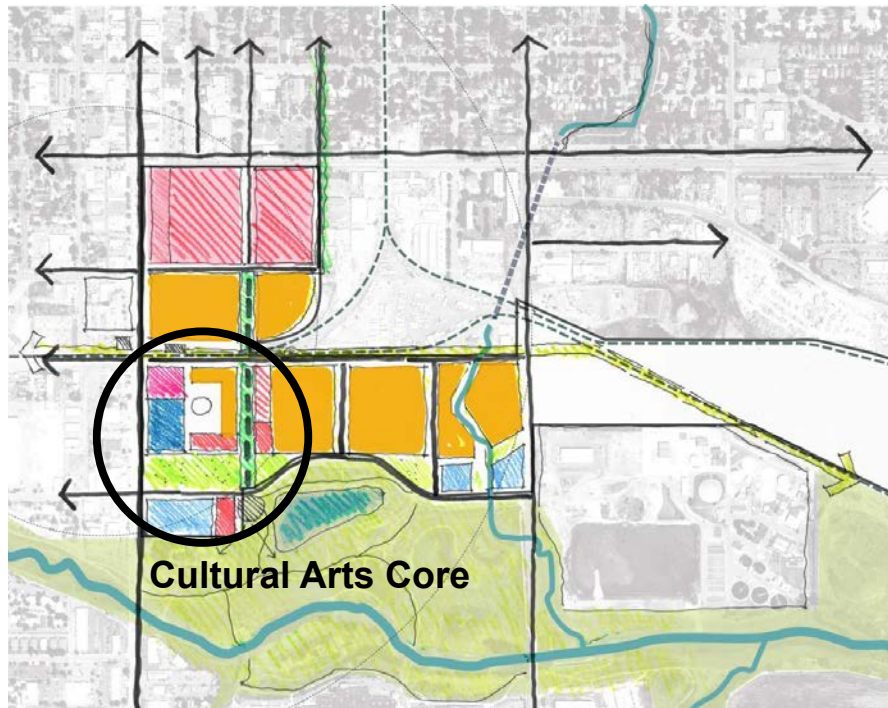


5.2 PLAN ALTERNATIVES

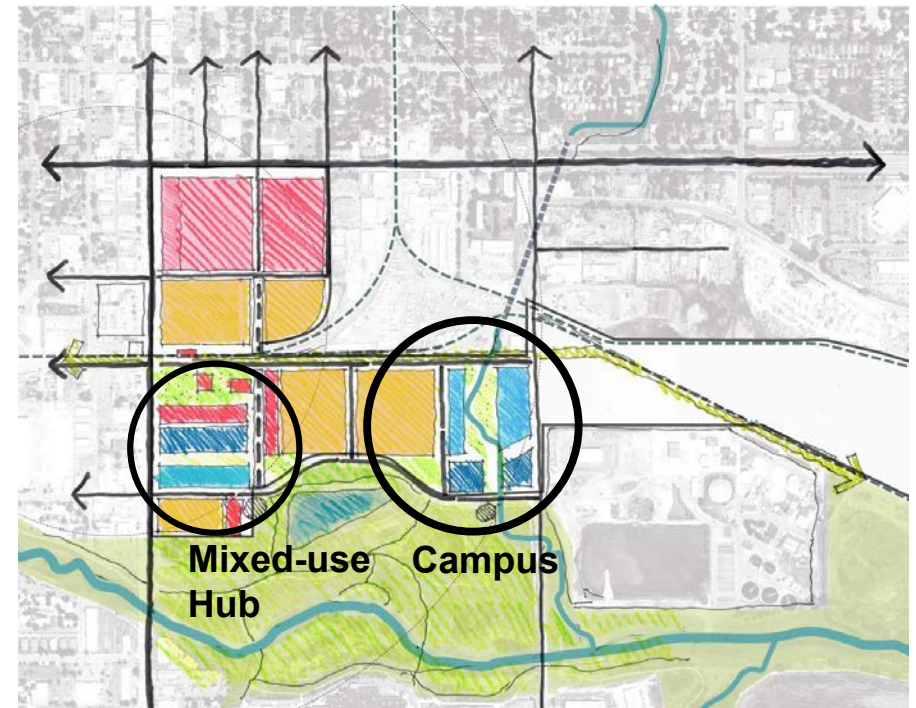


STEAM: TWO DISTRICT ALTERNATIVES

CULTURAL DISTRICT + HIGH DENSITY HOUSING



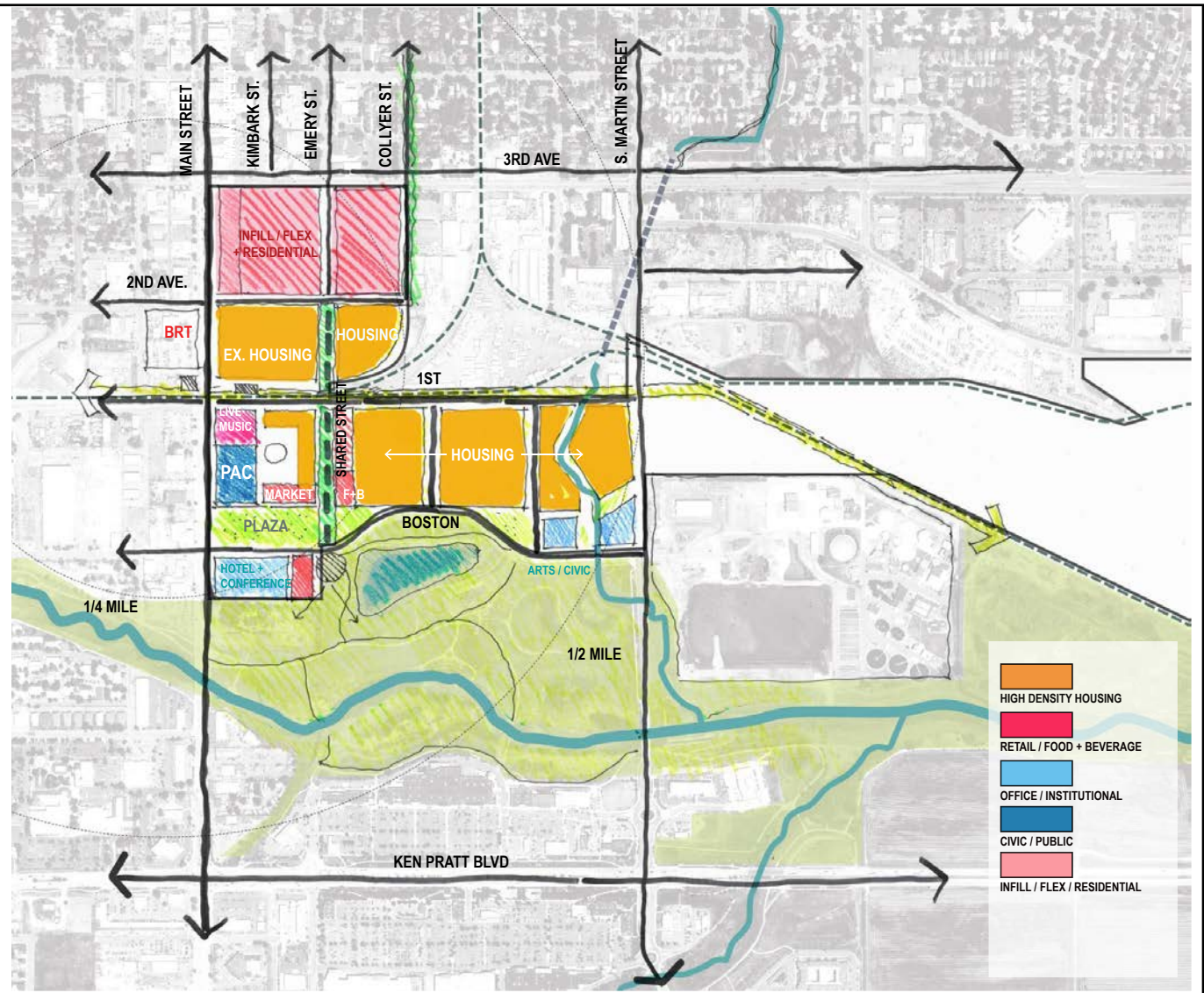
MIXED - USE OFFICE / JOBS / CAMPUS





STEAM CULTURAL DISTRICT:

NEW PERFORMING ARTS CENTER
 LIVE MUSIC VENUE
 MUSIC + ARTS FACILITIES
 HOTEL + CONFERENCE FACILITY
 EVENTS SPACE (WEDDINGS, ETC)
 HIGH DENSITY HOUSING + LIVE-WORK
 MARKET / FOOD HALL + PLAZA
 SHARED STREET
 PARK GATEWAY



APPENDICES



STEAM: MIXED-USE OFFICE / JOBS / CAMPUS:

HIGH DENSITY OFFICE / MIXED USE

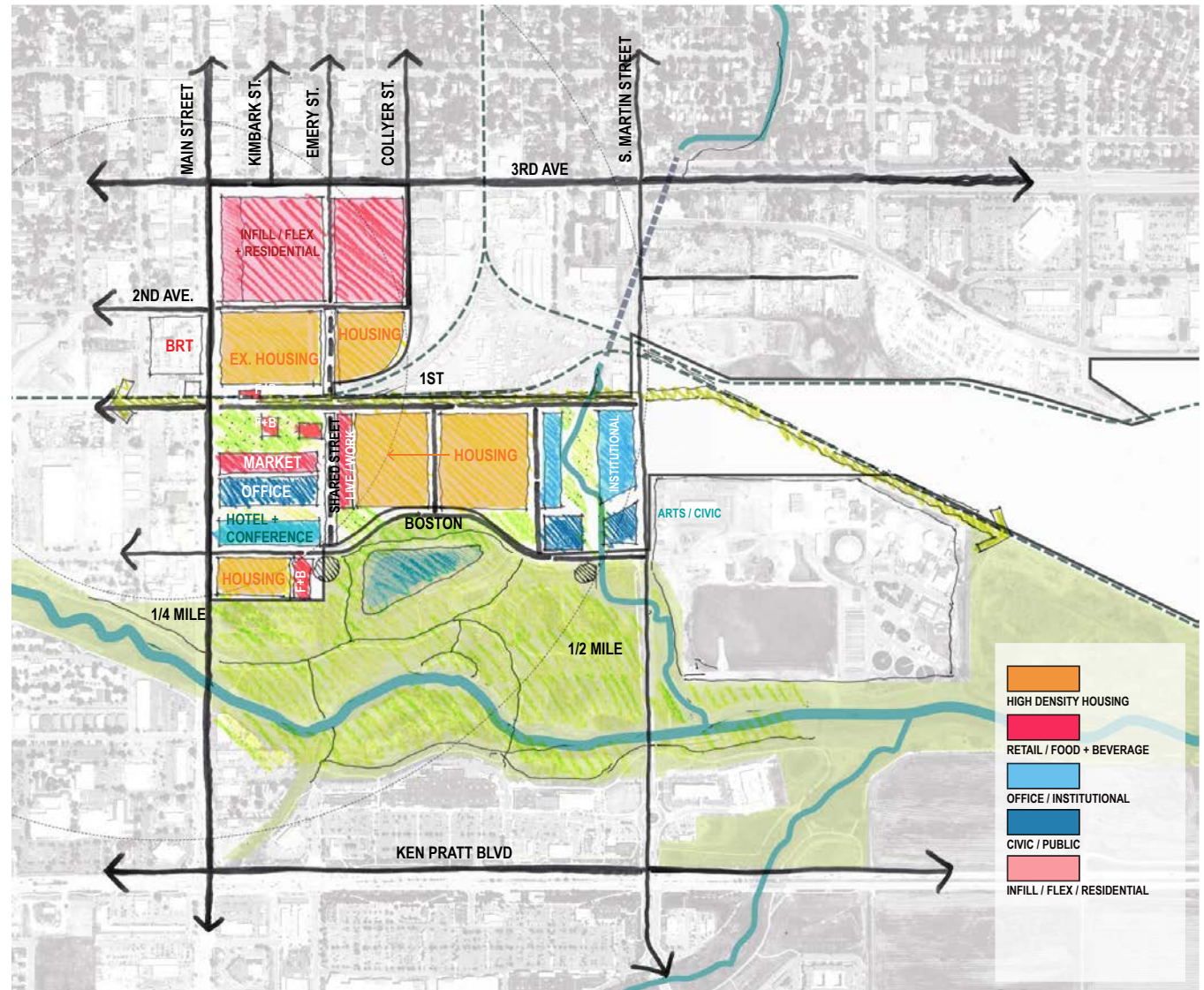
HOTEL + CONFERENCE CENTER

LIVE / WORK RESIDENTIAL

HIGH DENSITY HOUSING @ PARK

PUBLIC FACILITY ON PARK

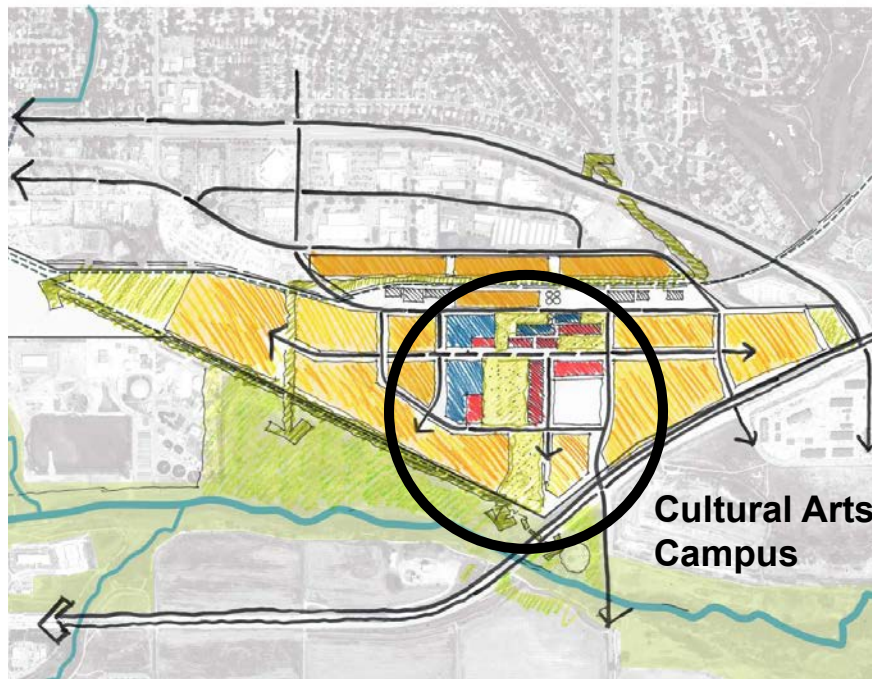
PUBLIC / INSTITUTIONAL CAMPUS
@ EAST PARCEL



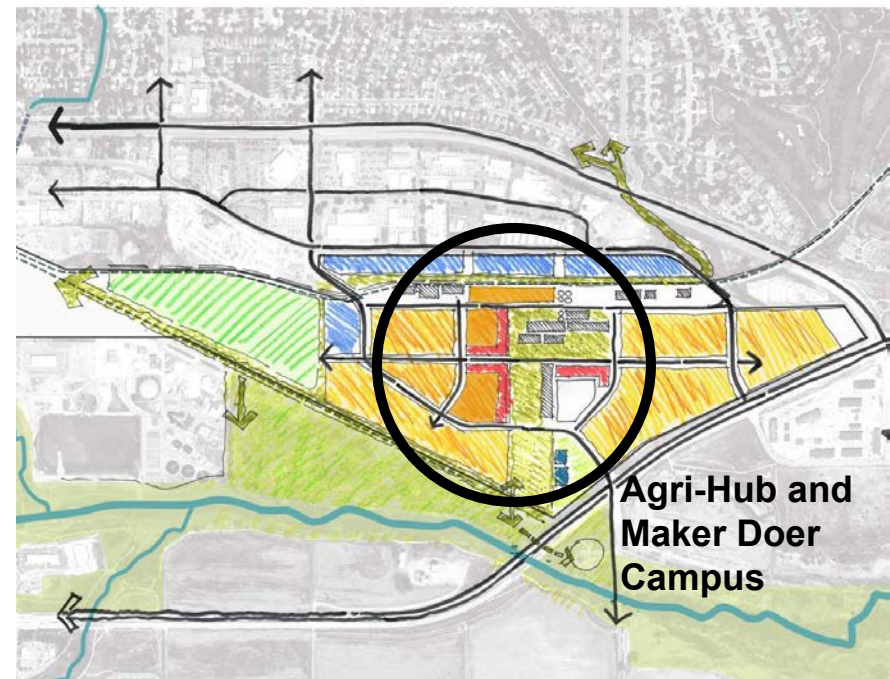


SUGAR FACTORY: TWO CAMPUS STRATEGIES

CULTURAL CAMPUS



AGRICULTURAL HUB + MAKER DOER CAMPUS

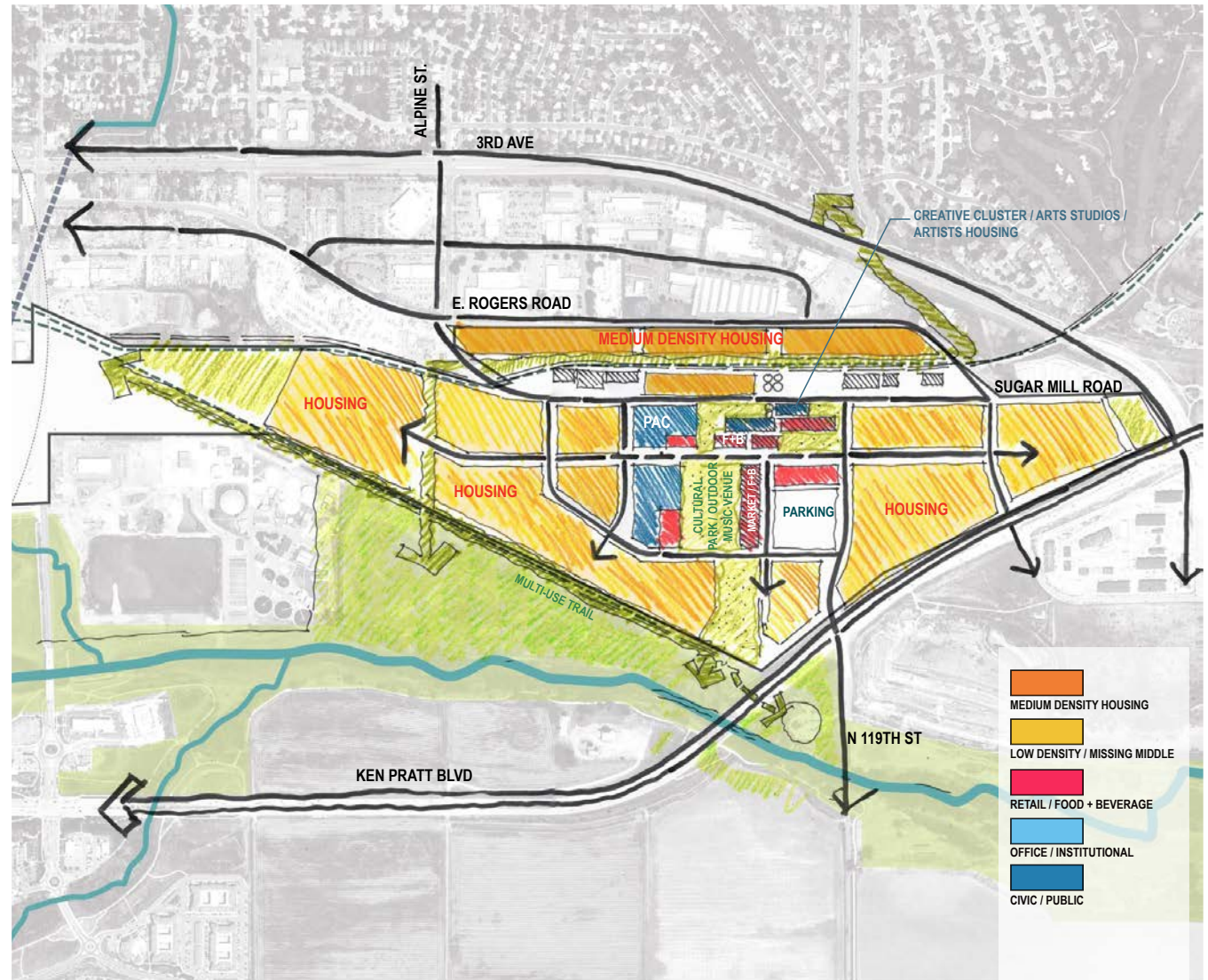


APPENDICES



SUGAR MILL CULTURAL CAMPUS:

NEW PERFORMING ARTS CENTER
 INDOOR / OUTDOOR MUSIC
 CULTURAL PARK
 ARTIST STUDIOS
 EVENTS SPACE (WEDDINGS, ETC)
 FARMER'S MARKET
 SMALL NEIGHBORHOOD RETAIL
 F + B / BREWERY / DISTILLERY
 MISSING MIDDLE HOUSING
 HIGHER DENSITY HOUSING





SUGAR MILL AGRICULTURAL HUB + MAKER DOER CAMPUS:

FOOD, AGRICULTURE + WELLNESS

FOOD INCUBATOR SPACE

AGRICULTURAL TECH +
INDUSTRIAL HUB

HIGH-YIELD URBAN GREENHOUSES

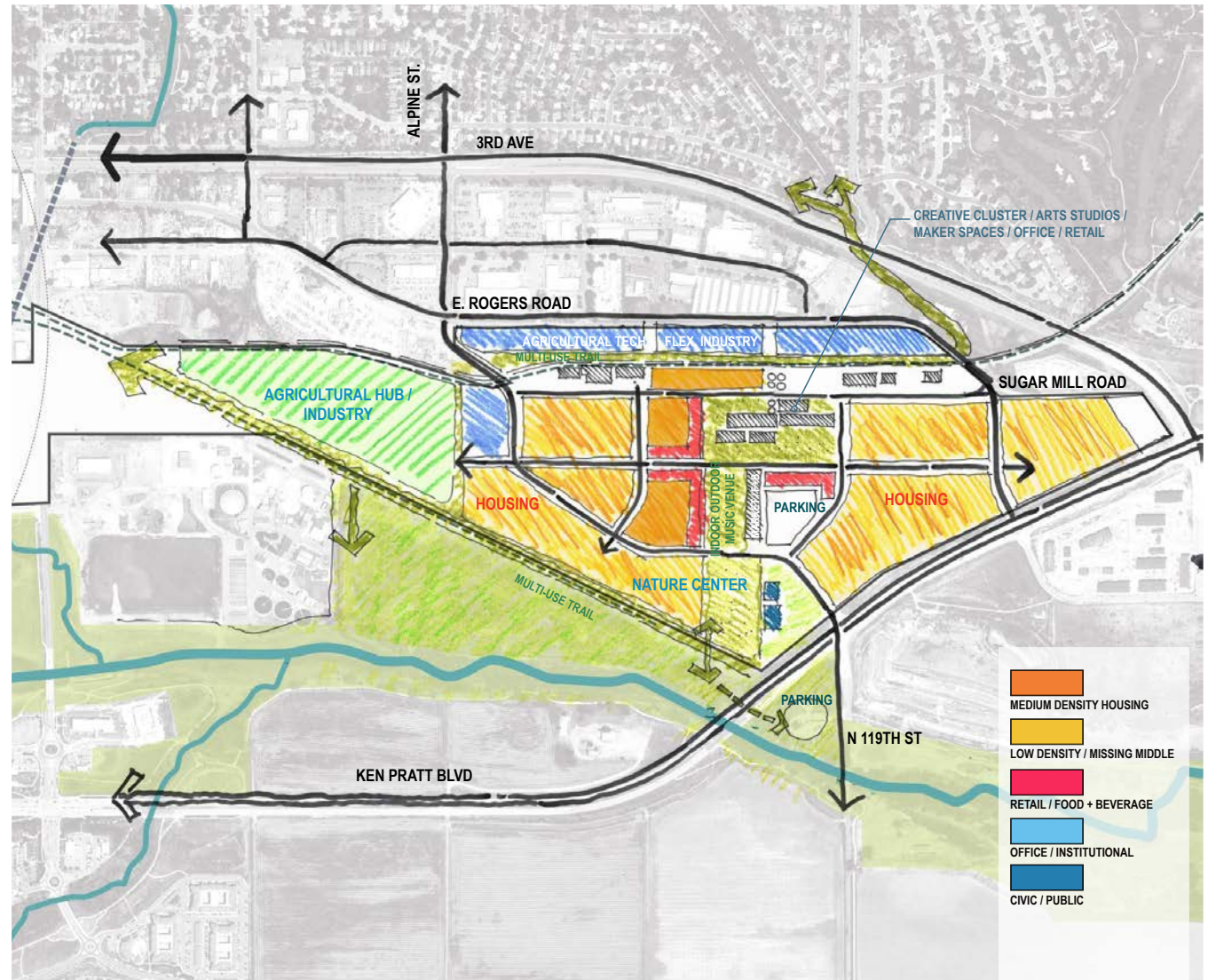
RIVER ENVIRONMENTAL CENTER AT
GREENWAY

SMALL NEIGHBORHOOD RETAIL

F + B / BREWERY / DISTILLERY

MISSING MIDDLE HOUSING

HIGHER DENSITY HOUSING





WHAT WE HEARD FROM COMMUNITY SURVEY AND WORKSHOP

Community Survey

THEMES

Sugar Factory Adaptive Re-use
Arts venues
Bike and pedestrian trail access
Affordability
Mixed-use development
Urban farming
Regional transit hub

Community Workshop - Most “thumbs up” responses:

LAND USE + HOUSING

Integration of nature center into STEAM
Outdoor Learning Center - nature and history of people
Affordable/accessible space for community groups
More affordable housing options, other than single family
Live/work housing
Live music venue
Re-use/adaptive re-use of historic structures

ENVIRONMENT, SUSTAINABILITY & RESILIENCE

Higher density housing closer to transit
Sustainable buildings, green roofs, biophilia
porous paving, food forests, performative landscapes, habitat areas, green infrastructure, pollinator gardens
Integration of recreation and play in historic facilities

TRANSPORTATION + CONNECTIVITY

More bicycle facilities, safer roadway crossings, more Copenhagen style facilities
More connections to St. Vrain Greenway

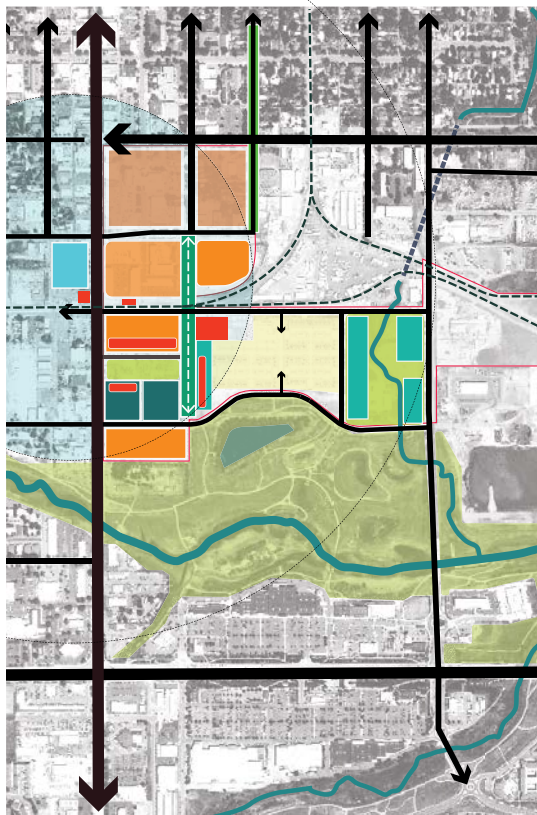
ARTS, CULTURE, INNOVATION + PLACEMAKING

Multi-use facilities
Community uses integrated into buildings – hobby shops, tool share, etc
Live music venue
Rock climbing in silos
Performing Arts Facility at Sugar Factory
Performing Arts Facility at STEAM

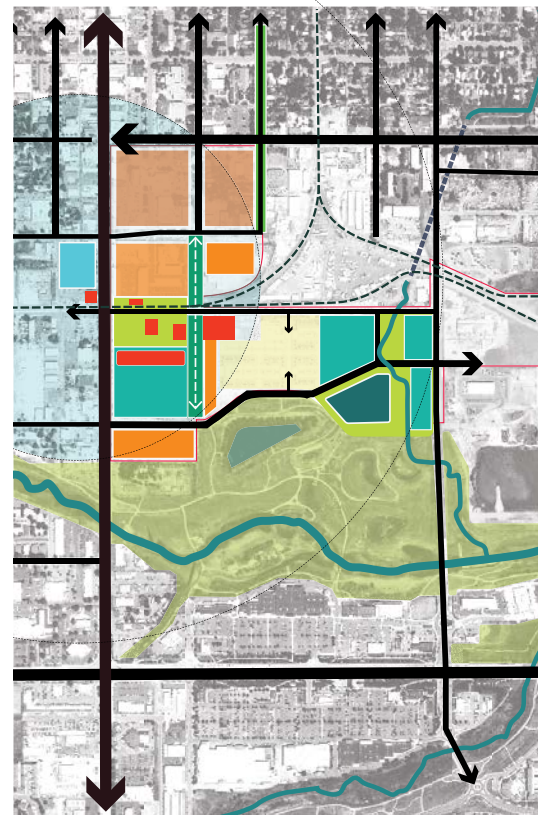


STEAM: THREE DISTRICT STUDIES

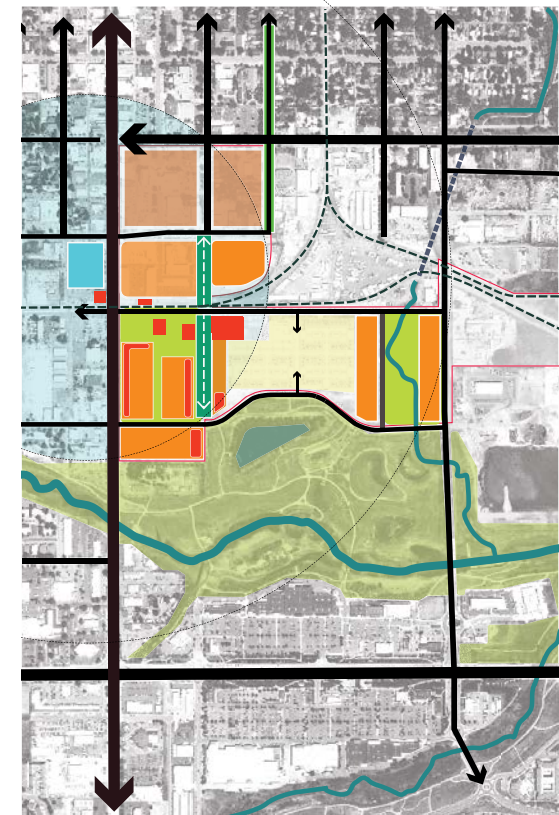
CULTURAL DISTRICT



MIXED - USE OFFICE / JOBS / INSTITUTIONAL



HIGH DENSITY RESIDENTIAL NEIGHBORHOOD



APPENDICES



STEAM: CULTURAL DISTRICT:

NEW PERFORMING ARTS CENTER

CONCERT VENUE / BALLROOM

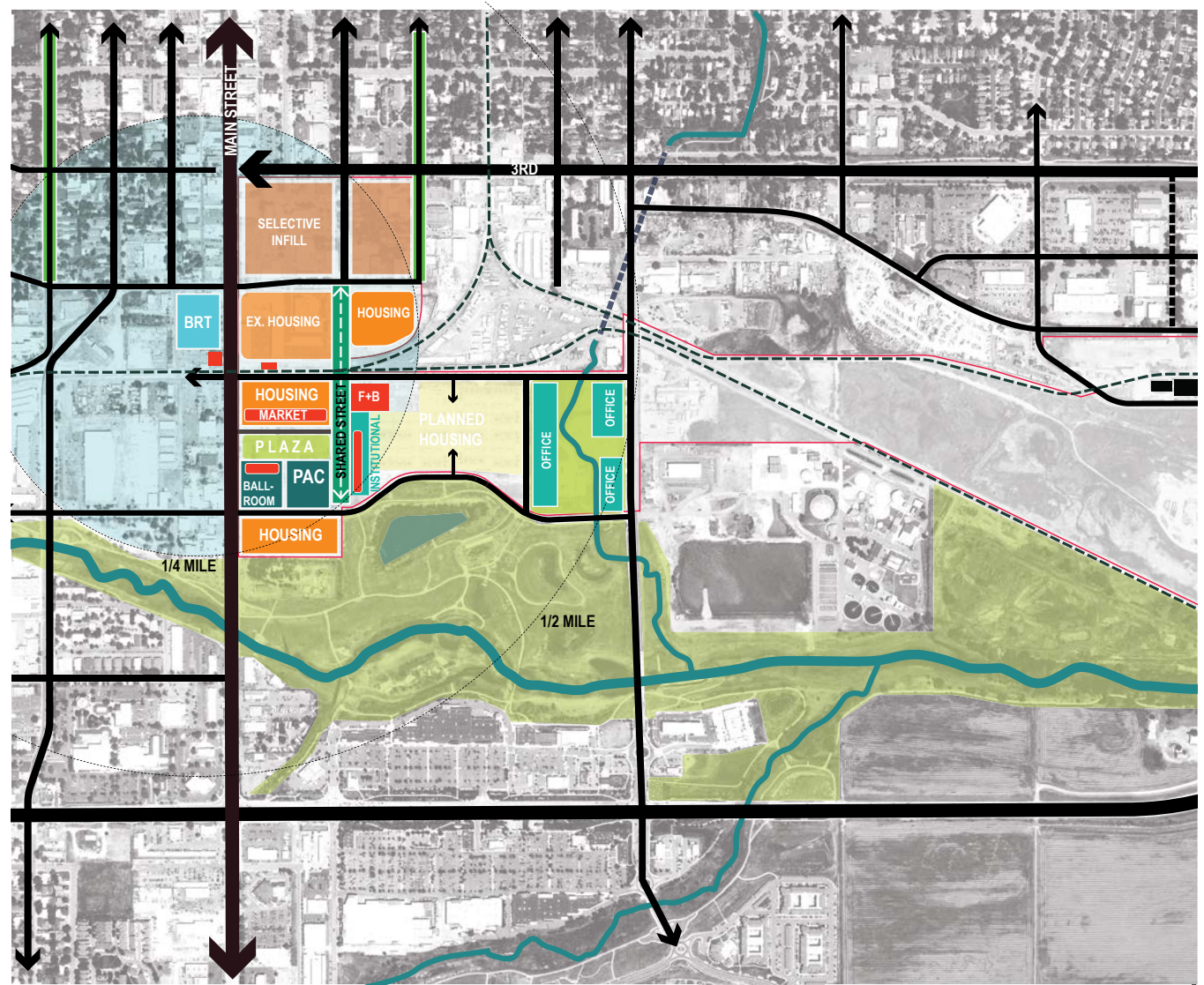
MUSIC + ARTS INSTITUTION

HIGH DENSITY HOUSING

OFFICE / JOBS @ EAST PARCELS

EVENTS SPACE (WEDDINGS, ETC)

MARKET / FOOD HALL



NORTH WYNKOOP + MISSION BALLROOM



LUBBOCK CULTURAL DISTRICT



ARCHITECTURE, LLP

JACKSON HOLE CENTER FOR THE ARTS



APPENDICES





STEAM:
MIXED-USE OFFICE / JOBS:

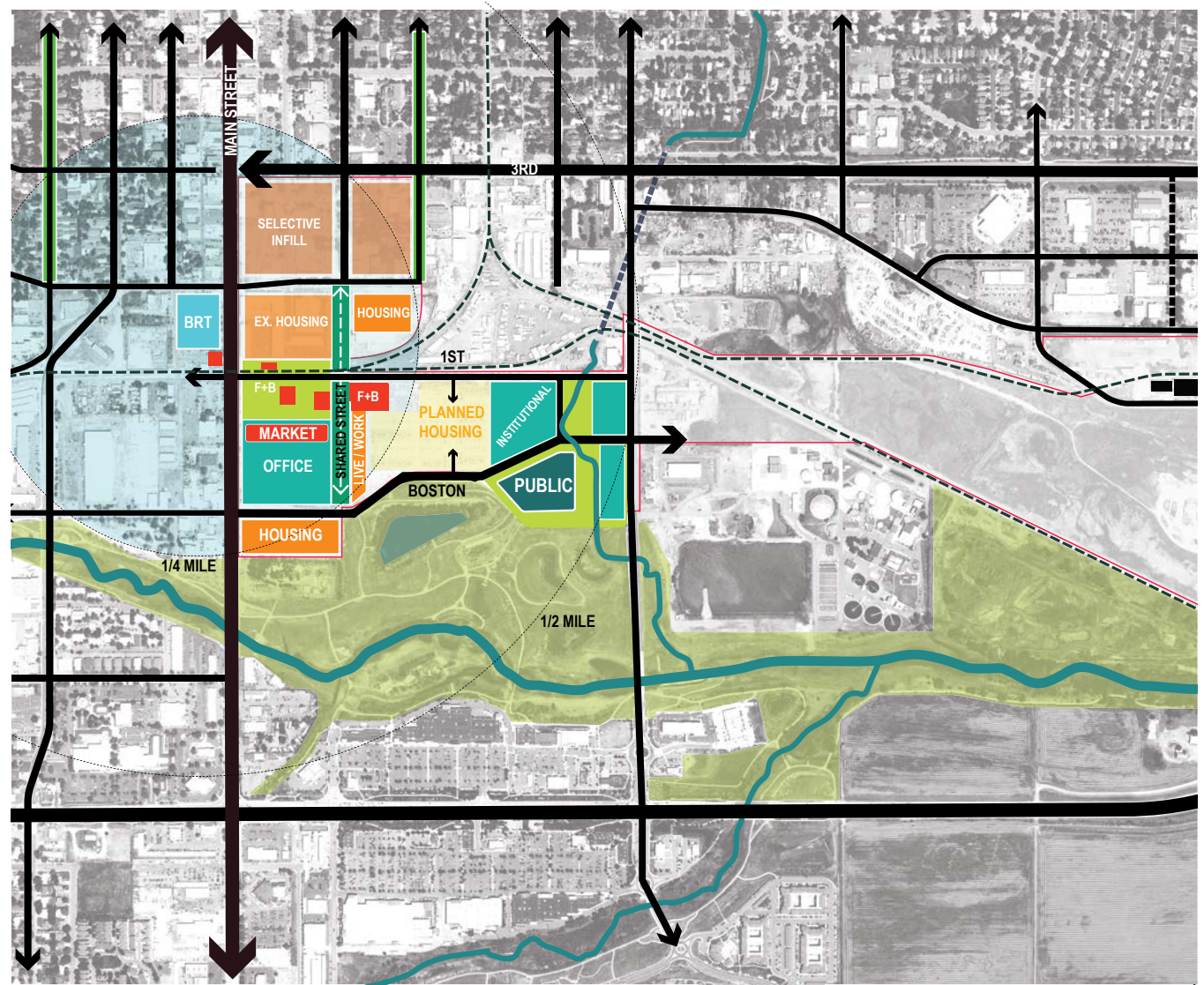
HIGH DENSITY OFFICE / MIXED USE

LIVE / WORK RESIDENTIAL

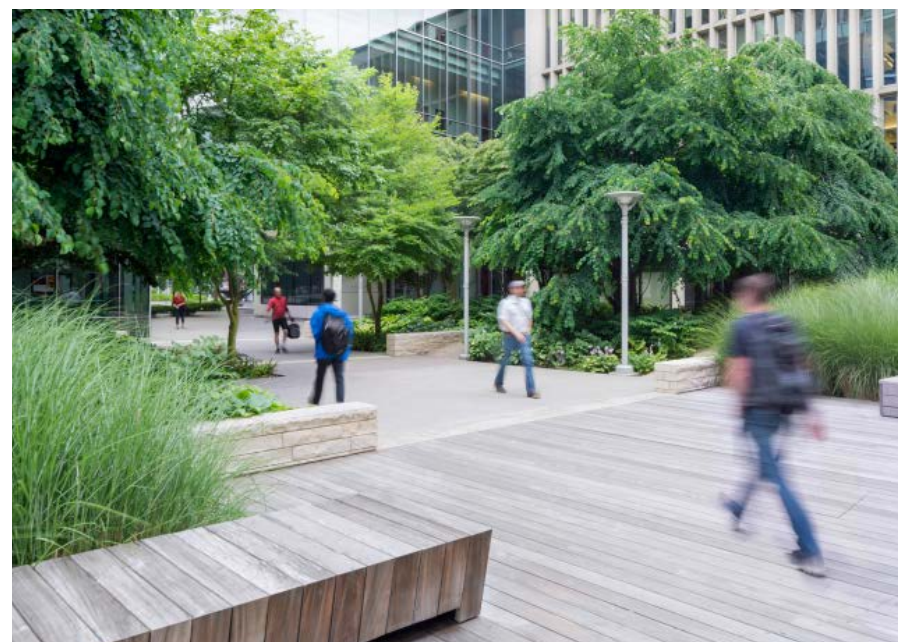
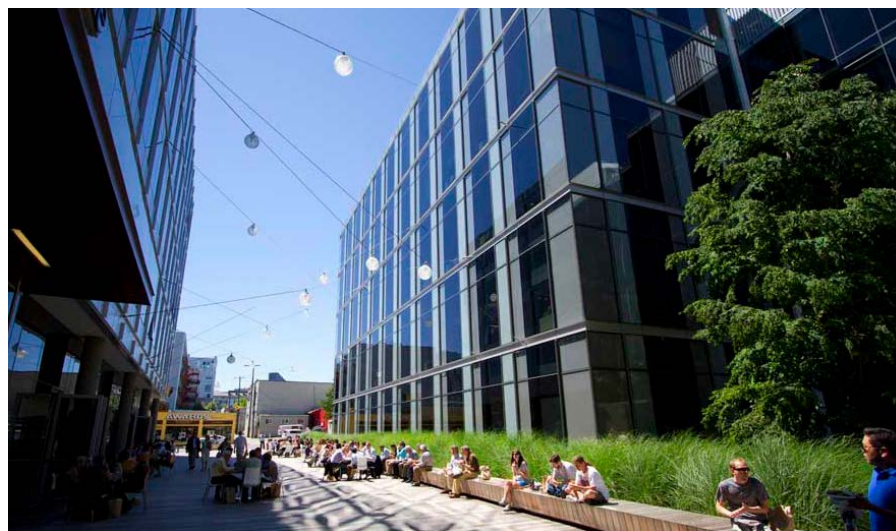
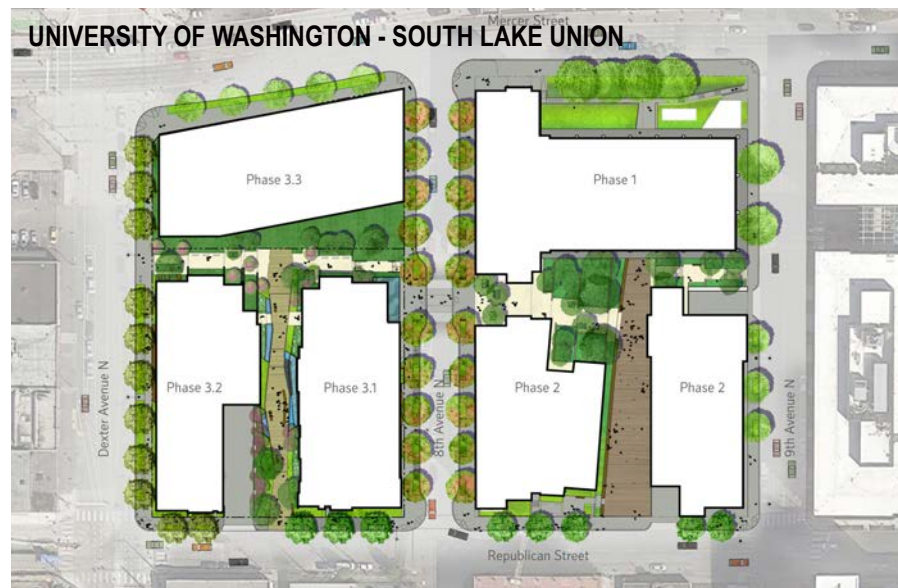
HIGH DENSITY HOUSING @ PARK

PUBLIC FACILITY ON PARK

PUBLIC / INSTITUTIONAL CAMPUS
 @ EAST PARCEL



APPENDICES





STEAM: HIGH-DENSITY RESIDENTIAL NEIGHBORHOOD

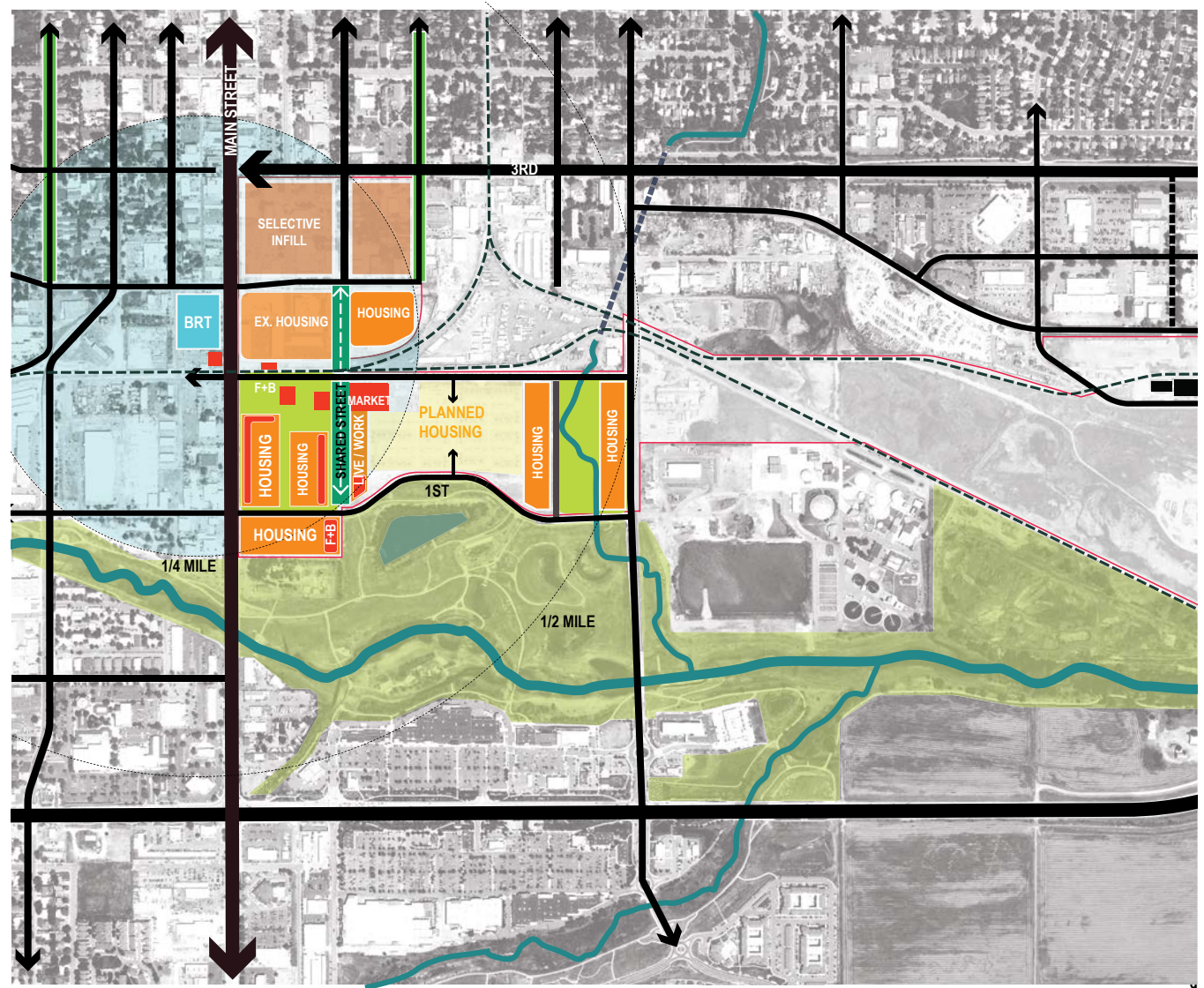
RESIDENTIAL / MIXED USE

LIVE / WORK

EXISTING BUILDINGS REPURPOSED
AS A MARKET / F + B

HIGH DENSITY RESIDENTIAL ON
@ EAST PARCEL

MAXIMIZE RESIDENTIAL DENSITY
ON THE PARK



APPENDICES

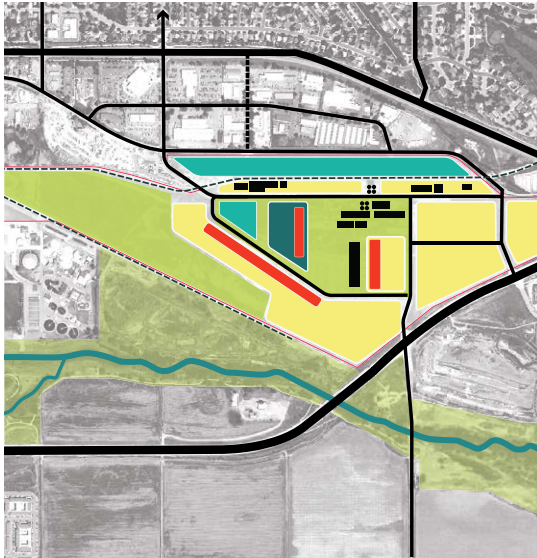
SAINT ANTHONY'S REDEVELOPMENT



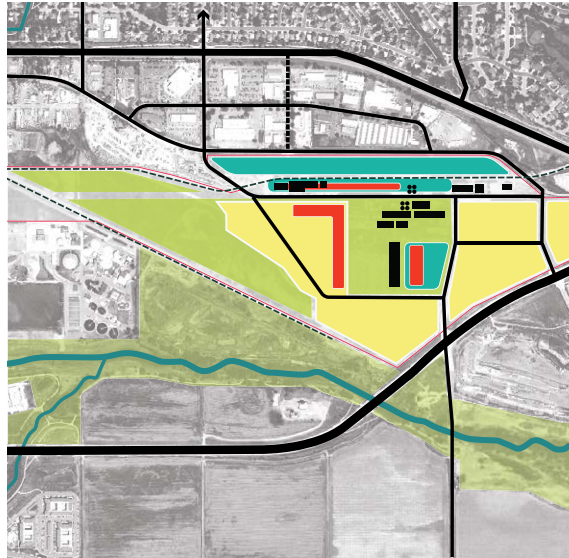


SUGAR MILL: THREE CAMPUS STRATEGIES

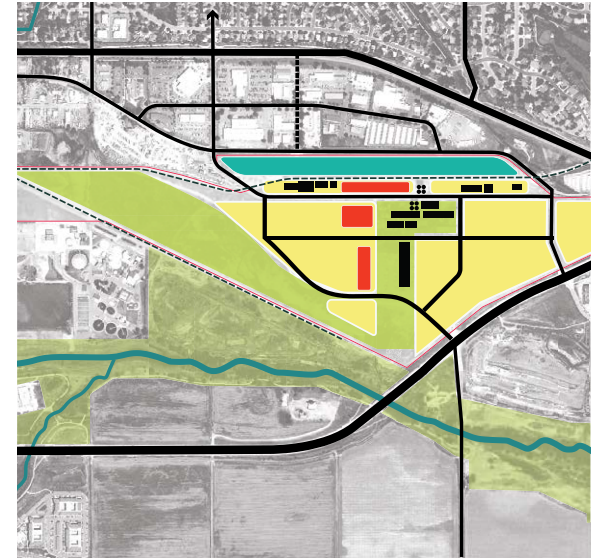
CULTURAL CAMPUS



AGRICULTURAL HERITAGE CAMPUS



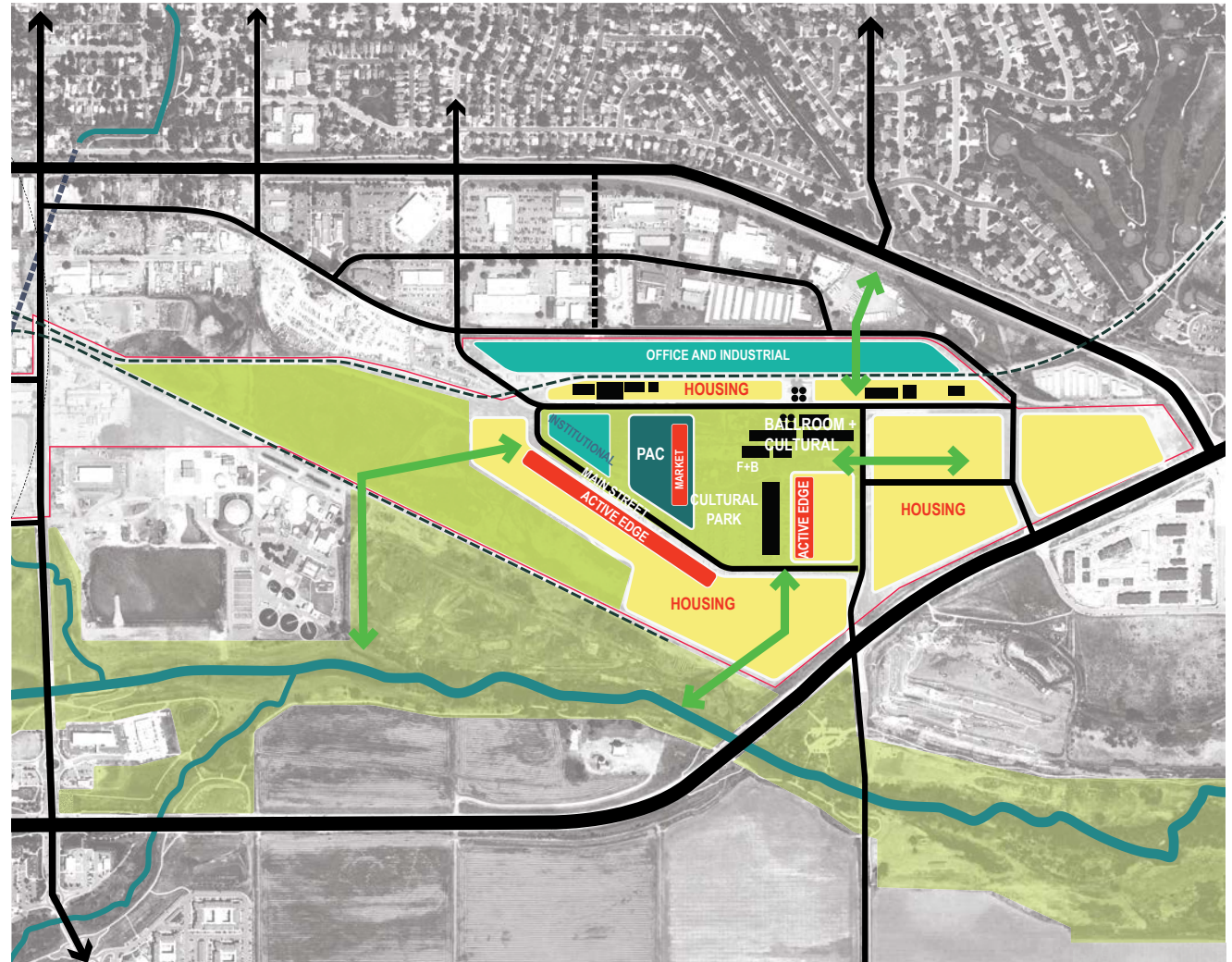
MAKER-DOER CAMPUS

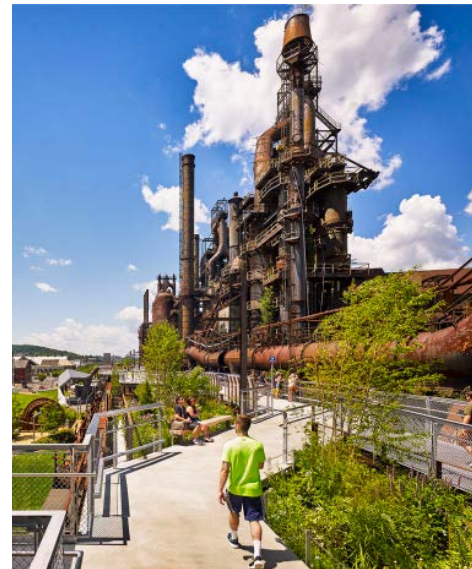




SUGAR MILL CULTURAL CAMPUS:

- NEW PERFORMING ARTS CENTER
- CONCERT VENUE IN MILL BUILDING
- INDOOR / OUTDOOR MUSIC
- CULTURAL PARK
- MUSIC + ARTS INSTITUTION
- EVENTS SPACE (WEDDINGS, ETC)
- SMALL NEIGHBORHOOD RETAIL
- F + B / BREWERY / DISTILLERY





APPENDICES

SILO PARK - AUCKLAND, NZ



DUISBURG NORD, GERMANY



SUGAR MILL

AGRICULTURAL HERITAGE:

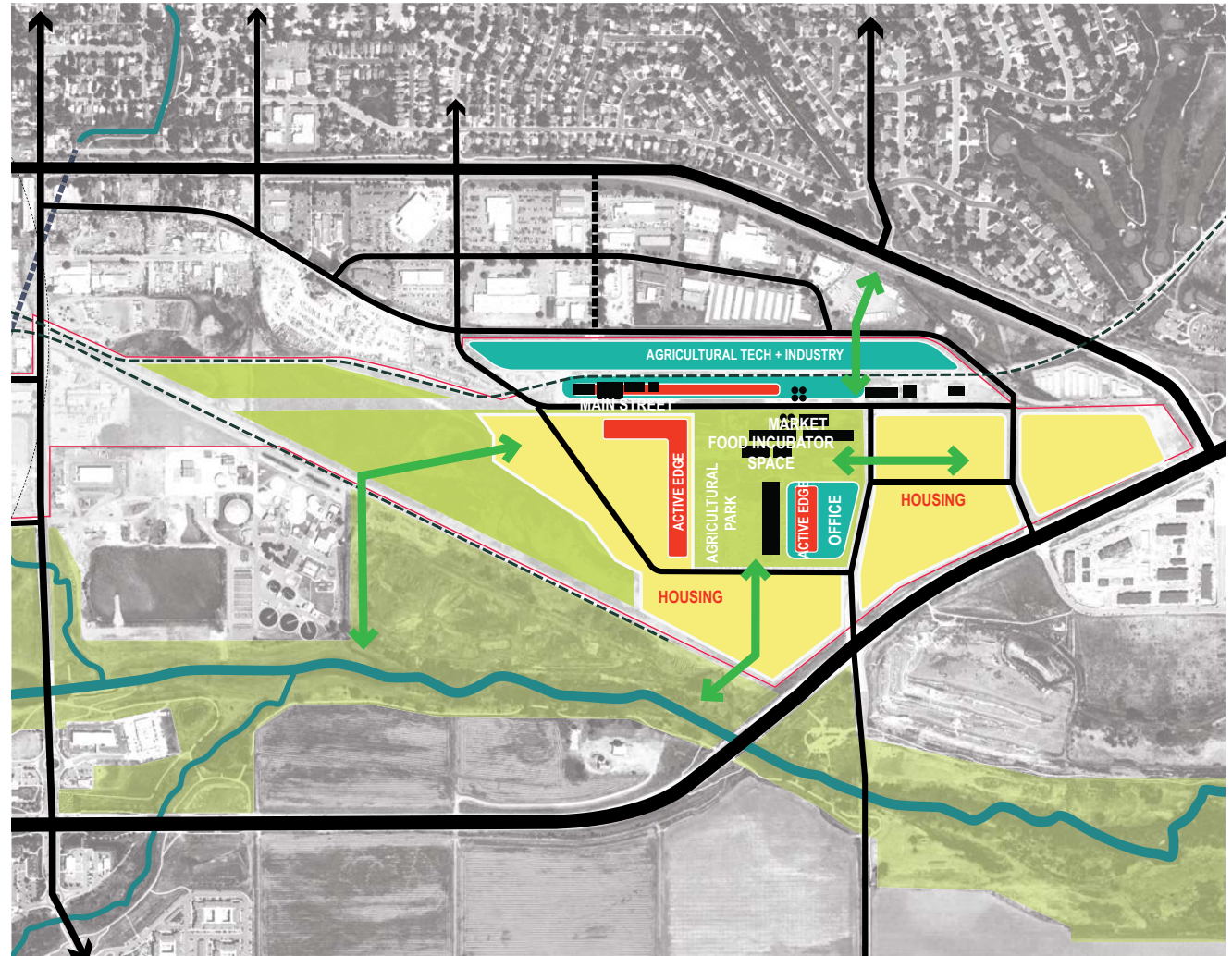
FOOD, AGRICULTURE + WELLNESS

FOOD INCUBATOR SPACE

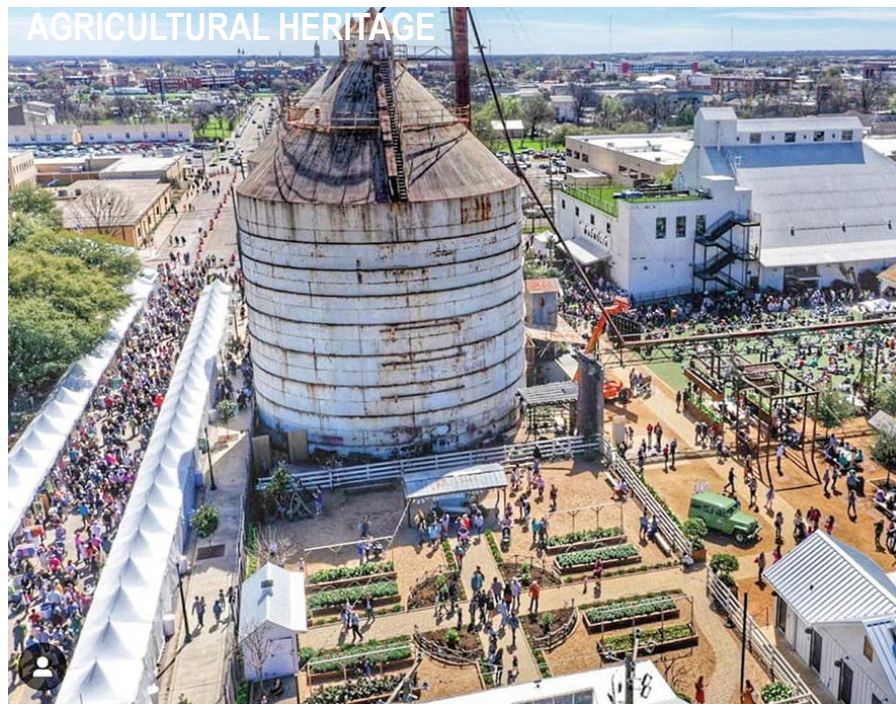
**AGRICULTURAL TECH +
INDUSTRIAL HUB**

HIGH-YIELD URBAN GREENHOUSES

F + B / BREWERY / DISTILLERY



APPENDICES



MAGNOLIA MARKET - WACO, TX



S*PARK - DENVER, CO



GROW DAT YOUTH FARM - NEW ORLEANS



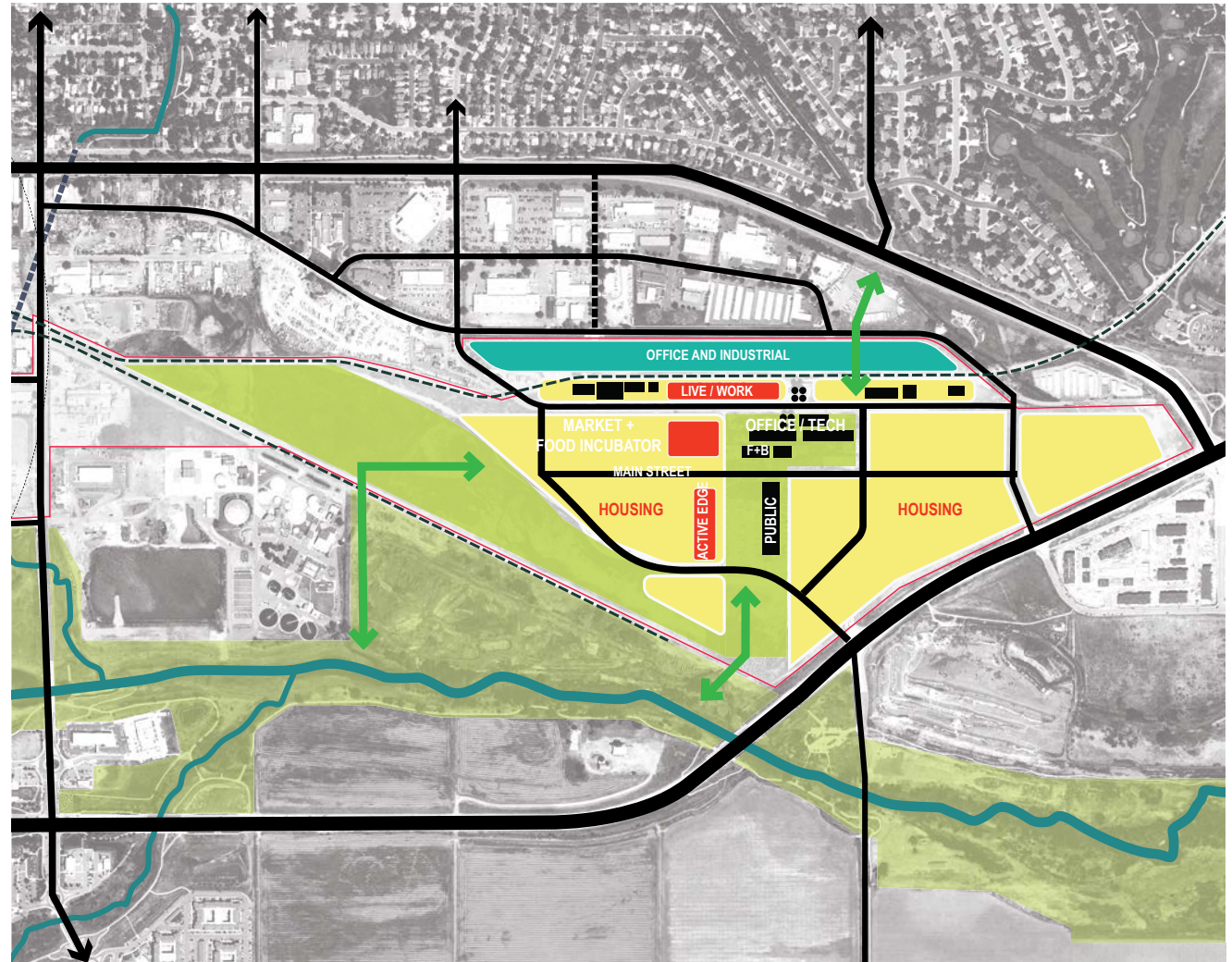
VERTICAL HARVEST FARMS - JACKSON HOLE, WY





SUGAR MILL MAKERS + DOER CAMPUS:

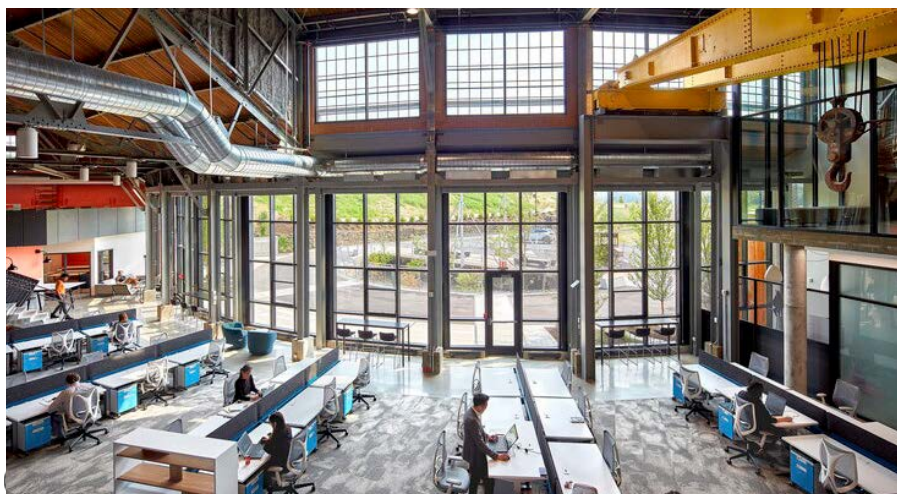
- INCUBATOR OFFICE SPACE
- SMALL SCALE MANUFACTURING
- PUBLIC MARKET + FOOD
INCUBATOR SPACE
- STOREFRONTS / RETAIL
- ANCHOR OFFICE TENANT
- F + B / BREWERY / DISTILLERY
- SMALLER AGRICULTURAL HUB



MAKER + DOER CAMPUS



HAZLEWOOD GREEN - MILL 19 + ROUNDHOUSE



OFFICE + RETAIL: ADAPTIVE RE-USE



APPENDICES

MISSING MIDDLE + COURTYARD HOUSING



SEATTLE, WA



PORTLAND, OR



BRAINPORT SMART DISTRICT, NETHERLANDS



EAST GREENWICH, RI



5.3 PUBLIC ENGAGEMENT

SUGAR FACTORY DEVELOPERS & STAKEHOLDERS MEETING, 12/1/21

COUNCIL BRIEF, 12/14/21

STEAM STAKEHOLDER MEETING, 1/7/22

PROJECT GOALS SURVEY_RESPONSE SUMMARY, 2/1/22

COMMUNITY FORUM, 3/2/22

SLOPE EVENT, 3/12/22

GROWING UP BOULDER FINAL REPORT, 5/9/22

ALTERNATIVES SURVEY BOARD, DECK AND RESPONSES, 5/23/22



Sugar Factory Developers & Stakeholders Meeting

December 1, 2021, 9:30-11am

Attendees

- **City of Longmont:**
 - Glen Van Nimwegen
 - Erin Fosdick
 - Phil Greenwald, City of Longmont Transportation Planning
 - Hannah Mulroy, City of Longmont Environmental
 - Todd Fleming, Business Analyst with
 - Tony Chacon, Exec Director for Urban Renewal
 - Tyler Stamey, Traffic Engineer
 - Lary Wyeno
- **Stantec / Trestle Strategy:**
 - Rhonda Bell
 - Nancy Locke
 - Danica Powell
 - Eric DeNando
 - David Starnes
- **Fairfield Residential project:**
 - Lenne Viehlehr, Norris Design. Planning and Landscape for Fairfield Residential
 - Lee Busse, Fairfield Residential. 23 acres at NE Corner of Ken Pratt. Horizontal mixed use
- **Remington Homes - St Charles Town project:**
 - Guillaume Pouchot, ____, Remington Homes
 - Jesse Adkins, Chris Shears: SA+R - St Charles
 - [Charles Woolley II](#), St Charles Town Company affordable housing, MF
- **Golden Family**
 - Barb Brunk, Resource Conservation Partners (north of RR trucks and reps Golden Family), DGM Ilc
 - Regge Golden, DGM Ilc
 - Drew Golden, DGM Ilc
- **Others**
 - Andy Welch, Sun Construction
 - Brad Slott, Civil Engineer
 - Alisa Romero, KTGy. Architects
 - Wendel Pickett, Frontier, 16 acres Rogers Road to RR tracks. Do flex industrial and workforce housing
 - Brad Slott, Kimley Horn

- Torry Willis

Meeting Goals & Agenda

As you know, the City recently initiated a sub area planning effort for the Sugar Mill and STEAM areas (see map below). This effort will build off previous community planning efforts to produce a detailed vision and development strategy for this area.

The project team, led by the City and Stantec, would like to meet with property owners, developers, and prospective developers to better understand current and future plans. Please join us, either virtually or in person, to discuss your plans and ideas. We will send out an agenda and meeting location as we get closer to this date, but would appreciate it if you can hold this time on your calendars. If possible, please let me know if you will plan to join virtually or in person so I can make sure to find a suitable location.

While there will be a public outreach component of this planning effort and involvement of other project stakeholders and community members, this will be a working meeting to specifically discuss development plans for this area (rather than a more general community meeting). Please let me know if there are others from your project team that you would like me to include on this invite.

I hope you can join us on December 1st, either in person or virtually using the Microsoft Teams link included in this invite. In the meantime, if you have questions or ideas you'd like to discuss, please feel free to reach out to me. I look forward to working with you on this exciting project!

TIME	AGENDA ITEM	PRESENTER
9:30	Introductions	Erin/Rhonda
9:40	Review Preliminary Site Analysis	Nancy
10:00	Property Owner/Developer Focused Questions/Discussion	Facilitated Discussion
10:50	Schedule and Next Steps	Rhonda
11:00	Adjourn	

Attachment

Meeting slide deck

Meeting Notes

Market comments

Charlie - Unlimited demand for affordable housing in the region. Building at 60\$ Median Housing. Also preservationists and would like to repurpose buildings into commercial uses. Until the rooftops are



Overall Comments

Wendell Pickett - City has done extensive study of the RR crossing issues. There is a lot of information for this area.

Barb Brunk - Costco site is only a portion of the PUD. The remainder is for mining and reclamation. There is a deeded access to _____ and N 119th Street and Sugar Mill Road. Railroad crossing that gets you to the city's open space on the south side of the tracks. Couple connections or integrated connectivity. The mining area will make beautiful open space once reclaimed. This piece of ground could contribute to the open space network for the neighborhood to allow density on other pieces and use this as community open space.

Chris Shears - what is the status of the Fairfield development. Answer: Lee Busey (Fairfield) - we are preparing the prelim plat (had pre hearing) to go in early January. Shears requested the plat to align curb cuts.

Lee Busey - Really helpful to see all of the plans. Before we get into questions, is there a timeline or goals for when the subarea is meant to be formalized?

Erin - Planning on working through Q1 and having something to Council Q2. Hope to have it wrapped up during second quarter of 2022 with Council.

Tony - when will extraction happen? How does this time with the development and potential to have residential next to mining operations. Barb: In a perfect world, it would be done in the next 5 years. Mining is only extraction (not processing). Planned access for gravel mining is south on 119 and east.

Reggie Golden - Any and all of the land could be available for open space. There was an idea to allow increased density on the other parcels and utilize the open space in the reclamation area. Everyone wouldn't have to provide their onsite open space. That is the best use for this, adjacent to city open space.

Nancy - Are these wetlands of value? Answer Reggie Golden: Not high quality wetlands. Would require 1:1 replacement, but nothing more. Army Corps has evaluated.

Tony - there are two flood plains. One from the north, Spring Gulch. This is where the flood plain. As part of this process, we would like to remedy the floodplain from Spring Gulch.

Barb- Mining and reclamation is a great way to resolve this floodplain issue.

Matt Cavanaugh - Residential developers don't get the same treatment as removing gravel. Not allowed to touch the wetlands. Have gotten a determination that they are not allowed to touch them. Does mining have different rights than developers.



Barb - Not exempt from 404 permit. The mining has always been a solution/opportunity to clean up that piece of ground. They are old wash basins covered with cattails. Because it is such a low quality, mining can get 404 permits and replace at 1:1 onsite as part of the reclamation. Offsite credits are hard to manage and expensive to manage. Cheaper to go pay someone else's bank than find your own. Will go into a deed restricted area through a conservation easement and open space - available to the public. Not buildable. Difference is that the wetlands will be removed, restored and replaced as part of mining process.

What are Challenges and Opportunities

Guillaume - concerned about aligning access to adjacent properties. Coordination with CDOT for primary entrance. Is there a desire to take the local street and make it a wider arterial street that borders the sugar mill north?

Chris Shears - Does the city anticipate street improvements (sugarmill and 119)? That would influence our planning. What is the feasibility of extending Lashley south to western bow of bowtie? Curb cut alignment with Fairfield.

Guillaume - Goes back to what David said and the realistic mixed use demand. How do we confidently as a group figure out what is viable. A lot of things have changed in the last 5 years. How this relates to access. What is a realistic

Charlie - Reuse of historic buildings could help with placemaking for the entire community. Whatever connectivity you can make to those buildings as a core placemaking for the region, that would be great. Looking at saving the big tin shed. What we don't want to have is roads dead ending into buildings. Need a mobility grid that works for everyone.

Lee (Fairfield) - Appreciate this. Open to any and all conversations about connectivity and connecting roads. Have done a lot of this in a vacuum. None of this is solidified or in place. Have worked with the City for a year, but didn't have info on adjacent parcels. More than happy to discuss how to connect, and make roads work.

Charlie - pretty new at this too. Through Tony's efforts working through the 2nd phase of environmental studies to figure out what it will cost to do environmental mitigation on the buildings and soil issues that may be encountered. Won't be finished until early part of 2022. This sets up the feasibility of saving buildings, remediation, funding, financing, etc. Very committed to making that happen.

Barb - There is a Phase 2 on the property that was shared with ULI. Charlie - we have all the historic stuff.

Barb - Blank old metal building that is a blank canvas. Paint the roof. It is the gateway to Longmont. Could be an extraordinary piece of art.





Guillaume - building is structurally sound. Exterior needs help. Could be an incredible gathering space. That itself could be a special spot for people to come and gather. Farmer's market, etc. Could work well with restoration and preservation that Charlie talked about to the north.

Nancy - There are a lot of agricultural facilities there and the ULI Tap talked about it. What are your thoughts on that?

Charlie - will look at exploring. Don't know how many more seed labs or ag uses are out there that could be attracted and create synergy of ag business. Willing to explore.

Wendell - Rogers Road could be a more transitional piece with 5-10,000 sf buildings for industrial/flex space to complement what we are doing.

Barb - Renewable energy? Solar panels on metal roof.

Wendell - Did 2 proposals. One is biowaste for 8 acres (did not move forward). One was a recycler of electronic goods.

Charlie - last 3 projects did solar and use green building standards. Clean energy LLC has tried to see how to make boilers into biomass generator

Tony - one opportunity that may be possible is energy that is produced off of wastewater treatment plant. Generates heat from processing. Can developments utilize heat? Agricultural grow houses? Small scale version is Stanley Marketplace. Greenhouses grow produce used by restaurants.

Hannah - If you are interested in solar, sustainability or community solar, reach out to her to coordinate with LPC. Has to be uniform credit with net metered. Don't have community solar billing at this point. Community solar is hard.

Timeline for Development

Andy Welch – NW corner of intersection. Land will be a for sale piece of ground. Most likely multi family with some mixed use. Probably going to come forward quickly. Want to coordinate with Fairfield.

Wendell- 601 Rogers Road - At least 10 years out.

Guillaume and Charlie - long road to go with environmental issues on both projects. Will continue to work with staff. Guillaume wants to get through annexation in early 2022. Looking for shared opportunities on roads, utilities, open space, detention, etc.

Guillaume - started out as SF developer, but shifting towards more attainable housing. Don't want to create the same product over and over. Mostly fee simple projects. Have partnered in Broomfield and other communities to be affordable at 80%. Remain very flexible. Don't see this as a single family project



Andy - won't be the end developer. Interest is exclusively for rent multi family. With some mix. Comparable to what Lee is seeing.

Charlie - what does the acronym STEAM stand for. ? Focus on sugar mill building and elevate "arts" pieces of it.

Example <https://www.youtube.com/watch?v=e0C1kRtVYAY>

Phil - RTD - is looking for 5-10 acre sites west of Lashley alignment.

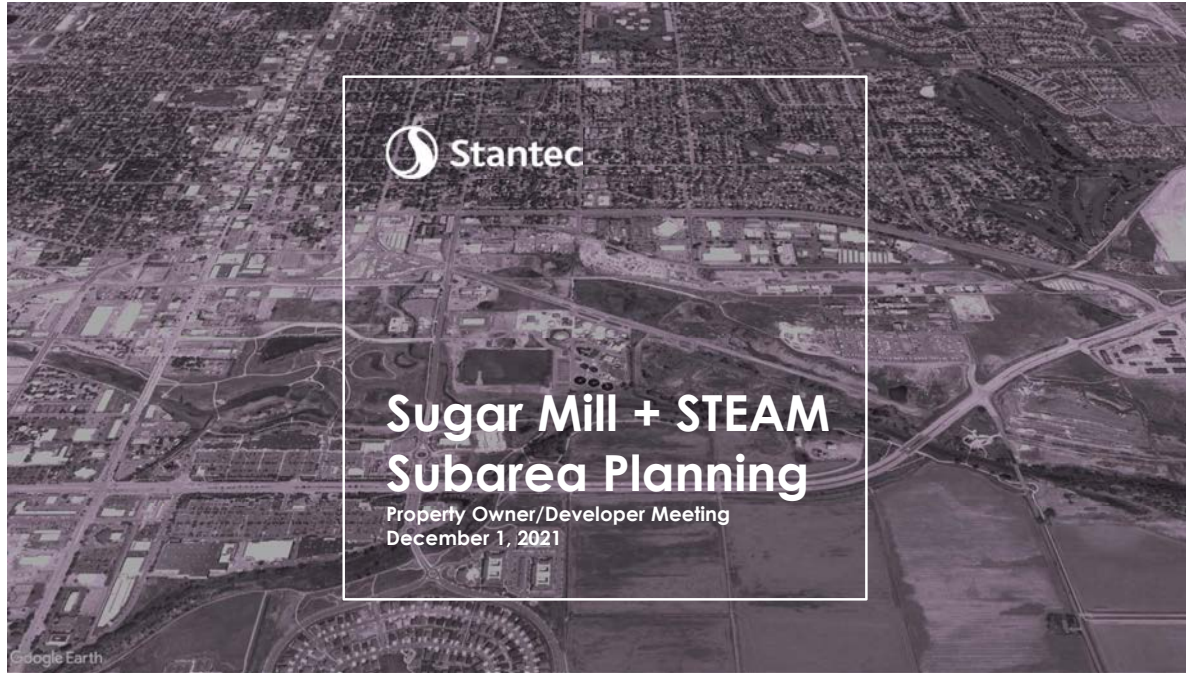
Guillaume - Excited to look at this as a gateway project comprehensively

Lee - Getting to critical juncture where looking at architecture. Overall conceptual direction conversations with the city. Does it play off sugarmill or something completely different? Will start to share info with neighbors. Gateway project. Have looked at [Agritopia](#) in Gilbert AZ . How could we take best practices from projects and apply them?

Next Steps

- Working session on immediate projects to coordinate on roads, access, open space, detention, utilities, cost sharing, etc.





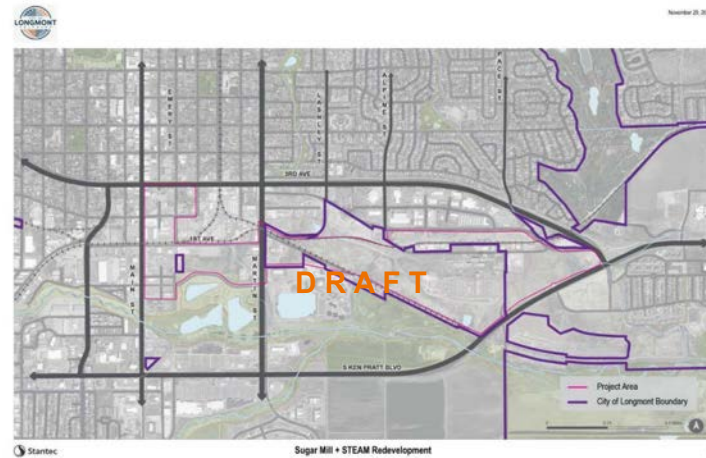
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10:00	Property Owner/Developer Focused Questions/Discussion	Facilitated Discussion
10:50	Schedule and Next Steps	Rhonda
11:00	Adjourn	



Scope and Purpose

- Prepare detailed subarea plan for the Sugar Mill and STEAM areas
- Focus:
 - Opportunities and Challenges
 - Urban Design
 - Multimodal Connections
 - Implementation timing of public and private uses
 - Infrastructure needs and phasing

Meeting today – gather property owners/developers to better understand plans, needs



Existing Planning Guidance

- ULI TAP study
- Envision Longmont
- Main Street Corridor Plan
- Building STEAM Process
- Other resources



Preliminary Challenges and Opportunities

- **Cohesion:** knitting together the neighborhoods, the site area and the river
- **Environmental:** clean up environmental conditions
- **River:** connecting to the river
- **Gateway:** eastern gateway to the City
- **Destinations:** new and vibrant destinations at STEAM and Sugar Mill sites
- **Diverse housing:** infill of diverse housing types appealing to all income levels
- **Enhanced connectivity:** vehicular, pedestrian and bike
- **Extending transit:** to eastern portions of the site

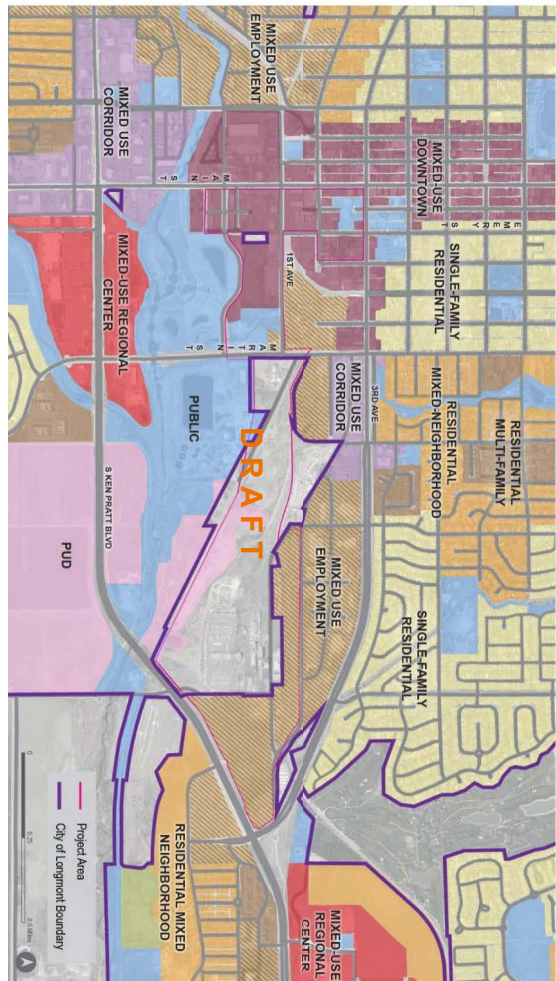


Preliminary Market Findings

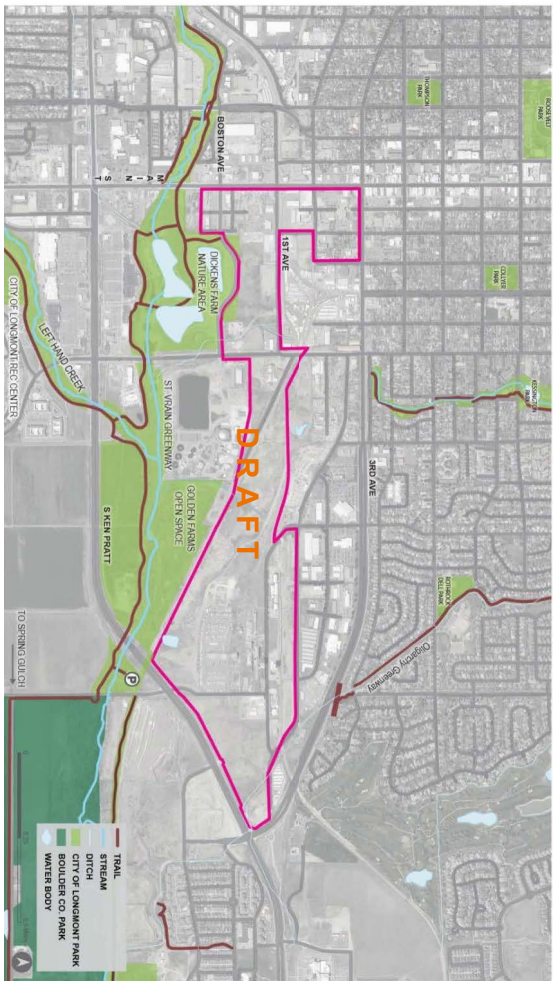
- **Demographic shifts:** aging households, households w/o children) significant driver of housing demand going forward – different housing preferences
- **Missing middle:** Forecasted increased demand for diversified housing stock, including missing middle housing
- **Decelerating growth:** forecasted reduced growth nationally and regionally (births, migration, mobility)
- **Pandemic's divergent outlooks:** industrial and multi-family humming along; retail and office going through great reset as changes continue to accelerate
- **Work anywhere, live anywhere:** smaller vibrant suburban communities attract; less office space demand expected
- **Flexibility and convenience:** drive the next decade in terms of working, living, shopping
- **Q: What are you seeing in the Market?**



Zoning

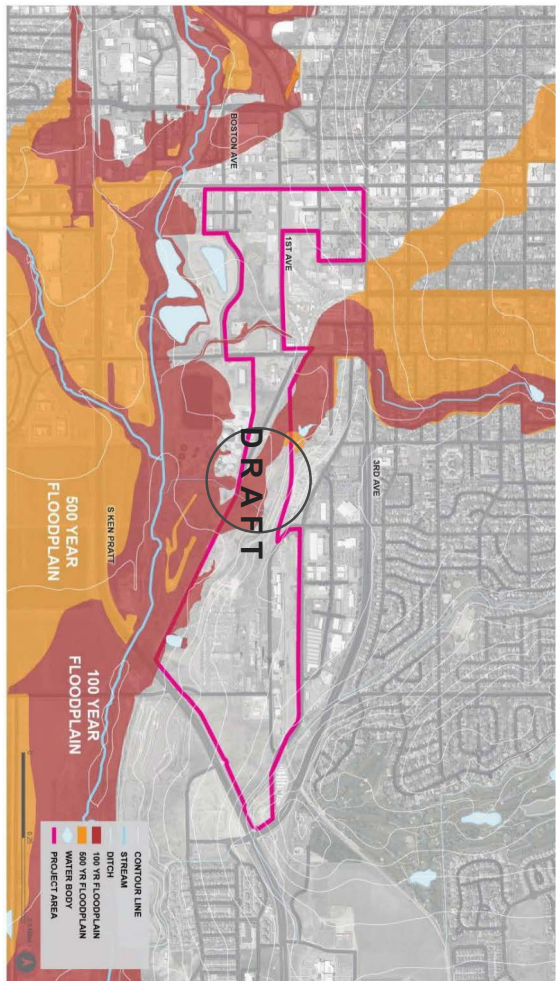


Open Space

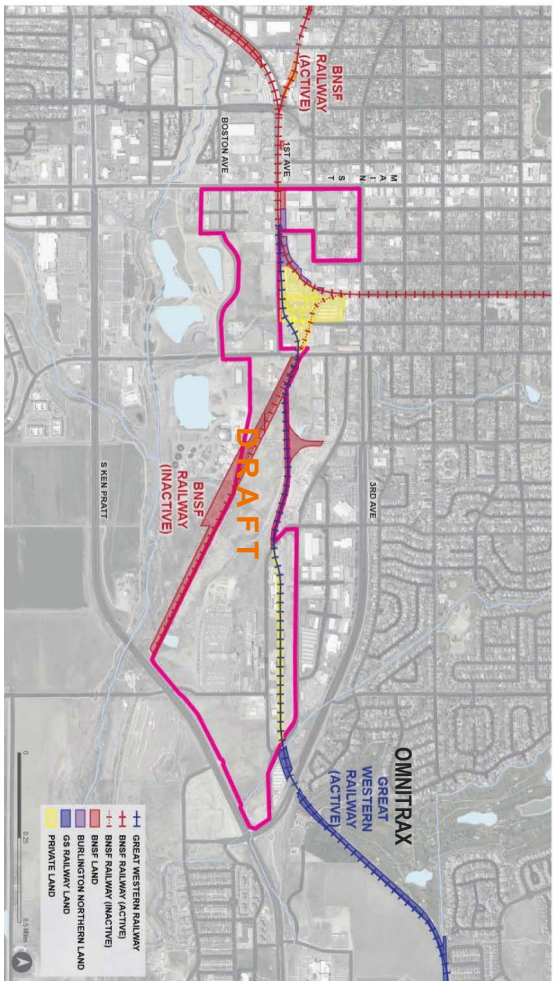




Floodplain

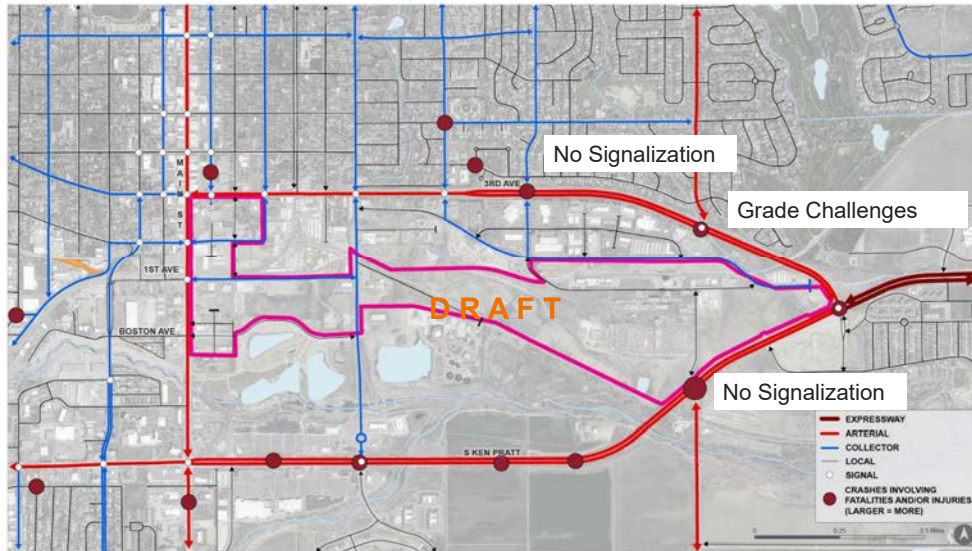


Railroads and Ownership

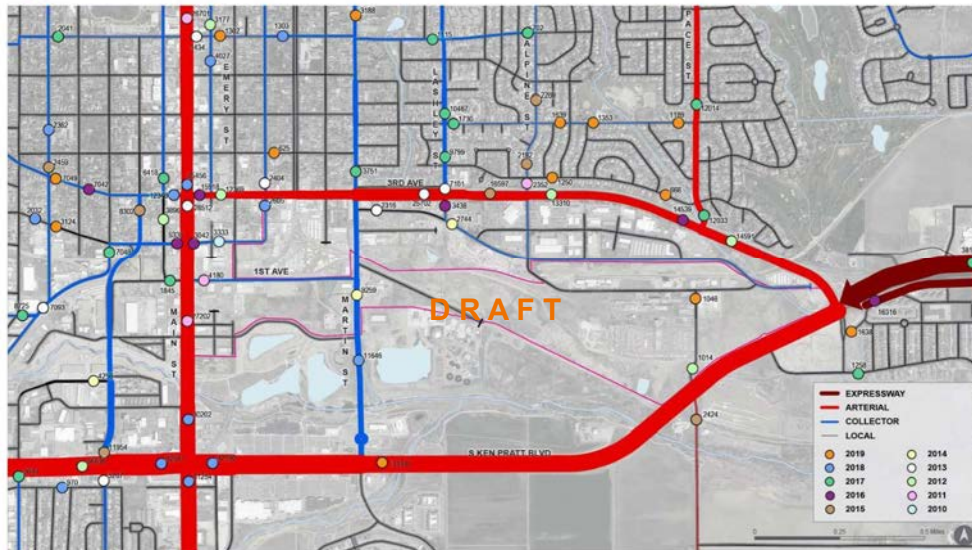




Streets

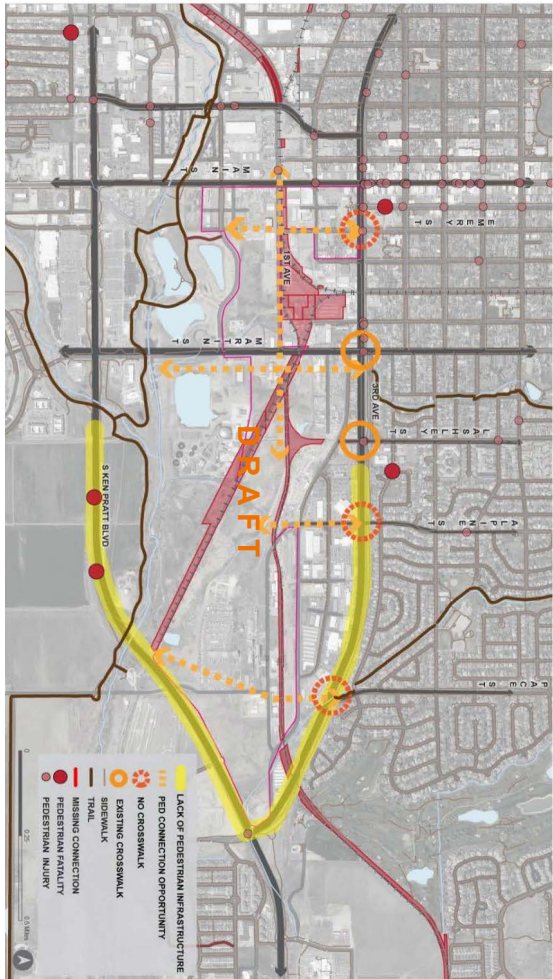


Traffic Volumes

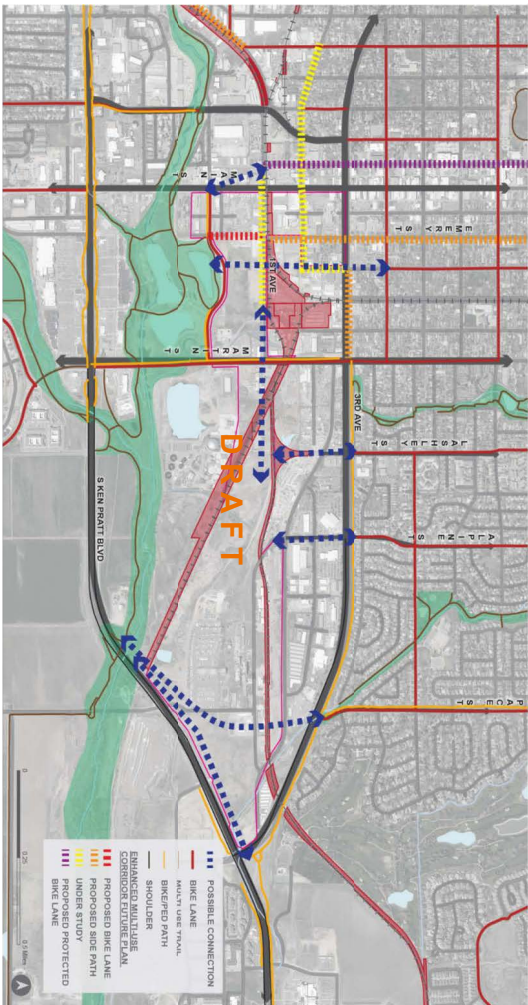




Pedestrian Access



Bike Infrastructure







Key Challenges - Connectivity

3 rd Ave: ped/bike orientation	N/S connectivity: 3 rd Ave	N/S connectivity: Ken Pratt	E/W connectivity	Pedestrian
<ul style="list-style-type: none"> Streetscape enhancements / Calming 	<ul style="list-style-type: none"> Lashley extension Alpine signalization Pace grade challenges Safety issues: Rogers Rd/3rd and 3rd/Ken Pratt 	<ul style="list-style-type: none"> Great Western Drive intersection/N 119th St: safety issues Other access? 	<ul style="list-style-type: none"> Roadway/railroad crossing conflicts 	<ul style="list-style-type: none"> 3rd ave <ul style="list-style-type: none"> Access to St. Vrain Lack of trail north of St. Vrain river Bike Infrastructure <ul style="list-style-type: none"> Lack of bike infra in east

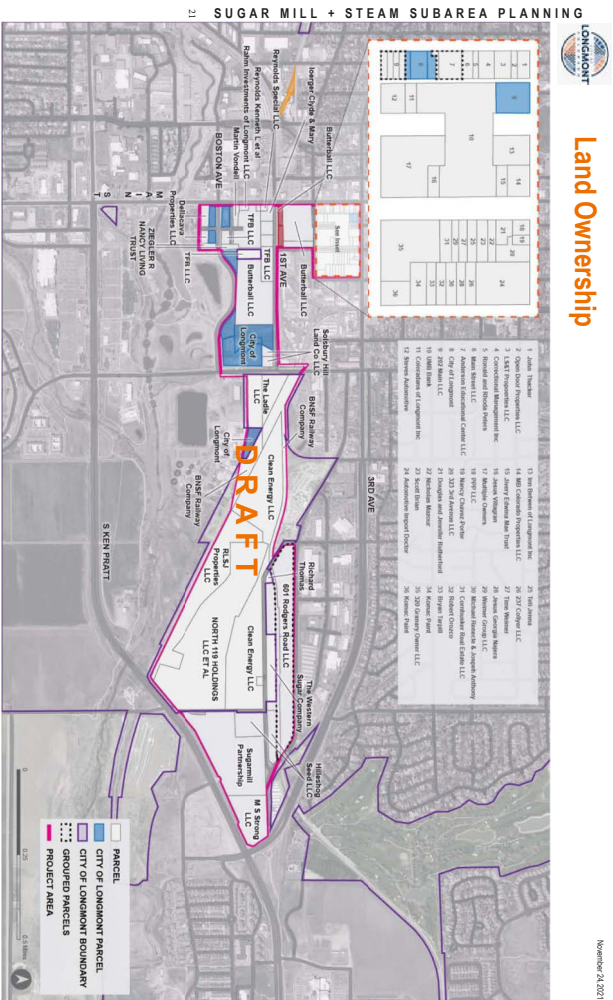


Key Challenges – Open Space

Green Infrastructure	Private v Public	Phasing	Access	Agriculture
<ul style="list-style-type: none"> Integration of drainage vs. recreation 	<ul style="list-style-type: none"> Block level open space vs. development open space Open space spectrum across STEAM and Sugar Mill 	<ul style="list-style-type: none"> Temporary 'placeholder' open space Open space catalyst – Phase 1 or rooftops 	<ul style="list-style-type: none"> Connection across (over / under?) RR to St. Vrain Greenway, Dickens Farm, Golden Farms 	<ul style="list-style-type: none"> Role of agri-hub / agri-oriented amenities



Property Owners/Developers

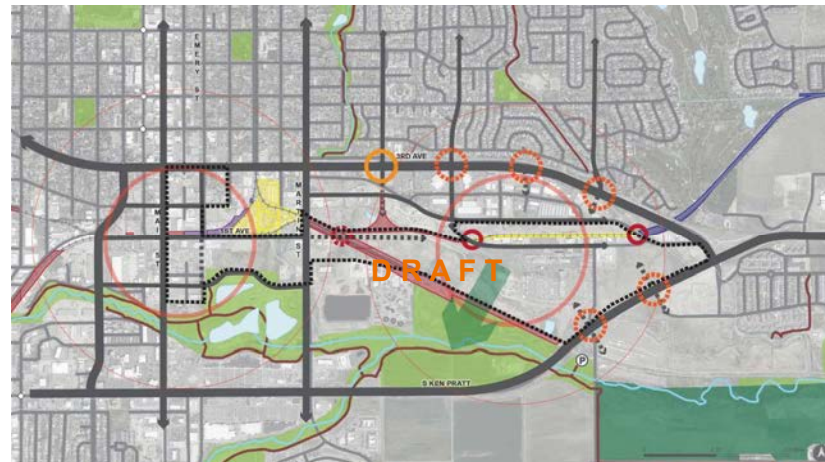




Concept Plans



What Ifs....Connecting the Site





What are **challenges** can the subarea plan help resolve?

- Access
- Utility infrastructure



Will you include **housing**? Approaches to attainable housing, housing innovations?

- Housing diversity – families, mixed income choices, rental v. ownership
- Attainable housing – percentage mix, targeted AMI



How do you plan to connect to **St. Vrain River** open space?

- Trails
- Open space corridors



Is there something we haven't discussed but
you feel is important to bring forward?



What is your **vision** for the area?

- Big picture view: what does success look like?



Schedule/Next Steps



Meetings from City Council update on December 14, 2021

1. Presentation by Erin and Stantec Team
2. Council Discussion

Rodriguez (mayor pro tem)

- Don't agree with combining the areas. Martin is a dividing line. West side is more walkable and connected to downtown
- Missing middle isn't rental housing. Likes paired housing, townhomes, etc. Not only multi family

Waters:

Important to pick up the pace in the steam area and build on the work that has been done already. Advisory panel. Don't start from scratch. Collection of narratives. Link to this on Engage Longmont.

Preamble with statement of value. Use this info from 2019 which was a "starter discussion". What attention is being given to this work? Summarize this in the current workplace and build on it. "Full Steam Ahead". Important question. Erin: we are linking the two projects. Ronda: the work will be built upon and utilized.

Martin

- 2 areas have different levels of unknowns. West of Martin has been studied hard and there is a lot of city property. Fitness for public amenities of project has been studied.
- A lot of unknowns about east side, especially hazardous materials. Property owners consortium. Council doesn't know if council's vision aligns with Steam.
- Don't know what transit vision east to west.
- Much more walkable on west side, versus east.
- Would like to see a multi modal transit plan
- What do you know about the hazardous waste situation. Tony: there is environmental sampling for the buildings and the grounds for the property that Dick Thomas owns. Effort has been running parallel with this effort. The info is being fed to the Stantec consulting team

Peck (Mayor):

- Need to separate the two areas, housing will be the number one thing we need. Sugarmill site will have more of an attraction for middle range housing.
- Push back on the for sale. We don't know in 18 years are we going to have such a mobile population that they want to rent and not own. Don't like making those assumptions in advance. People would like to have control of owning their own equity and home. Townhouses, condos, paired - more for sale units. Likes engagement.



Martin:

Question: what was the basis for combining the two study areas. Erin: These two areas are in proximity to each other. While we are looking at these at 1 subarea plan, there are 2 components. How do we knit them together. Sugar mill and areas to the north, etc. Steam to downtown. Concern that this is a homogeneous plan, but this isn't the case. Need to look at them as character areas. Combining is way to maximize funding, engagement, existing conditions, floodplain, etc. What do they both have in common? Sugar mill is part of the east 119 gateway. Priority for the community. More interest in the eastern portion. We hear loud and clear that these areas are very distinct. Harold: Efficiency is important. How to look at two sub plans and knit them together. Eastern part of study area has 2 active development projects with tangible interest and east part of Longmont has a lot of energy. Combine with development of hospital. Definitely hear the council. Have a sugar mill plan and Steam plan and understand the linkages. And how does this link to the east. Pull them apart and keep them together.

Peck:

What is STEAM

Erin: Science, technology... + maker space

Harold: University interest and model is shifting real time. Opportunities are changing and universities are changing. Mindful about what is the future of education and what that looks like 10 years down the road. World is changing quickly in education and employment.

Hidalgo Fahrig

- In a meeting with different organizations (Growing up Boulder and Trestle meeting). Museum, Library, etc and other partners. She spoke in her capacity as an educator and council member. Important for them to have a voice. What is culture, and what does being a Longmont resident mean? Hearing that this is a design challenge type of effort. What is most desirable and most sustainable use of that space? Excited to see what comes down.



STEAM Property Owners Meeting

January 7, 2022, 11:00-12:30am

Attendees

- **City of Longmont:**
 - Glen Van Nimwegen
 - Erin Fosdick
 - Hannah Mulroy, City of Longmont Environmental
 - Todd Fleming, Business Analyst with
 - Tony Chacon, Exec Director for Urban Renewal
 - Tyler Stamey, Traffic Engineer
 - Ben Ortiz
 - Kimberly McKee
- **Stantec / Trestle Strategy:**
 - Rhonda Bell
 - Nancy Locke
 - Marine Siohan
 - David Starnes
- **Property Owners / Developers / Stakeholders**
 - Erica and Jon Eggers: TFB LLC, with Jim Cobb, 1st and Main and 10 South Main
 - JD Parker
 - Jim Cobb: owns 11.5 acres south of 1st & Main. Had developer under contract, walked away after due diligence because of high costs and ways land would be divided up.
 - Michael Tignanelli with Global Pacific: developer who was under contract with Jim Cobb
 - Brian Bair: bought Butterball property in 2015 (2nd & Emery, 1st & Main). Almost 12 acres. Redeveloping area, finished 230 units, commercial, 61 units under construction right now. Starting additional construction as well. Wibby brewery included.
 - Andrew Dye: Corecivic, 236 Main St, purchased whole portfolio in 2016-2017. Helping folks getting out of prison to get them back on their feet.

Meeting Goals & Agenda

As you may know, the City recently initiated a sub area planning effort for the Sugar Mill and STEAM areas (see map below). This effort will build off previous community planning efforts to produce a detailed vision and development strategy for this area.

The project team, led by the City and Stantec, would like to meet with property owners, developers, and prospective developers in to better understand current and future plans. Please



join us virtually to discuss your plans and ideas. We will send out an agenda and questions for your consideration as we get closer to this date, but would appreciate if you can hold this time on your calendars.

While there will be a public outreach component of this planning effort and involvement of other project stakeholders and community members, this will be a working meeting to specifically discuss development plans for this area (rather than a more general community meeting). Please let me know if there are others from your project team that you would like me to include on this invite.

I hope you can join us on January 7th using the Microsoft Teams link included in this invite. In the meantime, if you have questions or ideas you'd like to discuss, please feel free to reach out to me. I look forward to working with you on this exciting project!

Attachment

Meeting slide deck

Meeting Notes

- David: What are you contemplating for the area?
 - Michael Tignanelli: need for townhomes in the market; we are contemplating multiple apartment buildings, some of which are mixed use, market rate apartments. We're seeing declining growth around the nation in housing, migration plays a big role in Colorado. We were evaluating this for apartments primarily and also some townhomes.
- Rhonda: just a note that we have over 60 different property owners in the study area (45 in the STEAM area)

Challenges

- Jim Cobb: on your 2019 diagram, my property is showing as multifamily. I'm a little confused about whether you are planning for multifamily housing or STEAM uses.
 - Rhonda: this 2019 diagram is not meant to guide any development and is completely flexible and open for discussion.
 - Nancy: this drawing was not done as part as this planning effort, it was done as a brief visioning process done in 2019.
- Jim Cobb: one of the first things the City should look is improving the infrastructure. When you load one development with all the initial infrastructure costs, it stops the development. Maybe the City could make all the improvements with a fee system on the different properties.
 - Nancy: I think it's a great comment and a very real concern, and we can look at that.
- Brian Bair: in 2013 we started developing in RiNo; none of that would have happened if the City hadn't put in infrastructure along Brighton. It is a real concern, how to deal with infrastructure, who pays for it, how does it get allocated over time, etc. I'd like to have a



roadmap for that piece, and then be less prescriptive on what the verticals are. I am almost 100% leased at Emery, Main St is a little more challenging.

- Nancy: Is there a physical difference between how the spaces are arranged on Main St and at Emery?
 - Brian: no, they are very similar.
- Brian Bair: how to mitigate taxes is another challenge that's in the mix. Costs and land costs are high. All of these things are limiting the ability to develop.
- Jim Cobb: mitigation of flood plain seems to be dragging on.
- Nancy: how possible do you see providing for the missing middle is?
 - Brian Bair: we're doing it. Institutional capital is pretty siloed making true mixed use, mixed type developments very difficult to implement. Have to solve for larger properties.
 - Michael Tignanelli: all the requirements from the City don't pencil down on the proforma. No large institutional capital group will fund public projects for zero or negative returns.
 - Nancy: I think the hope with a sub area plan is to get a cohesive vision for the area and recommendations to the City.
- Nancy: are there items specifically that you would like to see implemented going forward?
 - Brian Bair: predictability and consistency is key. Planning timeline is very slow compared to development timeline. It would be nice to create a plan so everybody can develop into it. I love the concept of the whole town connecting down to the river, we would need to get that set in stone so to speak.
- Brian Bair: flexibility within the zoning is very important as things can change over time. Being able to work out changes with staff is very important. Put the framework in place and let the market do the rest.
 - Erin: are there still things specifically that we ought to look at in the code?
 - Brian: I'm not a code expert, but I would say things like parking regulations, mix of uses, etc.

Housing Gaps in Longmont

- Rhonda: we're really interested in attainable housing.
- Brian: I think all of it is needed. But it's hard to start thinking about this when costs and fees are prohibitive. We don't do affordable housing at all, so we approach projects on the market rate standpoint.
- Michael: missing middle and density is a little bit of a cat and mouse issue.

Amenities

- Rhonda: are there any kind of amenities you would see in the area?
- Brian Bair: I haven't read the performing art center report.
- Michael Tignanelli: our group purchased a property in Canada with a performing art center, but it wasn't profitable. A more flexible event space may be easier to manage.
- Brian Bair: public services more data driven. We've always thought that the proximity of downtown is the community amenity. Connecting STEAM and Sugar Mill to the Old Town would be a great win where everything is walkable and accessible.
- Rhonda: what about connecting south to the green space?

Next Steps

- Erin: send out the slides. Please share any additional thoughts on any of the questions or anything else, we'd like to continue the conversation.
- Erin: we'll invite you to conversations we'll have with the DDA as well.

Project Goals Survey

SURVEY RESPONSE REPORT

13 February 2019 - 31 January 2022

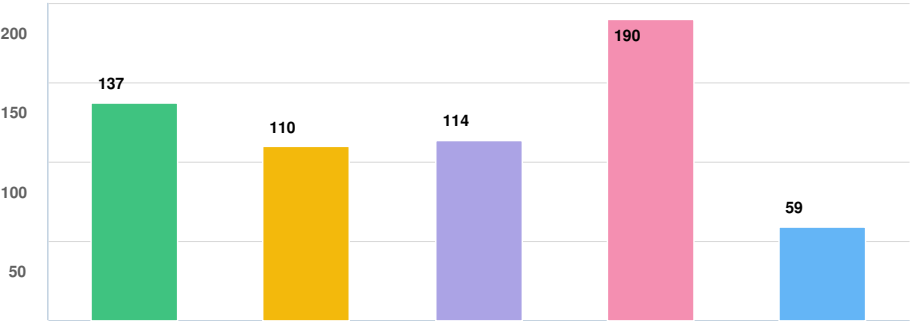
PROJECT NAME:

Sugar Mill + STEAM Sub Area Plan



SURVEY QUESTIONS

Q1 HOUSING | GOAL: ENCOURAGE A RANGE OF RESIDENTIAL OPTIONS WITH A VARIETY OF HOUSING TYPES AND PRICE POINTS.What type of hous...



Question options

● Condos and apartments ● Senior housing ● Tiny homes ● Townhomes ● Other (please specify)

Optional question (254 response(s), 0 skipped)
Question type: Checkbox Question

Q2 TRANSPORTATION | GOAL: PROVIDE FOR MULTI-MODAL REGIONAL TRANSPORTATION CONNECTIONSWhat facilities could the site integrate ...

OPTIONS	AVG. RANK
Bike connections	2.57
Passenger train	2.90
Connection to a Bus Rapid Transit system	2.94
Bus station	3.10
Car share	4.33
HOV lane connection	4.88

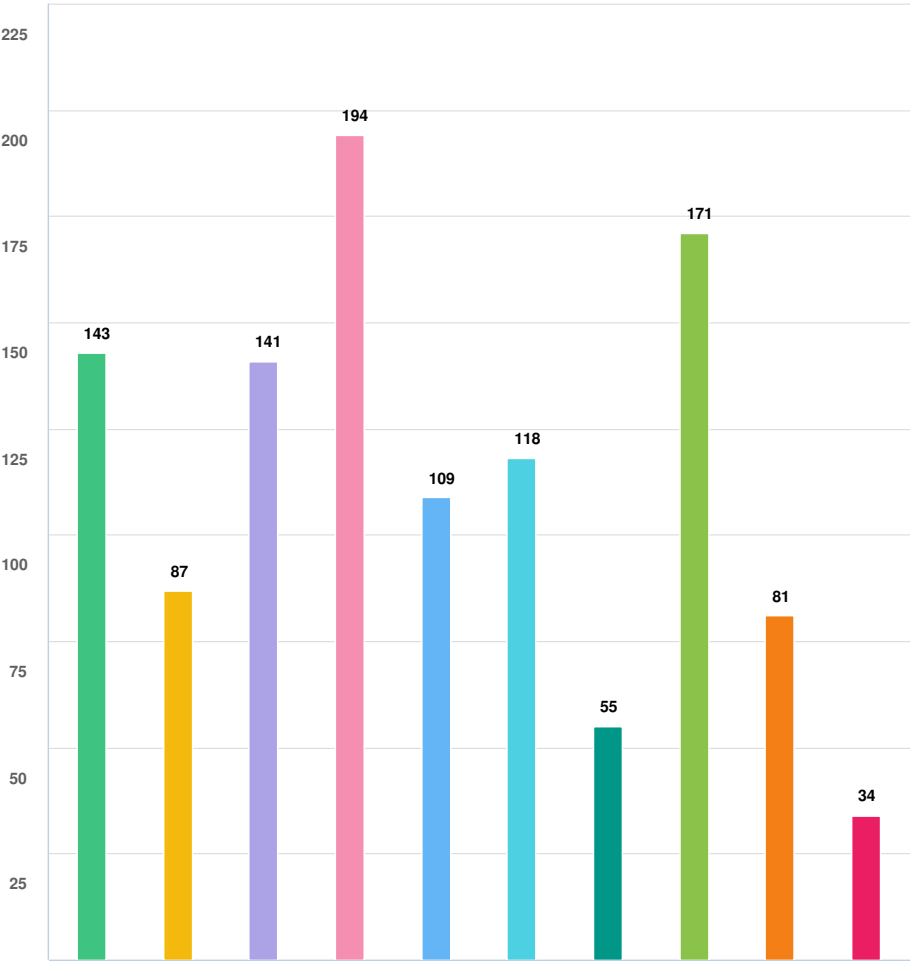
Optional question (247 response(s), 7 skipped)
Question type: Ranking Question

Q3 DEVELOPMENT | GOAL: ENSURE CONNECTIVITY BETWEEN THE STUDY AREA AND THE BROADER LONGMONT COMMUNITYHow should the study area connect to the broader Longmont community? Rank the following from most important (1) to least important (5):

OPTIONS	AVG. RANK
Pedestrian connections (e.g., sidewalks, multi-use trails, hiking trails)	2.04
Bicycle facilities (e.g., multi-use trails, on-street bike facilities, separated/protected bike facilities)	2.40
Public transit (bus, train)	2.95
Roads for cars	3.55
Shared mobility programs (e.g., short-term car rental, bike- and scooter-sharing programs)	3.95

Optional question (248 response(s), 6 skipped)
Question type: Ranking Question

Q4 COMMUNITY | GOAL: INCENTIVIZE AND ENCOURAGE THE IMPLEMENTATION OF CULTURAL/ARTS FACILITIES AND COMMUNITY HUBS FOR CREATIVITY AND INNOVATIONWhat kind of facilities or uses do you envision as being the most appropriate for a cultural hub or center fo...



Question options

- Art gallery (contemporary arts) and/or shared art experiences
- Conference center
- Interactive education and learning experiences
- Live music venue
- Maker's space
- Multicultural center
- Museum
- Performing arts center
- Research center
- Other (please specify)

Optional question (252 response(s), 1 skipped)

Optional question (250 response(s), 1 skipped)
Question type: Checkbox Question

Q5 SUSTAINABILITY | GOAL: FOCUS LONG TERM EFFORTS ON SUSTAINABLE BUILDING AND DEVELOPMENT PRACTICES What does sustainability mean to you as it relates to development in this area? Rank the following from most important (1) to least important (6):

OPTIONS	AVG. RANK
Natural world (habitat preservation, compact development)	3.16
Land use (smart growth principles, urban agriculture, adaptive reuse, etc.)	3.22
Climate and risk (reduced emissions, flood/disaster resilience)	3.28
Quality of life (preserve historic/cultural resources, create community, improve equitable access to housing and employment)	3.41
Mobility options (pedestrian and bike facilities, micro-mobility, transit services, electric vehicle charging, etc.)	3.83
Resource allocation (adaptive reuse, recycling/composting, waste reduction, alternate energy, energy/water conservation, green building)	3.93

Optional question (250 response(s), 4 skipped)
Question type: Ranking Question

Q6 Is there anything else you'd like to share about the Sugar Mill + STEAM project and its goals?

Anonymous

1/13/2022 04:32 PM

Please preserve as much of the Sugar Beet Mill and its history in this town. I grew up in another sugar beet town in CA. It was really special to find this place that had the same history. Why is it not celebrated more? Why is there no museum about it here? Idea: Bring the climbing industry and climbing tourism to town. Turn the silos into climbing walls inside. That would be really cool - super tall, if not tallest top-rope, man-made, interior climbing walls, accessible year-round... With a name and logo harkening back to the sugar beet days and times. something like that...

Anonymous

1/14/2022 09:21 AM

This is an opportunity to do something really awesome, and I appreciate the extension for community insight.

Anonymous

1/14/2022 09:32 AM

Fix the homeless issue before overdeveloping. More housing isn't the answer, stop letting them camp out all over. Stop with the urban sprawl and apartments

Anonymous

1/14/2022 09:38 AM

Bike access and managing population density are my largest concerns over the development of this space. I lived in Boulder before Longmont, and people density and overcrowding pushed us away. We love this area today (Wibby, etc.) and though welcoming the growth and new opportunities, I am hopeful the population will remain manageable.

Anonymous

1/14/2022 09:46 AM

Always thought the mill was a beautiful property, can't wait to see it revitalized! My vote would be for new developments to have the same brick/industrial feel (in other words, the look of the South Main apartments wouldn't fit with the mill aesthetic).

Anonymous

1/14/2022 10:07 AM

This opportunity is made to order to make Longmont a place where people want to settle down. A performing arts/convention center with bring much needed dollars into the community and encourage hotels and restaurants to accommodate the people using the facilities. Other cities in the country have revitalized their downtown areas for the benefit of all. We can learn from all of them.

Parkside Studios

I see Longmont flourishing from this development in a couple of

1/14/2022 10:11 AM

ways. On main street, I see that as the area where a lot of the culinary aspects of the town will continue to grow. The town has a rich history in farming, and I think we should lean in to the farm to table movement. Meanwhile, the Sugar Mill and STEAM district is where the culture and arts can continue to grow. As a composer, I've worked with and love the pool of talent in Longmont and the surrounding areas, so I could see more opportunities to show that off in either a music venue or performing arts center. And on the affordable housing front, we do need to make Longmont more affordable to live in. The tiny homes they've built over by live on the south side of Longmont are well built and a great use of space for their size. I could see more of these in the Sugar Mill area, especially if they're surrounded with opportunities to reach main street and any cultural areas nearby.

Anonymous

1/14/2022 10:17 AM

Less chains, more small businesses! Variety would be nice. Longmont has a lot of coffee and tacos. Maybe something else?

Anonymous

1/14/2022 10:28 AM

Those of us that live on the East side of town want some good local (non national chain) restaurants and breweries/bars. Please incorporate some restaurants, cafes, breweries, pubs!

Anonymous

1/14/2022 10:36 AM

This is prime real estate with location and mountain views. I would love to see it developed into an area that will support the growth of Longmont

Anonymous

1/14/2022 10:38 AM

This is an opportunity to expand and build upon the vibrancy of our community. I would love to see a wide variety of opportunities there (art, education, technology, nature) and the means of people from a variety of income backgrounds to visit and live there.

Anonymous

1/14/2022 10:42 AM

Not at this time

Anonymous

1/14/2022 10:50 AM

I'd like to see goals include decreasing light pollution. Lights can be minimal and pointed downward so as to be less of a wildlife concern.

Anonymous

1/14/2022 10:53 AM

Housing options for first time buyers. No more rental units/condos

Anonymous

We travel a lot and see some amazing development in other parts of

1/14/2022 10:59 AM	the country that have taken abandoned areas or run down areas and turned them into amazing community development projects. I would love to see a larger, full time farmers market to support our local artists and growers. Here are a couple of our favs: https://atpearl.com/ https://www.gatheringplace.org/
Anonymous 1/14/2022 11:24 AM	Please offer sale of homes.. mixed community with seniors.. walkable village..
Anonymous 1/14/2022 11:25 AM	I work in the area currently and it is super important to me to have biking/walking/ trail options for commuting from other parts of Longmont. Currently it feels unsafe to do so.
Anonymous 1/14/2022 11:55 AM	I find labeling this project "STEAM" to be extremely odd from a development standpoint. I would be interested to know what industries, types of development, etc. have expressed an interest in the area - not what agenda the current city council has for it. In addition, Longmont already has some of these things that have developed more organically or the community has specifically asked for - tax increases for the school's Innovation center and mobile unit, Tinker Mill, etc. Please ensure whatever happens enhances what's already existing - not duplicating. Forcing it is going to make it unsustainable.
Anonymous 1/14/2022 11:59 AM	This development should create both housing AND jobs. It should not be an isolated "island" - it needs restaurants, stores, living spaces for seniors and families along with access to all bike trails. Ideally, it provides a hub for a train to DIA and/or Boulder to reduce traffic overall. Our town's traffic is becoming a nightmare, and continued housing just accelerates the issue.
Anonymous 1/14/2022 11:59 AM	Please prioritize bike and pedestrian mobility over cars as these modes of transportation improve the community in many ways. Also please try to preserve meaningful space for the natural environment as we've already permanently destroyed enough of it.
Anonymous 1/14/2022 12:02 PM	There seems to be an emphasis on STEAM but very little mention of any STEAM related facilities or activities in the report or any articles about this subject. Curious why that is and how to get involved. I've sent an email to Tony per one of the articles.

Anonymous 1/14/2022 12:41 PM	I'm excited for this to hopefully go forward!
Anonymous 1/14/2022 01:05 PM	would like to see the sugar mill structure remain
Anonymous 1/14/2022 01:26 PM	NO MORE HOUSING. HOUSING SHOULD BE REDUCED IN BOULDER COUNTY. LIKE BOULDER. Any new housing should be made only for East of I25/weld county. Keep disgusting looking condos and apartments away. The sugar mill is wayyyyyyy more enjoyable to look at than some whitewashed apartments/houses/condos designed by some some kind of back east builders with no style. No more traffic! Build a gosh darn train station to Denver/Fort Collins already!! Thank you. From your favorite Colorado and Longmont Natives.
Anonymous 1/14/2022 01:41 PM	An art gallery that incorporates/surrounds a facility for vertical farming research and the latest solar technology would be ideal. Throw in a few cellos, violins and other instruments, and it could be a magnificent center. What impressed me in my youth was new architecture sprouting up amid Europe's old structures making a striking urban picture. An IM Pei inspired structure? Composting and alternative energy would be components. Check out vertical farming in Jackson's Hole, WY. https://verticalharvestfarms.com/
Anonymous 1/14/2022 01:50 PM	I work at StickerGiant and drive through the area every day. Anything we can do to make it a usable space and not just a trash building would be amazing!
Anonymous 1/14/2022 02:06 PM	This is what I included in survey for the Riverwalk development, and I feel the concepts are also relevant for the Sugar Mill area... Community Partnerships: To truly create an epicenter of science, technology, education, arts and entrepreneurial-ism, I envision an environment where Longmont is viewed by its residents and also across the nation, as being a culture and resource destination for "The Art of Business and Design". Goals, Vision, and Possibilities: - Be inclusive: Develop Designers, Design Thinkers, enjoy culture, and create jobs. I once heard a person mention a goal about assisting homeless people to learn technology and to become designers of 3D models. That could be an amazing success to pursue. - Provide a community art gallery to fill the void of the lost Arts Longmont "Muse" gallery. It could provide opportunities for life drawing, exhibits, and perhaps even a maker/ design development space. - Create "Longmont ADD"- Arts and Design Destination: Stroll (think of San

	<p>Antonio), visit galleries, share a meal, sip a beverage, plan a session in a maker space, etc. Develop an innovative and creative environment which encourages cross-generational, diverse culture and wide income range community participation. - Enable an interactive / immersive art environment, similar to Meow Wolf or Boulder's Wonder Wonder. Perhaps be an incubator for the upcoming Denver Meow Wolf or for exhibits at the Longmont Museum. Provide workstations with open source online STEAM learning, or pay per usage industrial software licenses. Centralize community tech hardware, licenses, volunteers, paid staff, etc. - Be a gathering place for STEAM mentors, a Showcase for SVVSD and/or EBCA Longmont Studio Tour opening receptions. Refine a tradition and an arts culture within our community... Be a destination for professional design workshops (See Gnomon School in Hollywood, Boulder's Dairy Center, Loveland's sculpture identity, or Fort Collins' Lincoln Center). - Develop and strengthen partnerships, support, and business models: Longmont Start Up Week & Incubator space Maker Spaces (TinkerMill) SVVSD (Innovation Center, resources for district Art and STEM/ STEAM teachers) Longmont Artists' Guild Longmont Quilt Guild Boulder Potter's Guild EBCA Defunct Arts Longmont Longmont Museum courtyard, exhibit rooms and Stewart Auditorium Longmont Library- 3D Printers Maker Boulder STEAMfest 4H Scouts Private Schools IBM & SVVSD P-TECH, P-TEACH programs Etc. Thank you for considering a variety of ideas. I look forward to learning of Longmont's next steps on this topic. Sincerely, Tim King</p>
Anonymous 1/14/2022 02:20 PM	Low income housing, retail stores, in n out
Anonymous 1/14/2022 02:23 PM	Many sub developments I've seen self-isolate by restricting the number of exits to other divisions and areas. This creates traffic jams, delays, restricted emergency vehicle access, and slow evacuation egress if necessary. Please don't make this mistake here.
Anonymous 1/14/2022 02:26 PM	A wonderful, pretty safe walkable area with mixed use (food, activities, housing) that brings in young people and families in and with an ordinance that bans vagrants, panhandling, etc.
Anonymous 1/14/2022 02:30 PM	n/a
Anonymous 1/14/2022 02:42 PM	Concerned about the area becoming congested.

Anonymous 1/14/2022 02:45 PM	It would be great to see an area dedicated to the Longmont experience, where people would be able to spend a significant amount of time going from one experience (restaurant, gallery, music venue, brewery, tasting room, creative experience, shopping, etc...) to another. The experiences would keep people in the area, they would not have to go elsewhere.
Anonymous 1/14/2022 02:45 PM	Make sure to involve community creatives in the development and implementation processes, not solely community leaders and business experts.
Anonymous 1/14/2022 03:25 PM	Can't wait to see that wasteland turned into something beautiful and beneficial to the city of Longmont.
Anonymous 1/14/2022 04:23 PM	Performing Arts complex included as part of a conference center would be wonderful. Please make sure to confer with local music organizations to understand the needs.
Anonymous 1/14/2022 05:18 PM	I'd love to see the preservation of the landmark of the sugar mill stack as a beacon of the past. Also a mix of housing, retail, dining and entertainment would be so valuable to the community with narrow streets and walkable areas, like that of a small ski village.
Anonymous 1/14/2022 05:20 PM	I am excited at the prospect of this area being reimagined into a hub that honors its history while providing services to the greater community. As someone who lives nearby, I always hoped that this area could become a beautiful and impactful entry to our town. I remain hopeful that this project will come to fruition!
Anonymous 1/14/2022 07:00 PM	I realize it's tempting to build all types of housing for financial reasons, but please take into consideration that long time residents (homeowner for over 30 years) live in Longmont because of the open spaces and small town feel.
Anonymous 1/14/2022 07:23 PM	By limiting the use of cars and parking you are isolating this portion of Longmont. All of the cutesy trails for pedestrians, bicycles etc. are only going to make it harder for small business to succeed.
Anonymous 1/14/2022 07:28 PM	Please bring more restaurants and bars to east longmont and more bike paths!

Anonymous 1/14/2022 08:45 PM	Please preserve the sugar mill. It played a significant role in the history of Longmont
Anonymous 1/14/2022 09:21 PM	PLEASE DO NOT PUT IN AN OUTDOOR MUSIC VENUE. THE NOISE TO NEARBY RESIDENTS WOULD BE HORRIBLE
Anonymous 1/14/2022 10:01 PM	Please don't cram the area full of building like so many other developments. Park, open space and habitat for animals makes these new areas so much more pleasant. Smart, slow growth will be more sustainable
Anonymous 1/14/2022 10:13 PM	Study The Villages Florida as a senior village plan.
Anonymous 1/15/2022 01:22 AM	Make the entire area a public park.
Anonymous 1/15/2022 06:47 AM	I have lived in Longmont since the early 70's. Our town has changed so much lately and the continued growth is taking away the quaint city I once loved. Hardest for me to swallow though is to see how our prairies are being ripped out without a care. Landscapers keep covering our valuable soil with concrete, landscape fabric and rock which is terrible for so many reasons. That rock scenario is too hot. We should be trying to cool our planet not make it worse. Second, it's not sustainable. What happens to that landscape fabric in 30 years? It's basically an environmental nightmare where neither the bird nor worm can find food. Also, where are the insects supposed to live in that? I beg and impore you... when you develop that area, please consider our planet, the wildlife and us. Plant native. Native plantings can be quite stunning. Native plantings reduce water and pesticide use. Native plantings harbor insects which the animals eat and most importantly provide a sustainable habitat for bees which we all know are struggling to survive due to loss of habitat and pesticide use. Native plantings of flowers and grasses would be spectacular around Sugar Mill. They would look like they belong because they do. Feel free to call me (720-635-2544) if you don't know how to accomplish this. I have 40 years of experience in landscaping and now specialize in pollinator friendly and edible gardens. I would be more than happy to volunteer.
Anonymous 1/15/2022 07:38 AM	I would love to see an arts and cultural center in this area with a belief that arts are an integral and valuable avenue for developing shared

	values and practices in health and wellness
Anonymous 1/15/2022 07:52 AM	Connectivity to the rest of the state. Dining, retail, fun community atmosphere.
Anonymous 1/15/2022 09:08 AM	Parking is important. Every project downtown eliminates parking and parking and the rebuilds with not enough parking. Parking has become critically bad in south downtown. Please include enough parking for the uses of the space. The new apartments south main do NOT have enough parking to sustain businesses in that building. Think of this: that all you do will fail if you don't have people there to support it. And people won't bother if there isn't parking.
Anonymous 1/15/2022 09:16 AM	I prefer the idea of redeveloping this property into multiple uses that includes residential, historic preservation, cultural such as art and music , sustainability through means of transportation options, and perhaps light industrial that includes agribusiness. This to me seems exciting and creative and would be more democratic for the city meaning it could be attractive to many different people for many purposes including entrepreneurship and community. The idea of a hotel and performing arts center seems stagnant and relies on resources outside the realm of the city, such as tourists and attracting performers. Or, if it was developed solely as residential, that would be boring. In conclusion, I think the people of Longmont would really like to see something happen with this property and the more people it can benefit the better. Good luck!
Anonymous 1/15/2022 10:13 AM	Really disappointed that we voted for a train, money was collected and yet nothing has been done. Now we are hearing it is too expensive? Bureaucracy should get in the way of the will of the community.
Anonymous 1/15/2022 11:15 AM	It would be nice to have more restaurants on that side of town
Anonymous 1/15/2022 11:20 AM	This would be the ideal area for the Performing Arts and Conference Center.
Anonymous 1/15/2022 01:50 PM	Please no more artificial malls or ugly apts like the ones at 2nd and main!!!!

Anonymous 1/15/2022 04:16 PM	This will be difficult because of the rail freight activity. Move the railroad out, or focus on another part of the city.
Anonymous 1/15/2022 07:52 PM	Please please please please high density mixed used residential/commercial. We need density
Anonymous 1/15/2022 08:17 PM	Affordable housing is a must!
Anonymous 1/16/2022 07:38 AM	Longmont really needs a finance / business development district, business growth, housing and area clean up with flow naturally out of that. Right now there isn't any concentration of that but if you go into Boulder or Denver it is and you can see development flow out of that
Anonymous 1/16/2022 09:15 AM	I've lived in Longmont my entire life (I'm 46). The changes to this town have made home ownership impossible for half the population here. You ask for opinions but you really don't care what people want or need. The north end of Main Street and the south end are two completely different places. Crime rates keep rising, open drug use in city parks, homeless drug addicts everywhere. But by all means, build your overpriced apartments, Starbucks and yoga studios. This is the Bouldering of Longmont. Welcome to CaliforniaRado!
Anonymous 1/16/2022 09:22 AM	This project should look to bring large cap companies to longmont that'll bring more jobs to the area.
Anonymous 1/16/2022 11:34 AM	All housing should be accessible to those earn the median income or less.
Anonymous 1/16/2022 12:42 PM	I'd like to see smart and graceful integration between outside and indoor spaces and the various natural and urban features. For example: an outdoor gathering/presenting space (e.g., amphitheater) instead of a large enclosed ballroom; a sustainable garden/urban farming space adjacent to the natural riparian corridor that affords for multi-use water projects; or a long circular path surrounding project that could be used for families taking a stroll or non-motorized commuting from housing to work-place.
Anonymous 1/16/2022 12:52 PM	This could be another opportunity for Longmont to lead the way in helping our residents in a sustainable and even profitable way, much like the NextLight project. Please do not let this turn into another

	enclave of the rich like Prospect.
Anonymous 1/16/2022 03:19 PM	I would LOVE a community with several types of housing options along with what they did in Lodo, when they developed the warehouses downtown. Turning them into multi use retail and marketplaces, art spaces and work areas.
Anonymous 1/16/2022 06:31 PM	Please, please conserve and focus on this significant historical area
Anonymous 1/16/2022 09:33 PM	We all know the city will do what it wants and screw it up in the process. There's no functionality to this idea unless you can really amp up the area and keep the historic value of the sugar mill intact.
Anonymous 1/17/2022 05:51 AM	Leave the space alone! Don't develop it at all. We don't have the infrastructure to support more development, we don't have the water to support it, we have way too much traffic already. Stop building and focus on dealing with the existing problems!
Anonymous 1/17/2022 03:30 PM	Leave the history in place. Do not tear down the sugar factory.
Anonymous 1/18/2022 07:28 AM	I hope the sugar mill isn't torn down, but preserved/reused in some way.
Anonymous 1/18/2022 12:45 PM	Longmont has a rare opportunity to create an area accessible to all making the city a good place to raise a family. Providing a performing arts/convention center will encourage people to keep there dollars here. Planning ahead for restaurants and hotels to follow would be smart. Adding a park would be a good idea for families to gather. Think in terms of 75/100 years from now when planning; not just the next election cycle.
Anonymous 1/18/2022 01:01 PM	The importance of the look and feel of spaces shouldn't be underestimated. Mixed use, interesting architecture (like prospect), and spaces to engage people. not just an empty field but spaces that encourage interaction and use.
Anonymous 1/18/2022 02:46 PM	We really need a functional arts center with interactive displays appropriate for differing age groups.

Anonymous

1/18/2022 06:44 PM

Community gardens!!!

Anonymous

1/18/2022 09:24 PM

Being able to have pedestrian/bike connections that are able to cross 119 safely is the most important. Additionally, having a bus to get to the bus stops to go to Boulder or the Park and Ride for Denver. I would also love to have a small library annex or community gardens included in public facilities.

Anonymous

1/18/2022 10:38 PM

Very excited to see how the space transforms!

Anonymous

1/19/2022 01:18 PM

Plenty of trees and open space.

Anonymous

1/19/2022 05:57 PM

I believe and support things like habitat preservation, but this area strikes me as one that had it's habitat severely destroyed many years ago.

Anonymous

1/19/2022 07:00 PM

Please make actual affordable housing not a 2 bedroom for \$2000+

Anonymous

1/19/2022 07:52 PM

My granddaughter (who is now eleven) has been interested in the Sugar Mill since she was three. Here, in Madison's words, is her vision for Madison's sugar factory, as submitted to Times-Call. Ever since my granddaughter turned three, she has been acutely interested and mildly obsessed with the sugar mill/factory. We drive by it at least weekly on our way to the Rec Center and a discussion always ensues. She began her vision saying she was going to grow up and be the President of the Sugar Mill, but time has peaked other interests for her. A recent story in the Times Call regarding a fire has spurred her to begin taking action and revealing her vision. So below, please find her concept that she feels compelled to put into words. Madison Jayd Wagner's revelation as dictated to her Nene, Ginger Fleegel. "When I was very young, my Nene and I drove by the big factory all the time and we talk about what happened to it. I get so sad when I hear about the fires and the kids that are trespassing on it. Something needs to be done before someone really gets hurt or dies. My dream is to turn the mill into a candy factory that is surrounded by a hotel, in the shape of what the silos look like now, only with windows facing the mountains. I'd also like to have a bed and breakfast there too, but each door leading into the room would have a theme with candy on it, like sugar canes, Hershey kisses or M&M's.

The inside could have some decorations inside that would match the doors. I will also build a swimming pool that can be both indoor and outdoor. There would be a Lazy River that runs around the outside that has paddle boats that people can paddle to places to eat. I would add another lazy river, but this one on the inside of that one that is covered and heated where people can just float. Since everybody in Longmont has a pet, I think we should have a Veterinary Clinic and doggy day care place. Also, since adults like to shop, I will invite Macys, Dillard's & JCPenney to build there too. I am only seven, but I keep getting more ideas all the time. If someone would let me team up with them, I think we can add lots of jobs. The name of my place would be Longmont's Sweet Sugar. P.S. I live in Thornton but my Nene says that doesn't matter."

Anonymous

1/19/2022 09:46 PM

I would like for no one, person, company, or other to own multiple properties, more than 1, at the location so as many people have the chance to own their housing so it doesn't end up being a majority of all for rent location

Anonymous

1/19/2022 11:32 PM

Adaptive reuse of the sugar mill is crucial@

Lfr12

1/20/2022 12:16 AM

I would love to see high density mixed use with a pedestrian and/or bike focused infrastructure in this area. I would especially like an adaptive reuse of the Sugar Mill itself. It is an iconic Longmont structure and could become a centerpiece for our town in redeveloped creatively.

Anonymous

1/20/2022 03:29 AM

Excited for this area to go from blighted to thriving, appealing, and vibrant!

Anonymous

1/20/2022 10:57 AM

Longmont has tons of taco places and coffee shops. Add something new! Variety. Locally owned, no chains.

Anonymous

1/20/2022 07:42 PM

Please keep the Sugar Mill!!!

Anonymous

1/20/2022 07:48 PM

Provide a part of land/ park to remember the sugar mill/ steam

Anonymous

1/21/2022 10:27 AM

Please ensure history and natural surroundings are top priority when revitalizing the area.

Anonymous 1/21/2022 12:13 PM	A large multi use space would be great for events as well as emergencies
Anonymous 1/21/2022 01:52 PM	Maintaining historical landmarks and preserving open space is important to me in Longmont, specifically in this area. I am afraid to see everything wiped out to put more and more over priced apartments and traffic in town. Creating a thoughtful community space that respects the town's history and natural beauty would be an invaluable use of this space.
Anonymous 1/21/2022 02:44 PM	I would like to see some adaptive re-use of the facilities and the materials that can be re-purposed. There are a lot of bricks on the site it would be nice to see them somehow re-used instead of thrown in the dump like they did with the old Twin Peaks Mall. That was a shame and an incredible waste. If that is what they are going to do I would just rather see it stay the way it is.
Anonymous 1/21/2022 02:52 PM	Nature-based play areas for kids with year-round (fans/ misters in summer and heaters in winter) outdoor community gathering spaces for parents and adults feels urgent
Anonymous 1/21/2022 03:41 PM	It would be nice to see this become a neighborhood of Longmont that values sports, arts, small business, and affordable housing. This area does not need to be just another development with apartments, townhouses, and single family homes. This is a real opportunity for the City to think about the people of Longmont and what makes them happy to be in Longmont.
Anonymous 1/22/2022 12:27 AM	Local community activist in Old Town. Glad to get involved in other meetings on this area. There is a way to weave what I've chosen here.
Anonymous 1/22/2022 06:16 AM	I think combining the Sugar Mill area and the STEAM area into one area is awkward at best. The area along main st (perhaps as far east as martin, but more reasonably emery or collyer) should clearly be planned as part of downtown development. the rest of it should be thought of as a separate area.
Anonymous	I would love to see a design that was walkable, bikeable at the center

1/22/2022 07:05 AM	but connected easily to the main car arteries.
Anonymous 1/22/2022 08:22 AM	I hope that is can also bring small local businesses such as restaurants, coffee shops, or more fun options escape rooms, etc.
Anonymous 1/22/2022 08:22 AM	I feel this should be an entertainment hub that brings the city together with special events etc. An outdoor amphitheater would be great for live music. The ability to access this area through walking & biking paths - along with public transportation would greatly cut down on the traffic in our area.
Anonymous 1/22/2022 09:46 AM	Connectivity with all surrounding neighborhoods and areas (north, south, east, and west) is essential for avoiding congestion and isolation and for creating a vibrant, multifaceted development that becomes a healthy and resilient part of our community.
Anonymous 1/22/2022 11:02 AM	I'd like to see a new area of town that builds things we don't already have in Longmont. Namely, a place for tweens and teens to congregate without being bored and getting into trouble. A place where teens, young parents and elderly can go without the usual Longmont onslaught of homelessness, drugs & crime. No tents, sleeping bags or camper parking. (Stop INVITING them and they won't come!) A place that draws the community because it's beautiful and friendly and offers lots of unique things to see and do. This is a great opportunity to build a whole new vision.
Anonymous 1/22/2022 05:31 PM	This gigantic development is a scheme mostly set up to benefit a few developers who bought county agricultural flood land at dirt cheap prices, got the city to rezone it and pay for flood mitigation. It will benefit the developers, real estate, banking, building, and legal industries far more than the citizens of this town.
Anonymous 1/22/2022 06:49 PM	Whatever the ultimate design may be, I would like for it to have a distinct quirky charm, not sterile and minimalist.
Anonymous 1/22/2022 09:04 PM	I would like to see parts of the skyline preserved (Sugar Mill)
Anonymous 1/23/2022 10:31 AM	Need something more than just low income housing and a lot of parking lots. Something well thought out and unique. Also, no strip malls and minimum commercial.

Anonymous 1/23/2022 12:55 PM	Why can't it be painted white? It's a building owned by some one, he should maintain it, just like a home or housing complex
Anonymous 1/23/2022 02:02 PM	You sound like a bunch of old Boomers who still think that the future is dominated by giant corporations and banking interests who can call all the shots and control projects with their economic clout. You have clearly decided that hiring a bunch of technocrats to make all the decisions and to put in tech heavy surveillance, smart-city infrastructure, is THE WAY to go. More and more research is demonstrating what an enormous waste of resources this approach results in. Have you met any Gen Zs? They'd rather chew off their own hands that do anything by the rules of your decaying world.
Anonymous 1/23/2022 09:51 PM	Recruit and incentives for local businesses rather than chains or national companies.
Anonymous 1/23/2022 10:59 PM	Don't tear down sugar mill and associated buildings. Refurbish and use them as historical sites. If for transportation, use as an actual train station for the I25 corridor passenger trains that have been theorized forever. For bikes and scooters etc. Weave a path through, landscaping and historical plaques. Sugar Mill is very historic to this town, as was the small one on Ken Pratt Pkwy, before it was burnt down by some person for no reason. Don't build more apartments... The turkey plant site, and the areas you are building around that area are good enough.
Anonymous 1/24/2022 08:31 AM	I would love to see some of the old buildings used for artists' working spaces. It would be a great place to institute a ceramics/pottery venue. Is it possible to reuse any of the materials (bricks) in rebuilding? Could there be a community hands on effort to reuse materials over time? Loveland has placed a high priority on encouraging artists to live/work in the downtown area and has a vital artist scene with lots of live music, exhibits, etc.
Anonymous 1/24/2022 10:53 AM	Will the Longmont Museum move to this site? It is Longmont's hub for creativity and innovation.
Anonymous 1/24/2022 12:30 PM	Green space and agricultural development. Not dense housing...we have enough of that. Small houses with yards for families. traffic congestions is a concern all over Longmont.

Anonymous 1/24/2022 01:47 PM	None
Anonymous 1/24/2022 06:49 PM	So glad this area is finally being developed! It has so much potential for being a landmark with the history of the sugar industry, the justices buildings and the views of and from the site.
Anonymous 1/24/2022 11:01 PM	Do not fill up this area with buildings. Establish multiple car-free zones, also consider a small urban forest, community garden(s), and similar natural spaces. I would seriously consider creating "Sugar Mill Mountain" to contain the rubble from all the derelict buildings (excepting silos) and site remediation to create a signature geographic feature for the development and a unique hiking/biking destination. The view of the St. Vrain Valley would be stupendous.
Anonymous 1/25/2022 02:06 PM	This is a VERY exciting development. As a relatively new resident of Longmont I have always envisioned possibilities for the old Sugar Mill. It could be a revitalized live, work, shop, eat type area. How wonderful would it be for Longmont to be a beacon in the preservation and revitalization of this historic site!
Anonymous 1/25/2022 04:58 PM	Strong desire to enhance the Longmont Museum presence into the project
Anonymous 1/26/2022 12:08 PM	We already have a wonderful museum and cultural center in Longmont. How will this connect to that and build on it?
Anonymous 1/26/2022 12:09 PM	I am interested in the Growing Up Boulder contractor project getting feedback from kids in the community! So glad this is part of the development plan!
Anonymous 1/26/2022 12:52 PM	Any developments to this area should have the foresight to ensure Longmont remains affordable for all people, of all backgrounds and incomes, and also keeps pedestrian dignity as a top-tier objective. I can easily see this project becoming a quaint wealthy-elite hub, further driving people away from this city.
Anonymous 1/26/2022 01:08 PM	Rent controlled living

Anonymous

1/28/2022 10:37 AM

I really think that an urban agriculture project connected to education, technology, and the arts would be an incredible asset to Longmont.

Anonymous

1/28/2022 07:49 PM

I favor "Concept 3," though I recognize the limitations of a bubble diagram. I know some of the committee that advised through ULI, and yet I wonder what was missed. For example, build for-rent first, if only to evade the NIMBY response by owners of residences there. It happens. Ensure that building heights in the western reaches are lower, to ensure unfettered views from the remainder of the parcels. I'd bet that parts of the site will not be fully remediable, and so must be relegated to parking, park, transit, or athletic uses. I did not see much here of the Auraria model (only the Tivoli reuse). There is a broader exemplar there. For a more "dressed up" central pavilion kind of focal point, I suggest looking into Faneuil Hall in Boston. Boston is not Longmont, but this thing, like Pike Place Market in Seattle, thrives. Xeriscape needed, if only to permit a water feature in a central spot. And don't forget a police substation nearby or within.

Anonymous

1/31/2022 09:59 AM

Focus on human interaction at street level, no blank facades at street level, perhaps very small storefront stalls for affordable local small businesses and community start ups.

Optional question (123 response(s), 131 skipped)

Question type: Essay Question

Q7 Please provide your name (optional).

Anonymous

1/14/2022 09:15 AM

Sean Mannion

Anonymous

1/14/2022 09:21 AM

Ashley Hopkins

Anonymous

1/14/2022 09:38 AM

Russ Willacker

Anonymous

1/14/2022 09:46 AM

Jack Fedak IV

Anonymous

1/14/2022 10:07 AM

judith watson

BREAKOUT ROOM 1: LAND USE & HOUSING

Sugar Mill + STEAM 3/2 Community Forum

MARINE SIOHAN MAR 01, 2022 08:07PM

Instructions

Goal 1 Questions

- What uses do you want to see in STEAM? Should it continue Main Street retail or create something new? How can it better connect to the river corridor?
- What land uses do you want to see in the Sugar Mill area? How does it transition to existing commercial uses on 3rd Ave?
- The sub-area plan is looking to diversify the city's residential options. Are there any types of housing that *should not* be considered? Why not?
- What places or projects have you seen that could inspire the study area? What parts did you like?

I would like to see more European models regarding mix use – ANONYMOUS

We need to make education the core of STEAM. Expansion or relocation of FRCC AND CU-extension. – ANONYMOUS

For Sugar Mill, we need to make our agricultural heritage the core of this area: farm, trade, malting mill, etc. STEAM and Sugar Mill are very different and should be treated differently. – ANONYMOUS

Very concerned generally about social equity w/this Goal 1. Specifically about the inevitable gentrification and impacts (displacement due to increased rents/cost of living) on the various people who live in this area now ---especially those who are in the margins economically -- the working poor – ANONYMOUS

This whole Goal generally: I am very concerned there is no goal addressing our desperate need for affordable housing - which needs to be more than 12% when using public monies to largely develop/build. – ANONYMOUS

*Missed the official presentation Wednesday and had never considered *keeping* the old SM structures, but their presence does provide an opportunity to anchor the Eastern property.* – ANONYMOUS

Goal 1: Diversify Housing

- Create density around transit, to increase access to bus and (future) rail
- Provide a diversity of housing – including 'missing middle' housing options such as duplexes, fourplexes, cottage courts and condos.
- Promote housing options that appeal to a full spectrum of life stages and family compositions.
- Create livable, green environments and shared public spaces for all scales of housing development.

Continue to have retail. I don't see the value of connecting to River Corridor as i am concerned about negative impacts of lights, noise and development on this sensitive riparian area as well as concern for safety due to fact there have been 11 flood events in this corridor and it will happen again--despite best mitigation. – ANONYMOUS

RE: land use want to see? I'd like to see this be functional in terms of small business, light industrial and housing – ANONYMOUS

RE: This whole Goal generally: I am very concerned there is no goal addressing our desperate need for affordable housing - which needs to be more than 12% when using public monies to largely develop/build. – ANONYMOUS

Very concerned generally about social equity w/this Goal 1. Specifically about the inevitable gentrification and impacts (displacement due to increased rents/cost of living) on the various people who live in this area now ---especially those who are in the margins economically -- the working poor – ANONYMOUS

Continue to have retail. I don't see the value of connecting to River Corridor as i am concerned about negative impacts of lights, noise and development on this sensitive riparian area as well as concern for safety due to fact there have been 11 flood events in this corridor and it will happen again--despite best mitigation. – ANONYMOUS

RE: land use want to see? I'd like to see this be functional in terms of small business, light industrial and housing – ANONYMOUS

Uses in STEAM

I would like that area to be more like an Arts District. Maybe boutique shops. Entertainment venues. Restaurants.

Love the image of movies projected on the silos, but there are too many loud hot-rods (119 and E 3rd) to consider outdoor entertainment a viable option. — ANONYMOUS

I would like to see a Nature Center built in the Steam area.

Agree with a Nature/Natural component, but a Center would need to be placed up on the 'lip' of the floodplain or it would defeat the purpose of all that recent landscaping. — ANONYMOUS

What do I want to see in STEAM?

Mix of retail on the bottom, continuing Main Street retail and housing on top. Not just retail, also eateries

STEAM Land Use -Mix of uses, retail, housing, maker space/light industrial/live work

Goal 1 Question 1

Townhouses --> more affordable housing options for those who want to stay in Boulder County, but are unable to afford single family housing units

Agree but everything's relative... what might be considered a more-affordable Townhome cluster will seem out of reach for the folks discussed living in Goal 1 (a single column to the left as I type this!). — ANONYMOUS

Housing in Steam

Live/Work spaces

Yes - more loft spaces would be good — ANONYMOUS

See above. Lofts close to Main = \$\$\$ - there would need to be higher density as we move East, right? trips, quads, quints, and sextiplexes? — ANONYMOUS

Creating something new #1

I feel like I'd love to see new things in STEAM area, there are so many things that Longmont needs. I'd like it to continue what we've seen with downtown's genuine character, but more creative uses. I'd like a lot of consideration paid to downtown not tanking with STEAM's success (people en masse abandoning downtown for the new area)

Visual connection to/from Main (ala Dickens) could make this an extension of the current downtown instead of an alternative to it (like Boulder's 29th vs Pearl). — ANONYMOUS

Steam: a big performance venue, sugar mill: more housing of varying types.

Answer: question 1

I'd love to see more retail opportunities, but also community uses like health/wellness, used book repositories, recreational spaces, etc.

Public Placemaking and Innovation at STEAM. Mixed Use, high density, river oriented.

car-free zone in higher density mixed use area

Shouldn't that be downtown? — MARCIA R MARTIN

Car-free downtown? Did I read that right? — ANONYMOUS

Q1 - STEAM use

It would be great to have a center that helps to develop new businesses, like the old EDC / Development Center. Bring together businesses that support tech start ups

How is a "Systems Thinking/ Multi-Solving" approach being utilized in this visioning process.

Transit oriented housing — MARCIA R MARTIN

STEAM - Q1

Retail is an older model - look for more experiences

High density live work and height to take advantage of the views, create density to support the multimodal system & the transient programs.

Height probably required to achieve the needed density, but won't that block the Westward views they'd hoped for from 3rd and 119? — ANONYMOUS

Activities

Need more areas open to public at all hours that are not shopping centers. Multi uses, with a farmers market feel for local crafts, health exercise group areas outside and public transportation hub.

not sure about 'all hours' though — ANONYMOUS

STEAM should be a denser, multi-use urban neighborhood with ground floor retail and housing targeting various incomes above, and very walkable. The Sugar Mill site should step down to a lower density but still mixed use and walkable, focused on one and two family homes. Maybe a small area specifically targeting artists. These need to be complete neighborhoods with unique design guidelines.

definitely support more walkability to /from residential areas to amenities — ANONYMOUS

Continue to have retail. I don't see the value of connecting to River Corridor as I am concerned about negative impacts of lights, noise and development on this sensitive riparian area as well as concern for safety due future flood inevitability -even with RSVP efforts unfortunately....

Question 2: Sugar Factory Area Land Uses

Creek Access & Railroad Spur

I heard mention of creating a type of Riverwalk, but the map shows NO access to the St. Vrain Creek.

Does the Railroad use the entire length of the spur that runs SE from Martin Street to E. Ken Pratt Blvd. (Hwy 119)? That cuts off access from the Sugar Mill property down to the St. Vrain Creek. If the spur could be shortened (no further than the east edge of the Wastewater Treatment Plant) the Sugar Mill project area could expand down to the Creek, which would be VERY beneficial.

Or could the RR spur turn to the south, past the water treatment plant? Somehow, having the Sugar Mill area actually go down to the Creek would be great. Music stages/venues could be in this area, with sound directed to the south (away from any residences). Also add another bike/pedestrian bridge over the Creek to connect with the trail on the south side of the Creek, making the area accessible to all those living south of Hwy 119, without driving.

Please consider the payoff of all that cement, light, noise and disturbance on wildlife/habitat. Riverwalks are nice, but I'd rather keep our greenway/riparian habitat and see evidence of actual wildlife vs. statues of what used to exist. — SAM IAM

Bike paths along Martin and all over the Farm could/should be reproduced to the north and along 3rd (as mentioned elsewhere in the presentation). — ANONYMOUS

land use - what wanna see

RE: land use want to see? I'd like to see this be functional in terms of small business, light industrial and housing

Sugar Mill area

A mixed-use area. Moving from South to North. Some apartments and then transitioning to an area of Condos/Townhouses. Then a commercial area to transition to the existing commercial use.

before any of this is green lit could we have environmental impact studies made — ANONYMOUS

Q1 & 2: Would like to see natural landscaping and park areas adjacent to river corridor, respecting natural environment and creating natural buffers between corridor and built environments

Critical that that lowest land be 'left' to nature since it will be repeatedly exposed to nature before very long! — ANONYMOUS

Create something that is complimentary but with a new twist that incorporates green space

Yes definitely an outdoor music venue

but also one that takes into account extra noise created in neighborhoods. I already hear the music from downtown, the football field, and now Lefthand Brewery. — ANONYMOUS

Outdoor learning center-nature and history of the people. For example pollinator garden and art that reflects the native people

Down near Denver, South Platte Park and Carson Nature Center have signs about the Plum Creek flood. Signage and a nature center with info about the 2013 flood and the riparian area of St. Vrain Creek would be awesome. — ANONYMOUS

Reuse existing historic structures.

Comments elsewhere. Some seriously cool ideas mixed in there. Anyone been to Pioneer Town in Cedaredge? — ANONYMOUS

Goal 1 Q2

Natural landscaping & park areas that maintain some of the old industrial feel of the area --> for inspiration: Stem Cider: Acreage in Lafayette

#2 Adaptive Reuse

I love the character photos of the community gatherings adjacent to an old factory

Sugar mill - creative reuse of the existing structures. mix of housing, community gathering space

Land use mix development housing, arts & cultural. Community Space that community can access/affordable

Another idea

A Nature/Science center or museum would be great!

Q2

Bar restaurant on top of silos

Question2

Less car centric

turning the silos into amazingly huge indoor top-rope climbing structures. Could be something that brings some tourism \$ into our area as well.

Q2

STEAM needs to have education as its core: expansion or relocation of FRCC AND CU Extension.

Sugar Mill feel

I would like to see some way to keep/preserve an aspect of the sugar mill to give it a historical feel.

Q2

Sugar Mill needs to reflect our agricultural heritage and present. Malting facility, trade, sale, teaching, etc. These two areas are VERY differently and should be treated as such.

How much of the Sugar Mill property is on the floodplain.

STEAM area is in floodplain. Sugarmill area is not in floodplain. — MARCIA R MARTIN

I think you are wrong about which property is on the floodplain. There is no flood mitigation east of the confluence with Lefthand Creek. The Sugarmill area should be a natural area, all of it, because it will flood. — ANONYMOUS

How do we maintain opportunities for industrial uses and not displace what's there; there's no other land in Longmont for industrial uses

As long as the area near Sunset and Boston (think Crocs and Golden, etc) are empty, there's plenty of industrial room in Longmont. — ANONYMOUS

Industrial Co-use

I also have concerns like Jim mentioned that we have a lot of small and minority industrial businesses in this area. I am also fearful that businesses like that will be pushed out. As well as rental rates pushed up. So that \$8 beer someone mentioned is because rental rates will be high for new development vs. existing development.

More units per acre help make units more affordable. — ANONYMOUS

Inside the sugarmill structure - great large piping, maybe museum space, art gallery space.

Old sugar beet structure is a clear span open large structure - great indoor community gathering space, multifunction that can be adapted as needed

no on climbing gym. thanks! :)

The giant metal storage building has got to go.

...what about the idea of turning something like that into a music venue or indoor farmers market, etc? — ANONYMOUS

An anchor (or multiple) such as art/music venue, combined with causal restaurant space, with ample outdoor space (Wibbys for ex), food truck venues, outdoor areas plus varieties of housing (not too close to the music ;) townhomes and multi-family -- commercial uses to north/streets, housing along park(s) to south.

Think about transitional uses in the interim

What if there was a warehouse section where people could park their RV or store their "box" house? Seems like there's a demand for something like this if we get desperate (I don't actually want to see this, I just see the need for my buyers who don't want or need much...like don't want hardly anything)... maybe this could be near the tracks? LOL

There's a cute Tiny Homes 'neighborhood' just North of Laverne Johnson in Lyons (i.e. next to the river). As long you can move 'em quick... — ANONYMOUS

Q2: Sugar Mill reuse

Take cues from MANY east coast models, reusing historic buildings as destination entertainment / work spaces. Put high density housing nearby

Question 3: Housing Types

Small town America

I would love to see (in the Sugar Mill area) small houses much like what is in old town Longmont. Front porches. Smaller yards. Narrow-ish streets. Imagine entering Longmont on 119 and seeing the backdrop of the mountains, the tall sugar "factory", and a neighborhood of beautiful little bright colored houses.

Homeless Shelter Opportunitites

Part of this area is near the OUR Center and it is away from current housing. It would be a good opportunity to provide new types of solutions for the homeless, like tent cities. Being away from other houses would avoid the NIMBY problem.

Equity Concerns

The amenitization of Longmont - including the STEAM vision, is a pure positive for social equity. Privileged children will see symphony, ballet, Broadway in DCPA in Denver or elsewhere. But kids whose parents work 2 jobs have a different deal. If they don't see it in Longmont they may grow up without having ever seen it.

The city can anticipate and prevent gentrification with zoning, inclusionary housing, deed restrictions, etc.

This sounds nice, but as many things, doesn't play out as "pitched" As an example, I looked over the names of participants and didn't see any faces of people of color and not many latinx nor other diverse-sounding names... — SAM IAM

have environmental studies been mde — ANONYMOUS

Single family detached - we have plenty in other areas.

Question 3

Large Single Family homes. We need more multi unit designs

We do need multi-unit. Not sure we need large single family homes. — ANONYMOUS

What I don't want to see:

Large McMansions/single family homes.

or for that matter, super small Single family - not a good spot for 'tiny homes' — ANONYMOUS

tiny house village for senior and Or others interested in living a simpler life style
— ANONYMOUS

Generally Single family detached with exception of cottage clusters or perhaps small cottages which may work nicely in some spaces

what the hell the above statement is an example of governmental double talk — ANONYMOUS

Housing to avoid

I would recommend that we stay away from single family homes. Given the proximity to downtown and to the rail/bus lines it calls for higher density and to support people who like to walk/public transit to events.

Goal 1 Q3

Housing --> tiny homes, container homes, van life space SHOULD be considered --> these options are affordable & sustainable for many diverse communities :)

These are not the densest form factors for a landlocked city. Tiny Homes in particular are mainly for transitional housing for traumatized populations. Manufactured housing that goes up fast and costs less is a better option, in dense configuration. — MARCIA R MARTIN

I disagree with this. When we're looking at double digit % growth in the housing market, single family homes are not affordable, even for upper-middle class incomes. Unique housing options can also bring more diversity to Longmont and encourage working millennials to move to the area. — ANONYMOUS

agree w/2nd comment about need for unique options — ANONYMOUS

comment 2 has my vote — ANONYMOUS

question 3: Longmont is more or less out of land to develop, so any structure/single family home built on more than .2 acres seems silly to build.

Seems like the City continues to annex and checkerboard... no? — ANONYMOUS

Respect for the views/high density housing

Whatever is done, the buildings need to have respect for the views. Dense, tall buildings though having most density, sacrifice SO MUCH by way of character and views. I think being mindful that sunshine (tall buildings block the sun) is so integral for mental health (and keeping the roads clear lol). Limiting height is important.

Density is one of the keys to affordability given the high cost of land and very limited amount of developable/redevelopable land. – ANONYMOUS

Perhaps a view 'corridor' low along the river the the townhomes and complexes growing up the bank? – ANONYMOUS

Q3

Consider something we do not have anywhere in Longmont, and that is an urban forest.

Q3

Question: Housing at Sugarmill, NOT single family residential. NOT car-oriented housing, a more walkable, car-free zone as much as possible.

Q3: Housing

We don't need more expensive condos. We need a focus on ACTUAL affordable housing, to allow all our Longmont workforce to be able to live here in Longmont. Actual affordable housing for everyone who needs it means a whole lot more than the 10-12% in our city's code. I'm talking 50% affordable housing within NEW builds. We need to address the housing crisis and not keep building expensive housing that is out of local's reach.

*I thought we *had* that years ago, but the Tera Rosa neighborhood (Kansas & Sherman) backed out of some of their mid-density stuff. – ANONYMOUS*

Third bullet point - No high density residential housing should be allowed in the Steam and Sugar Mill area. No tall buildings.

it might depend on the view towards longmont – ANONYMOUS

completely agree re: no tall buildings. defeats the point of wanting to preserve the view – ANONYMOUS

#3 - Housing

It would be great to see transit oriented development, scaling down to connected and eclectic housing areas.

Include trail connectivity throughout the area, low car speeds, and greenspace/parks

**Local* transportation will need to chase the population since we can't depend on Public Transportation™ to chase the public. – ANONYMOUS*

I'm a renter in old town, I would move there if I could afford... Maybe 70% income bracket housing?

Visionary Housing & Commercial Builds

All new housing and commercial buildings should be REQUIRED to be built with strict sustainability metrics: LEED Certified, Net Zero, full electrification (no gas), solar panels on all rooftops for renewable energy wired right into each building - we get SO much sun on the front range, there is no excuse for not incorporating solar into EVERY new big build project!!!

Q3

inter generational housing maybe look the traditional housing of the pueblos in NM

Q3

Community co-housing could be very attractive. Just keep the units small & avoid the luxury component.

#3 - Housing

I don't see single family homes as being appropriate in this area. It seems like a pretty inefficient use of land.

Very concerned generally about social equity w/this Goal 1. Specifically about the inevitable gentrification and impacts (displacement due to increased rents/cost of living) on the various people who live in this area now ---especially those who are in the margins economically --the working poor

This whole Goal generally: I am very concerned there is no goal addressing our desperate need for affordable housing - which needs to be more than 12% when using public monies to largely develop/build. It should be mandated that this area housing be significantly higher..

We'll need to support density to meet this goal on any kind of scale. — ANONYMOUS

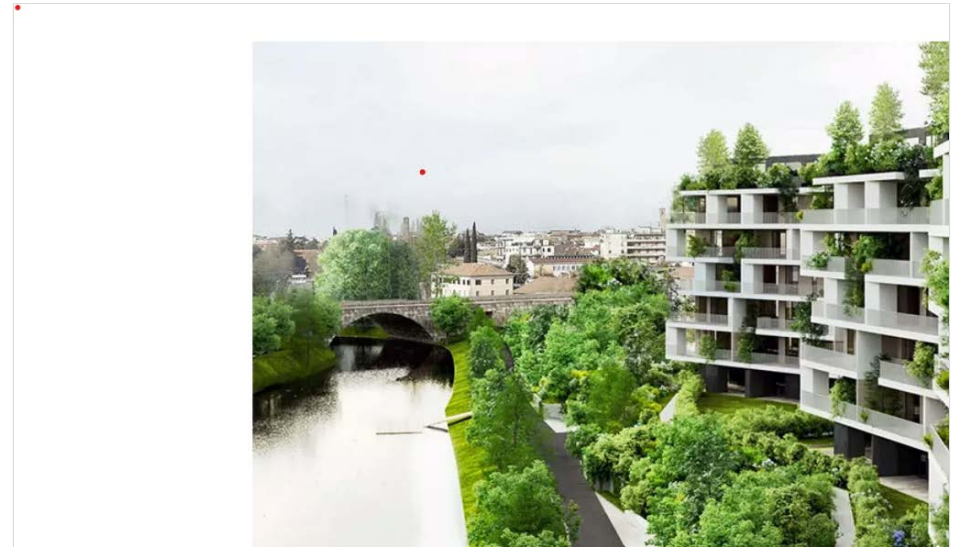
Q3, housing types

No need for large format single residents in this area. Also, let's build some affordable options, but stay away from mobile home parks.

Workforce Housing

We need a significant amount of denser for-purchase housing solutions so we can keep our offspring and workers growing roots in Longmont.

seems to me we could get affordable housing plus urban forest. Something not quite like this, but green roofs, etc.



curb friendly housing will encourage foot and bike traffic

Taller buildings discourage that...space from buildings to curb being bigger will encourage that.

Buildings should incorporate bird-friendly windows to minimize bird strikes, green roofs, and native landscaping.

Consider co-operative housing.

HOUSING

Single Family Housing is probably not appropriate anywhere in Longmont going forward.. Duplexes and Triplexes, stacked condos, lots of "missing middle" homes in mixed use neighborhoods. Think Brooklyn's Prospect Park area.

Q3

would love to see an opportunity for a co-housing community, lots of mixed use (not unlike parts of Prospect), small urban pocket parks with larger sections of open space, opportunity for outdoor concert venues, and more senior housing that is not part of a

for profit national company....

To be truly a community, wouldn't there need to be grocery? Bodega, maybe? – ANONYMOUS

Single

As someone who is single with a dog on a grocery store income housing is not available in town. The current rent is \$1,100 for my share of a rental house. Small homes in a condensed area with paths and public transportation would be great.

When deciding what if affordable the media income should to thrown in to the mix . We are losing our young people , at \$15 an hour they just can't afford to live here or anywhere for that matter – ANONYMOUS

Consider a section for senior housing not part of a for-profit corporation for independent living. Also, consider a section for a co-housing community.

See above. Food desert? Bus to groceries? – ANONYMOUS

Grocery and pharmacy necessary. The grocery need not be super markets . Also Kng Supers and Safe ay could offer free transport to stores – ANONYMOUS

Question 4: Inspirations

distillery district in toronto

distillery district in toronto

distillery district in toronto

Q4: Inspirational spaces

Mass MoCA is a great space, but more where it is a fully active community:
<https://massmoca.org/>

MASS MoCA

MASS MoCA is one of the world's liveliest centers for making and enjoying today's most evocative art. With vast galleries and a stunning collection of indoor and outdoor performing arts venues, MASS MoCA is able to embrace all forms of art: music, sculpture, dance, film, painting, photography, theater, and new, boundary-crossing works of art that defy easy classification.

MASS MOCA | MASSACHUSETTS MUSEUM OF CONTEMPORARY ART



I would like to see more European models regarding mix use

don't make it all starting at 8\$ beers

Q4

Transit and biking for those who can

Another idea:

Community gardens!

Love the idea of community gardens - maybe some that feed into the food bank?
– ANONYMOUS

Inspirational areas

Quaker Square in Akron, Ohio was an early adopter of reusing an industrial area like this.

Ft. Collins has been great at having shared areas in their downtown with cute architectural character

Inspiring areas

RiverWalk in San Antonio

brickbottom artist's housing in Somerville, MA

Inspiring places

Fort Worth Arts District

Q4

Baltimore downtown power plant

Yes!! Love this! – ANONYMOUS

Bullet point 4 - I would like the Steam and Sugar Mill areas to be a sustainable net zero energy community similar to the Modern West project.

Q4

Inspiring Places: European cities: Madrid's Super-Blocks, Copenhagen, Paris is converting areas to being car-free.,

Q4

Sydney Australia and Seattle both have wonderful old industrial buildings turned into successful farmers markets, housing, restaurants, teaching.

Q4

Norfolk VA repurposed a old warehouse into shops and restaurant

Providence Rhode Island - great adaptive reuse of former industrial buildings integrating the arts and education

Q4

Demand that all new building here have geothermal heating because it is financially feasible given the acreage

This was Olympia WA, very good use of historical with fresh

<https://www.olympiafarmersmarket.com/>

Olympia Farmers Market - Open During COVID-19 Response

The Market board and staff continue to monitor the advancing COVID-19 health crisis closely, seeking guidance from our partners at Thurston County Public Health and Social Services and the Washington State Farmers Market Association. As of June 24, masks are required for all Market staff and vendors, regardless of vaccination status.

OLYMPIAFARMERSMARKET



Q4

Minneapolis River Walk area

General Comment

FOOD TRUCKS! Northglenn has a highly successful food truck festival every May. Longmont has a vibrant food truck community & space for development would be a HUGE draw for the area. Would love to see opportunities for exciting, mobile restaurants with open, outdoor eating!

Q4

see Mill City Museum in Minneapolis for utilizing existing industrial character while near a natural waterway space

consider negative impacts of combining "industrial" and "Natural water way space"

– SAM IAM

REI re-purposed the old Forney Museum in Denver. The mill seems like a great opportunity for a large space venue - art/performance/music. (since that scale of retail is too large for Lmt)

Business incubator

What about a place like Boulder's Rosetta? It would be a great place to incubate new restaurants (rather than a food truck)

I'd like to see other business incubators like that whether retail or business to business.

<https://boulder.avantifandb.com/>

Home - Avanti F and B - Boulder

Home - Avanti F and B - Boulder Explore your culinary options after picking up one of our twenty draft beers, classic cocktails, or affordably priced wines. Find us for a beverage at one of two large bars (main level and rooftop) or grab a seat in our lounge, dining area, or rooftop deck and one of our many cocktail servers will find you.

AVANTI F AND B - BOULDER



Please make the presentation pictures reality. Too often the "concepts" are never reality because of parking requirements, setbacks, property sizing.

<https://www.youtube.com/watch?v=bnKIVX968PQ>



**LIVELY & LIVEABLE
NEIGHBOURHOODS**



**NOT
JUST
BIKES**

The Lively & Liveable Neighbourhoods that are Illegal in Most of North America
by Not Just Bikes
YOUTUBE

<https://www.youtube.com/watch?v=-sA2LeHTIUI&t=214s>

Youth Baseball Tournament Venue & Complex

Future Legends Sports Complex is located in Windsor, CO. Future Legends is a premium multi-sport complex dedicated for the stars of tomorrow.

FUTURE LEGENDS SPORTS COMPLEX



Transit / Connectivity

Bike-Friendly!

Bike racks, bike paths, include in the planning and building phase, particularly when looking at our greenways and connecting to main st

MAKE THE BIKE PATHS WATER PEREABLE — ANONYMOUS

work with the RR to allow parallel trail...?

connectivity

If the area is super cool, make sure that residents that live in that area have good access to Main street via bikes and walking. Otherwise...why leave?

where are all these people going to park?

We should be promoting transit, walking, and biking for those who can — ANONYMOUS

yes, make it really Cool, and people need to Walk/ Bike / Skate to get there, no parking there AT site. — ANONYMOUS

limiting parking sounds ok during the summer, but during the winter those businesses would suffer. also - you are talking more to people who aren't dealing with mobility issues — ANONYMOUS

You need a trail from near 300 Bar over to area, that is double wide, and some murals, etc.

Alternatives to Sprawl: Case Studies in Building Better 'Burbs

by Eco Gecko

YOUTUBE

We could use additional fields and facilities for youth sports. Longmont Open space policies don't allow for the addition of permanent structures or certain activities on open space properties. Fields allow for more participation locally, keeps revenues local, and attract regional participation allow hosting of tournaments.

Extreme example:

[Youth Baseball Tournament Venue & Complex \(futurelegendscomplex.com\)](http://futurelegendscomplex.com)

Temporary ball fields would be excellent as a transitional use as well!!

It would guide people towards the area

RE Main St.... through the area: Aerial Gondolas are actually being used in cities throughout the world to deal with congestion, and surprisingly offer a viable economic model and tourism, other benefits.

A lot of money is being invested in the Saint Vrain Creek corridor. There should be trail connections to established trails (that service older and lower income neighborhoods) to the Sugar Mill and Steam areas.

Boston needs to go through so that people STOP using 3rd ave as a freeway.

Add a bridge or underground passage on Third to connect the Oligarchy trail and the Sugar Mill site. The Oligarchy trail services neighborhoods as far north as Lanyon Park.

if there is a large transit center there, would be GREAT to have parking with direct access to airport (if not from somewhere else in Longmont). This is sorely missing!

PARKING PROMOTES CAR DEPENDENCY — ANONYMOUS

less parking, considering this should be a transit/walk/bike focused.

There should be safe connection (bridge or under pass) connecting Kensington Park and the Steam area.

Sustainability / Environment

Both developments should plan to achieve EcoDistrict certification

general comment

Much of this area is in the flood plain and due to history of a dozen or so floods in this area, it is not smart to use our massive public investment in flood mitigation (\$500 million and still not completed...) With this weeks IPCC dire predictions about climate change and the impacts we're already seeing in terms of extreme and more frequent events, this area will flood again despite the best mitigated engineering efforts...

If we're putting in arts and cultural center buildings, we should put in a nature center that would talk about the river and the animals in the area.

IF WE ARE CONCERNED ABOUT THE ANIMAL IN THE AREA THE ENTIRE SITE SHOULD SHUT DOWN A HOUR BEFORE DUSK — ANONYMOUS

We can't pretend like this area will not flood again. Aldo Leopold said in 1890 "Whatever is in the floodplain belongs to the river and it is up to her when she will take it back."

We need to have mandates that any/all development in these areas must be designed and built sustainably - including solar, efficient buildings w/green materials, EV-charging stations, Net Zero /carbon neutral

Any/all development needs to have MANDATED sustainability features

Question

What kind of wildlife preservation (if applicable) measures are you planning?

How are existential issues and "wicked problems" such as climate change, eco-system collapse, the digital divide, social inequities, etc. being addressed in the process?

we need actual affordable and equitable housing, and build with sustainable practices (new construction itself is hugely wasteful!) and we need to require all new large developments to use certified green building practices — ANONYMOUS

Longmont is updating building and land use codes. Sustainable building practices will be more and more required. The City partnered projects like the Cultural Arts Center will be green buildings. — MARCIA R MARTIN

My unborn grandchildren will be voting by the time these codes and practices are codified much less impimplemenyed — ANONYMOUS

There have been 11 flood events in the St. Vrain Corridor according to the Army Corps of Engineers. Climate change /chaos has repeatedly demonstrated that despite best flood mitigation efforts, this area will flood again, so putting people and property in harms way is unwise and economically irresponsible.

Add a demonstration garden for low water and native plants for educational/motivational purposes.

Native plants and/or xeriscaping. No sprinkler systems going at all hours of the day and night watering bluegrass lawn.

BREAKOUT ROOM 2: TRANSPORTATION & CONNECTIVITY

Sugar Mill + STEAM 3/2 Community Forum

MARINE SIOHAN MAR 01, 2022 03:49PM

Instructions

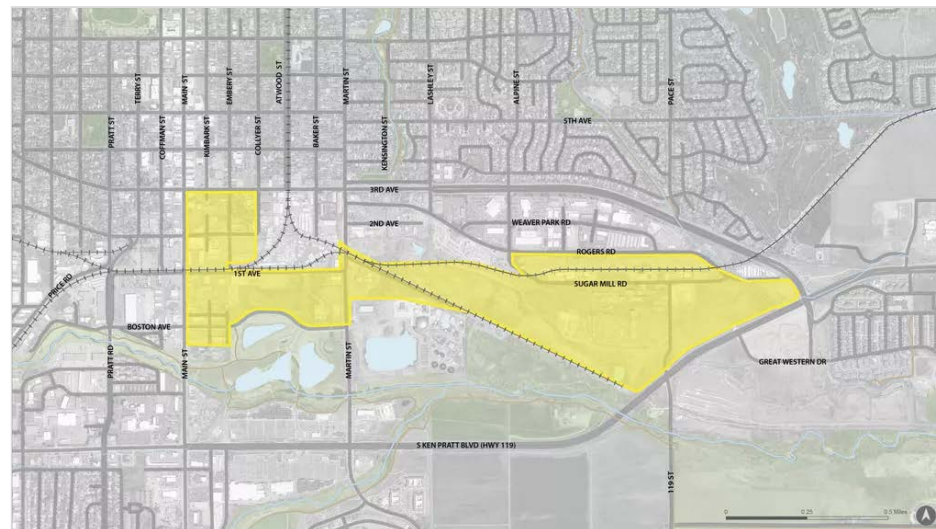
Goal 2 Questions

- Where and what kind of ped/bike connections are most critical to create or continue?
- What changes should be made on 3rd Avenue?
- What places or projects have you seen that could inspire the study area? What parts did you like?

Goal 2: Enhance Transportation

- Enhance 3rd Avenue to act as a multi-modal 'Complete Street' for pedestrians, cyclists and motorists.
- Bring bus service into the site by modifying existing or introducing new routes.
- Identify options for enhancing east-west connectivity for all modes.
- Extend existing trails into the site via a pedestrian / bike connection at Pace Street.
- "Trails as Mobility" - Enhance connections to the St. Vrain Greenway trail and position Greenway as important ped/bike mobility corridor.

Project Area Map



Question 1: Bike/Ped Connections

1st & Last Mile

Maybe consider scooters and other options?

From what I've seen in other, nearby towns, the scooters will come if it's profitable. They will end up in the bike lanes or sharrows routes. — ANONYMOUS

North/South protected biking connections from NW Longmont to this area.

North of 119, N/S is hardly the challenge found *crossing* 119 or E/W in this area.
— ANONYMOUS

Bike / ped connection

On the east side, there is no way to cross KPB from Martin to 3rd/119, so adding more connections across would be good, connecting to greenways, other roads, etc

Connect to St. Vrain Greenway

Need a connection from these sites to SVG

Bike/Ped Connections

Not easy to get into downtown by bicycle so any opportunity to help with that would be good. Need more north/south connections.

True at major crossings (e.g. 119, 3rd) — ANONYMOUS

Bike/Ped

What types of bike facilities would encourage people to get on their bikes?

Sounds like people would like to see grade-separated facilities over major streets.
— ANONYMOUS

Agree, separation from vehicles is important to get more people on their bikes. Separate paths, too stressful to ride along with cars. — ANONYMOUS

Road diets, speed limits — ANONYMOUS

Bicycle Facilities

Need ability to get across the highways. Without this ability it impedes ability to get on bikes. Over-under passes are needed. Grade separated crossings are important.

Need stronger bike/ped connections

Need to connect this area to existing neighborhoods. Grade-separated of major roadways.

Continue Oligarchy trail to this new site

connect via Pace alignment

bit easier to envision bike/ped/transit options if we knew more about the diversity of land use...residential, commercial, industrial, green space, etc.....

Bike Storage facilities

Inclusion of more bike boxes, especially near neighborhoods if homes/units are small, as well as in work spaces - more than just bike parking spots.

Bike connections to Bikeway

If the bikeway is to work, especially folks with ebikes will need to be able to transit across the City, generally along KPB or this area to then connect in to the Bikeway efficiently

We need bike facilities designed for the interested, but concerned riders and not the fearless riders. Think kids, parents, and older folks as your target users

Denver has been working toward a grid system that highlights bike-friendly (and bike-curious) streets to encourage (if not guarantee) regularly-spaced streets to 'get-around' on.
— ANONYMOUS

Re Connectivity: YES, bike, pedestrian, last mile solutions, multiple "bike/ped-ways" to avoid conflict between pedestrians/ e-bikes, and other multi-modal technologies. Need to consider seniors, juniors and physically disabled and their last mile (wheel chair, visibly impaired, etc needs.

Bicycle Facilities

Bike highway to 1st and Main

From...? – ANONYMOUS

...Rodgers? ...Farm/STEAM/Sugar? – ANONYMOUS

Where need for ped/bike

Some kind of North/South bike/ped corridor between Martin and Main street. A bike path that heads west but stays north of Hwy119.

'New' Boston is wide and under-used, vehicle-wise – ANONYMOUS

Connectivity

Missing connectivity Martin west to downtown including sidewalks and bike paths. At Martin everything disappears.

Cyclists on Bike Paths

There are all levels of cyclists on the greenway trails and everyone makes space for everybody else.

Question 2: 3rd Ave Changes

Landscaped median and landscaped edges along 3rd to slow traffic

ANYthing that will work toward this goal (see elsewhere) would be welcomed. – ANONYMOUS

Traffic signal at Alpine/3rd

Would 'break' the speedway, and allow for crossings *if* there's a continuation into the Sugar parcel. – ANONYMOUS

Access to Downtown via 3rd Ave

Need to make those connections from this site, across RR tracks, to downtown.

Also need N/S connections through this site.

crossing 119 to the South (for bikes/peds) – access to the greenway

road diet for 3rd Ave. – bike lanes + two lanes of vehicular traffic + reduced speed limits

3! thumbs if I could! – ANONYMOUS

Yes! – ANONYMOUS

Yes!! – ANONYMOUS

Yes!!! – ANONYMOUS

Bicycle Facilities

Grade separated crossings are important and needed. 3rd Avenue is wide and is difficult to cross.

Vehicle Speeds

Vehicle speeds need to be slowed down.

3rd Avenue Cross Section

Can the 3rd Avenue be reconfigured to reduce the number of lanes along with safer crossings.

Anything to encourage 119 as the pass-thru. – ANONYMOUS

Re Third avenue:

Taking a "complete streets" long-term vision and approach when upgrading this 3rd Street infrastructure. Even if not viable now... planning for and preserving areas for eventual implementation. - MLS

Using 'New' Coffman as model? - ANONYMOUS

Question 3: Inspirations

If no time to watch the entire webinar, start at the bike tour at 18min mark. Notice the parking design, housing design, neighborhood street design, and bike/walking only areas. Think how much safer and aesthetically pleasing this whole neighborhood is!



MOBYCON

MOBYCON WEBINAR SERIES:
Virtual Study Tour:
Dissecting the Dutch Street

Tuesday, May 12
18:00 Amsterdam
12:00 New York
9:00 Los Angeles

How the Dutch design streets (and why they do it this way) - Virtual Study Tour
by Mobycon
YOUTUBE

Longmont needs a small gauge trolley like in San Francisco.

Too large a commitment - ANONYMOUS

These Are the World's Best Cities for Biking. Copenhagen, Denmark for Longmont/precedent.

Let's consider a little bit of crazy :)

Love monorail - what about a gondola?

a monorail through the this area would have be great like vegas

Best Projects

Copenhagen-type bicycle facilities?

Bike highways. - ANONYMOUS

Need to have continuous bikeway connections that don't mix with vehicle traffic.
- ANONYMOUS

We've got a few that *start* reaching in that direction... - ANONYMOUS

Great Projects

Monorail

Stapleton comes to miind as a larger facility that has successfully transformed an airport to a community; re: the transportation, it is accessible by bike from the outside and MORE friendly to bikes/peds within the community. (park and walk if need be). narrow streets within, nice sidewalks and slow speeds)

Waoh, wait! Are we losing VBrand?!? - ANONYMOUS

(Woah) - ANONYMOUS

Re connectivity:

I mentioned this in Land Use breakout, but it applies here: Aerial gondolas are being used in many dense cities: it reflects our mountain heritage, skiing, creates a unique tourism experience/ appeal in the region, new, singular views of Longmont and the mountains. They have a viable economic market, and local contractors with experience to construct. It could take advantage of the silos and their height.

Smart-City technology for transit and connectivity: ? How will emerging "Smart-City" tech be considered and embedded, especially with respect to "SAFE", night-time pedestrian and bicycle connectivity to the RTD station... IE.. sensors to turn on lighting, alert safety issues, notify city staff in real time of issues: trash, aggressive panhandlers, etc. - MLS

Transit System

Longmont owned Bus/Transit with small busses. Maybe share with FRCC like other cities share with their colleges.

Happen to know there's buy-in on this from at least a couple members of the TCouncil.
— ANONYMOUS

Transit Service to the East

We need transit to the east side like the hospital, sandstone ranch, Union to downtown would increase transit usage

*Easier to get *to* each of those (3) locations than to reach Sandstone from *either* of the other two.* — ANONYMOUS

North/South Rail

Is the north/south train factoring into this discussion

Area sub transit hub

I think the area needs to be a magnet to convert SOV cars coming from the east to multimodal, to get through Longmont, and west to Boulder

the area around the sugar mill doesn't have any bus availability at this time. In fact you can't seem to get to the hospital easily. What ever is added to the sugarmill area should extend on to possibly as far a Sandstone

With apartment buildings being built that don't have enough parking to encourage people to use other transportation modes, the problem is that there are not the other modes available. If you can't go from point A to point B using alternative modes, people are going to continue to drive. Transit needs to go to where you want to go and more frequent service.

Improving ped connections

There is little to no bus service heading out to the East side of Longmont. Extend to serve the East side community. Stops by Sandstone, Hospital, Union Reservoir and Jimm Hamm Nature area to name a few.

City Transit

The City should consider offering transit to improve local bus service

See above. Fertile ground exists at City Hall. — ANONYMOUS

Bus Transit

Buses need to run more often. Once per hour isn't adequate. There is no service to Fox Hill which forces people to drive.

Other / General Comments

Focus on traffic calming within the design as much as possible. The MUTCD is outdated. Focus on NACTO and overall dutch principles. Or hire dutch consultant to actually design a liveable, walkable, quiet, efficient neighborhood. Be Creative!



lighting impacts on our greenway

Any lighting proposed along our greenway and/or natural areas needs to low to ground and projected downward. Wildlife moves at night. Our St. Vrain Greenway is a wildlife connector so we need to be mindful and respectful of the impacts of lighting/disturbance.

Unrelated but important

Staff did an awesome job - kudos!

This system is a fun way for all to participate. — ANONYMOUS

*But a request: advanced notice that the activity *won't* be 'phone-friendly.' Even a tablet might have trouble navigating. 'Definitely-desktop@!' — ANONYMOUS*

EV Charging

Provide EV charging stations.

Multimodal connections

As the tritowns grow, and they remain outside of RTD, we need a better solution to buses, etc, especially if we look at this as a hub.

More-powerful if we were considered a major 'spoke' in RTD's 'wheel.' We've become more of an 'appendix' lately? — ANONYMOUS

Multi-modal Hubs

Mixed Use facilities available for when you're waiting for a bus.

Transportation Options

1. Bus Depot --> Longmont is increasingly a "suburb" of Denver --> we need sustainable transportation options for daily commutes
2. Reroute 287 traffic AWAY from Main St. --> this heavy traffic deters people away from Longmont's most vibrant "small town" feel.

County-support needed to Coopt the 287 project, shunt traffic to Hover (or maybe CoRdI?)?
— ANONYMOUS

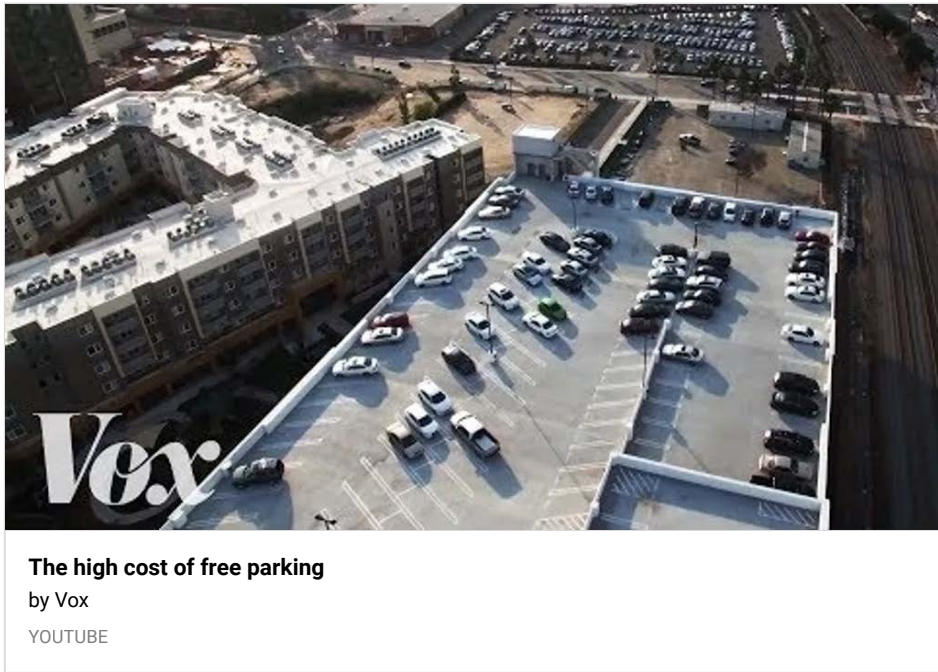
Multi-modal Hub

Perhaps a MMH could be helpful in an area with good access to low stress bike/ped facilities

Electric Charging Stations!!

Parking

"There are 8 parking spots for every car on the road in America." This is a problem and a waste of space.



Re Parking:

Minimize Car-oriented design. Parking primarily on the perimeter (not internally), with plenty of public last mile technology; rentals for cycles, e-bikes, scooters, and/or golf carts, tuk tuks, small open flex vehicles for 4 to 10 people, etc.
NO to LIMITED CARS in the area! - MLS

What does good parking look like?

Parking garages with smaller footprints, but more parking.

Below the retail and med-dens residential? - ANONYMOUS

Parking

Parking will need to be well-thought out.

Parking garages - ANONYMOUS

Parking

Car parking should not be free

Parking

Will there be adequate parking for visitors? Trailhead features

Parking Garage...

...consider ground-floor uses (like retail)

or below...? - ANONYMOUS

BREAKOUT ROOM 3: ENVIRONMENT, SUSTAINABILITY & RESILIENCE

Sugar Mill + STEAM 3/2 Community Forum

MARINE SIOHAN MAR 01, 2022 08:16PM

Instructions

Goal 5 Questions

The study area is quite large and could act as a 'test' for sustainable practices – what ideas do you have in this area?

What are your priorities in these 'areas' of sustainability?

- Natural world/habitat
- Climate and risk
- Quality of life/equity
- Mobility
- Resource allocation and conservation

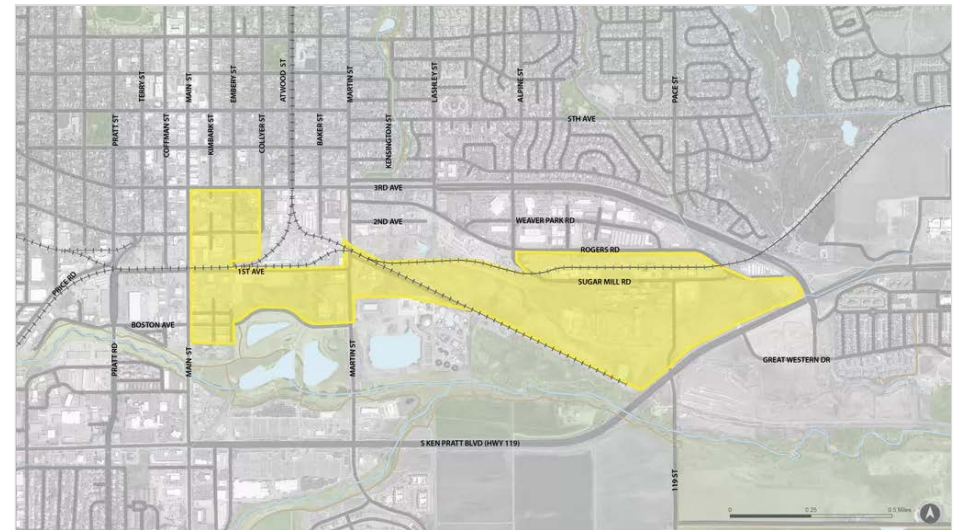
What places or projects have you seen that could inspire the study area? What parts did you like?

It seems to me this is a really large area to be a "pilot project." – ANONYMOUS

Goal 5: Promote Sustainability

- Provide compact residential options with a focus on incorporating open spaces.
- Provide space for employment uses that reduce the need for commuting and allow people to live near work.
- Preserve historic Sugar Mill structures and integrate them into the community.
- Create incentives for green infrastructure and sustainable development.
- Integrate renewable energy throughout the study area whenever possible.

Project Area Map



Question 1: Sustainable Practices & Innovations

Micro Prairies! rather than turf strips along roadways.

Micro-Prairies


We specialize in turning sod into short-grass prairie meadow Why? Do you want a landscape that... Requires minimal water and maintenance once established? Supports an abundance of insects, including honeybees and native pollinators? Sequesters carbon? Looks beautiful? Belongs in Colorado? Join us in restoring the short-grass prairie, one lawn at a time! How Does it Work?

LEFT HAND LANDSCAPING



Stockholm Tree planting method to encourage larger and faster growing street trees

The Stockholm Solution - Ten Years of Experience of Urban Tree Planning and Management Combined with Local Storm Water Management



Green Days & Green Techniques
February 11, 2014
Tampere Finland

110214-viherpaivat-trees-stockholm
PDF document
PADLET DRIVE

Look at this manual that the city of Tucson has created for the standard in street design to include ways to revitalize the landscape using our own resources.

GREEN INFRASTRUCTURE FOR DESERT COMMUNITIES



Watershed Management Group

green-infrastructure-manual-for-desert-communities-2016
PDF document
PADLET DRIVE

<https://www.youtube.com/watch?v=I2xDZlpInik> — ANONYMOUS

Repurpose Sugar Mill for vertical farming

Stumbled across a YouTube video about Plenty (<https://www.plenty.ag/about-us/>) They're based in Laramie, WY and working to develop sustainable indoor farming practices. Seems to me that this would be an excellent use of a stabilized and renovated Sugar Mill. We make use of the existing structures, create a bunch of jobs, and the community has additional access to high quality produce.

About Us

Plenty is an indoor vertical farming company that uses less space and fewer resources to grow flavorful, healthy, fresh, and clean produce year-round.

PLENTY.AG



Require green building standards, not incentives.

Incentives will not get us where we need to be going. Must be required. This is justified due to the massive public investment that will be used to develop. — SAM IAM

Would like to set a baseline and metric to track how we are doing.

Stronger than incentives for green practices - not a goal, but a policy.

Using LEED Sustainable SITES rating system to establish a baseline of landscape performance before/after.

*Precedent National Renewable Energy Lab (NREL) Research Support Facility Golden Colorado
— ANONYMOUS*

or passivhaus "passive house" metric. google it, research it. — ANONYMOUS

Low profile homes in an eco village setting

I do not agree that compact living quarters with conjoined walls is the only sustainable housing approach. This is not conducive to aspects of human sustainability and human ecology like privacy, creativity, and biophilia. Distributed low profile homes in an eco village setting is more sustainable over time in terms of time honored measures of human happiness. However this doesn't fit with city and urban planning goals of recruiting and courting developers who want to maximize their investment. This is why Longmont is clogged with giant big box apartments with nominal obligatory green areas and big asphalt parking areas rather than distributed parking and growing areas in a perforated pavement/brick type landscape with trees and "food forest" type plantings.

Would like to set a baseline and metric to track how we are doing.

Re: the "sustainability goals" presented at the start of this meeting

For green infrastructure & renewable energy – can we do something stronger than incentives??? Incentives are non-binding, we need to actually require green building practices (LEED certification, Net Zero, Electrification, rooftop solar, etc.) rather than just offering weak "incentives"

*Prior to council reinstating our Inclusionary Housing Ordinance, "incentives" for Affordable Housing produced not one unit much less structure. Incentives sound good but don't work.
— SAM IAM*

would like to see passive house metric required throughout the entire site.

Not sure what that means — ANONYMOUS

passive house is a german principle (now northern Europe). High insulation value so zero to no energy is needed to heat or cool house. This is due to properly placed windows, triple pane window (standard in Germany and Sweden), high insulation, and energy made from the house itself. Also, most homes eliminate the gas line entirely. — ANONYMOUS

<https://www.youtube.com/watch?v=-sA2LeHTIUI&t=214s> — ANONYMOUS

*research "passivhaus." the term typically uses the german rather than an english translation.
— ANONYMOUS*

the most sustainable housing uses passive solar orientation and earth based construction methods. This is not conducive to giant clumps of housing, many stories high, that cannot maximize the movement of the sun for heat and light and energy, and cannot be shaded with neighborhood trees.

It is wrongheaded to eliminate parking in order to disincentivize the use of cars. Instead put in rainwater collecting perforated pavers made of a sustainable earth based material like brick, that can be pulled out and additional garden areas added if and when car use and ownership is reduced. You can't reduce car use by just making it hard to park one. You need to follow human cultural pathways of desire.

Any multifamily housing in this area should utilize recycling and composting as well as regular trash collection. I am also in favor of permeable pavers.

Green roofs should be used with NATIVE plants. Also native plants should be planted throughout the development.

It would be delightful to incorporate an urban forest into the mix. not all buildings and pavement and planters.

Rooftop solar and gardens

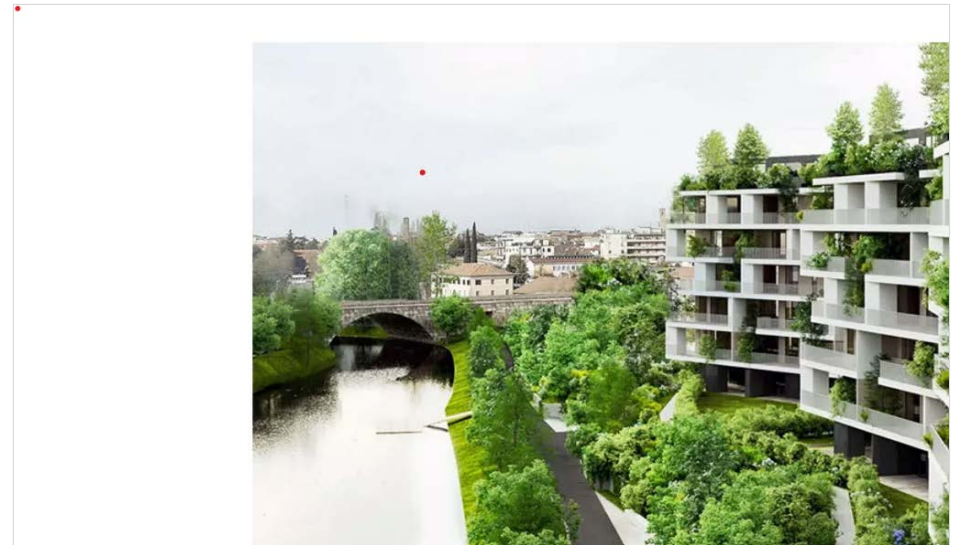
ALL new buildings in Longmont should have rooftop solar, we get so much sun year-round, we need to be moving towards harnessing our own renewable energy on every rooftop. This can also be integrated with rooftop gardens.

Integrating community solar into the STEAM design plan would be a really proactive way to reduce the energy demands of new construction on LPC while also supporting any residents in STEAM who are renting and not owning.

If there are any areas in the 250ish acres of the region we are discussing that are actually healthy soil, low rain agroecological planting with Southwest edibles could be an interesting experiment but it doesn't need to be in the heart of the transportation corridor.(see the Land Institute and other agroecology experts)

Integrating community solar into the STEAM design plan would be a really proactive way to reduce the energy demands of new construction on LPC while also supporting any residents in STEAM who are renting and not owning.

Seems to me we could get both affordable housing and urban greenery. Not quite like this, but some of the features like green roofs, etc.



like idea, but not photo of 6-7 stories — SAM IAM

I'm not a member of the development community, but the city could certainly look at what different developers are willing to offer in terms of sustainability and environmental considerations. And make it a competitive process, as competitive as possible.

Interesting opportunity with new build/ex renovation to take a net zero energy approach. Incorporate into solicitation of developers. Factor in operational expenses, not just consider an upfront cost.

I like the idea of flexible use, for example, less permanent baseball fields, etc. So future generations can rethink and redesign without having to unbuild and dispose of so much legacy stuff.

eVTOL

Electric vertical takeoff and landing (eVTOL) aircraft are in active development (Joby is going through FAA certification now). A vertiport could be included in STEAM development as a potential link to Denver, DIA, etc.

Question 2: Sustainability Priorities

bikeways as mobility, not just recreation

A uniting natural feature here is the riparian corridor w human and enviro benefits: highlight and improve.

Would like to maintain as much of the original sugarbeet structures as possible.

Depending on the viability of the individual structures, environmental contaminants, etc.
— ANONYMOUS

Know there's a lot of hazardous materials. — ANONYMOUS

Opportunity to create a unique place with adaptive reuse. — ANONYMOUS

The metal structure is undesirable due to its proportion, scale, and obvious deterioration. The cost to bring it up to code for any use would be enormous, even more so than the old turkey plant. The silos & brick structure are better suited to retain a connection to the agricultural past. — ANONYMOUS

I noticed there were plans for public parks/gathering spaces. I'd like to see some of that area set aside as non-"managed" natural areas.

Workforce and live/work housing

This is a large area with room for a wide range of uses. Housing is one of City Council's highest priorities. Denser, multi-family housing closer to the city core is both more environmentally friendly and energy efficient and far more likely to be accretive to city wealth building in contrast with single family housing sprawl which is generally subsidized by existing residents. It can also both increase the viability of transit and decrease road miles driven by workers by locating them closer to employment hubs.

alongside this, we need these new denser housing developments to be ACTUALLY affordable, not more expensive condos. We need a whole lot more than the 10-12% "affordable" in our city code. At LEAST half of all new housing built in this area needs to be affordable for our lower-income community members if we want this area to actually house our workforce, rather than just being another rich person's downtown condo — ANONYMOUS

Bird-friendly windows should be used to prevent window strikes.

Mobility - would like walkable neighborhoods --- vertical and horizontal mixed use. One stop shopping for a variety of needs. Walking, then bike, then transit, then private vehicle.

Absolutely. Being able to walk or bike to the grocery store and other essential businesses is crucial to cutting down on traffic and being more sustainable. — ANONYMOUS

I would like natural habitat and preserve area.

Mixed Use Development for walkability

Preserving the historic structures

Climate and risk and Quality of Life/Equity

Riparian Habitat

This property borders an existing beautiful riparian corridor complete with an existing RICD. I believe it is a feature that should be restored, maintained, and managed for the benefit of human enjoyment, but also for wildlife, flood mitigation, and water quality.

We live here for our natural beauty and incredible wildlife--who have no voice in the things we wall like w/out considering the costs to habitat/wildlife.. — SAM IAM

plant selection

All native or edible plants/community gardens, no grass, no pesticides, no annual plantings, all perennials, xeriscaping low water use natives needs to be a priority

Native, xeric and pollinator friendly! — ANONYMOUS

Protect farmers & agriculture

I work as a farm and agriculture advocate. The whole notion that there's going to be anything actually beneficial to local farmers in the "STEAM" area seems really unlikely. Farmers don't need "agricultural demonstration projects." They need public awareness about the policy dangers that are threatening even the most sustainable farmers in Colorado. They need public education in the relative nutritional benefit of regeneratively grown foods. The idea that there needs to be a food hub there is also bizarre. Do folks in here know what a food hub actually is?

The more we sprawl, the less land we have to grow our food. Cities need to stop sprawling, requiring denser solutions to meet housing demand. — ANONYMOUS

For Environment comments:

Any/all development must have mandates in terms of design and building sustainably including solar, energy efficient buildings using green materials, net zero, E-charging etc. Mandates are needed because incentives will not get us where we need to be going. With all the public monies that will be invested to help fund development, the impacts on our climate/environment should be paramount. Building green/sustainably aligns with our Climate Action Plan...

Agreed! — ANONYMOUS

We should consider the environmental noise impact of any developments that are considered, especially outdoor music/entertainment venues - community impact of outdoor music venues, red rocks, other venues. Would a performing arts center have an outdoor aspect? Think about acoustics and the environmental impact it has.

Mitigation measures are very specific to particular properties, noise ordinances do not address how music actually impacts the communities nearby, — ANONYMOUS

Solar on homes

We're (Habitat for Humanity) is putting solar on our new homes in Longmont without losing affordability or deepen subsidy because we know the utility costs will be lower for our homeowners and the mortgage can be adjusted so more of a homeowner's payment goes to the mortgage per month.

Question 3: Inspirations

**<https://santafe.com/going-green-the-earthships-of-taos/>
creative, human psychology friendly, recycled and recyclable dwellings.**

Going Green | Earthships of Taos | LasCruces.com

Find out what it means to live off-grid, in style, by exploring the earthships of Taos. These other-worldly homes, made from natural and upcycled materials - such as earth-packed tires, aluminum cans, and glass bottles - offer a unique, eye-opening experience into how a home can serve its occupants.

SANTAFE.COM



SITES | Developing Sustainable Landscapes

The Energy Department's National Renewable Energy Laboratory (NREL) puts their mission into practice by constructing high performance buildings and infrastructure that showcase clean energy technologies, renewable and recycled materials, water conservation, environmental stewardship, and waste reduction. Another aspect of their mission is fostering employee behavior change towards sustainable living.

SUSTAINABLESITES



Kestrel is all geothermal - put up by Boulder CO housing authority. Various price point, ADA housing also. Harmony (Hab 4 Hum), Ft Collins, all solar. All solar, mostly duplexes.

Look at Junction Place in Boulder as precedent: conservation, daylighting, solar. Energy is part of the experience but not a burden for users; built in up front.

yes we should require these being implemented up front during the initial construction for all new large builds! - ANONYMOUS

Jack's Solar Garden just south of Longmont would be an attractive example of powering new residences.

Work with COBrownfieldpartnership.org

for grant funding

<https://sustainablesites.org/national-renewable-energy-lab-nrel-research-support-facility>

Geothermal

Here's a good write up about the cost efficacy of all electric new construction in different climates if planned right. Geothermal can be one of strategies deployed to assist with heating and cooling needs coupled with high efficiency and solar.
<https://rmi.org/all-electric-new-homes-a-win-for-the-climate-and-the-economy/>

Solar co-op for residences.

<https://www.jackssolargarden.com/events>

Tour the farm | Jack's Solar Garden

Tour the farm in Spring 2022 The Colorado Agrivoltaic Learning Center The Colorado Agrivoltaic Learning Center is a project of the Colorado Nonprofit Development Center (CNDC). This team is dedicated to teaching our community and local schools about solar + agriculture + pollinator habitats + agrivoltaics.

JACK'S SOLAR GARDEN



Other / General Comments

There is an old city landfill behind the sewage treatment plant. Is the old city dump site part of the Steam/Sugar Mill project? If it is, does the City intend to clean up the landfill before building on the site? How much will it cost to clean up the old landfill?

People want to own something. Make it possible for young people to own.

<https://www.youtube.com/watch?v=cO6txCZpbsQ>



part two....

<https://www.youtube.com/watch?v=SfsCniN7Nsc&t=0s>



Who decided this area has to have a "steam" theme? Do we get to vote on that?

Yea - whose "vision" was this? — SAM IAM

These two areas are very different and should be treated as such. STEAM needs to integrate ti downtown culture and education. Sugar Mill need to align with our agricultural past and present: malting facilities, agricultural trade, creation, education. distribution. Some housing especially for-sale town homes and for-sale cottage zoning.

Agreed with Polly. A really intriguing comparison to the Sugarmill site is the Hazelwoof Green innovation area now being built out in Pittsburgh. It preserved the steel mill structure and origins while embracing the innovation and tech opportunities. — ANONYMOUS

Longmont has some key assets in the STEAM area - we own our own power system, and have great NextLight system. How are you integrating that background and those assets into companies that will come to the STEAM area?

I don't buy the whole STEAM idea as an "area." It seems forced and contrived. We all use science, technology, engineered products, arts, and math in our everyday lives. We have museums and schools and galleries and theatres already. This seems like a gimmick to attract high tech "Smart" tech companies and it seems clunky and contrived and not conducive to the creation of human psychology-friendly living environments.

What is happening with the Opportunity Zone section that overlaps with the Sugar Mill overall section. the Western Third.

How much re-evaluation of the Envision Longmont Plan have been done with the dynamic changes in work habits resulting from the pandemic? It is a different environment than it was when the plan was developed.

It is also unsustainable to compete with existing retail areas and commercial areas that are underutilized. Why are we looking at building new neighborhoods with retail when we have an underutilized main street for example.

I too worry about the impact on our existing Main St. businesses. I would prefer to see affordable housing, parklets and green corridors near the water ways, and perhaps a couple cultural centers that offer things we currently don't have (nature center, outdoor event venues) rather than a bunch of competing commercial space! — ANONYMOUS

This area should predominantly be preserved and rehabilitated. We have lost so much of the natural heritage to sprawl and lazy not well thought out development projects that are covered in KY Bluegrass and manicured landscapes that continue to suck our water supply dry. — ANONYMOUS

I like the idea of sporting and recreational fields.

Sandstone Ranch already has a bunch of baseball and soccer fields. I think it would be a shame to just duplicate something that already exists — ANONYMOUS

This is THE LAST thing we need! — ANONYMOUS

I would like to see low income lots where residents could actually for reals design their own homes, not have to choose from a couple predesigned options. And build them using genuinely renewable resources. Respect the desire of many people to have separate dwellings, light and privacy all around them. this is also a sustainable approach to housing because it taps into human desires and psychology. It is politically correct to want clustered and dense housing but that's a tired trope and we need to be honest with ourselves about what humans actually want and retire some of these easy to spout notions.

This is a contradiction - how to have both expensive options and keep it affordable? — ANONYMOUS

do not consider massive multi story structures. People really do not want to live in those; they tend to be temporary housing until people can afford better and it creates alienation and low community involvement to have lots of these apartments in a city or town.

I grew up in these kinds of buildings to a significant extent. They don't need to be massive, and they can be attractive to live in. They also allow folks on the margins to actually get into the market when condos are involved. — ANONYMOUS

Historical preservation

A lot of great ideas about energy, sustainability, natural habitat etc in this forum, but it seems like the historical preservation aspect is getting lost in all of that. The Sugarmill is an iconic set of buildings that represent the very foundations of the history of Longmont that can be seen from many areas across town and along the Front Range. It

would be a travesty to allow these structures to be lost. It should be a critical component of any plan to preserve these buildings with adaptive re-use.

BREAKOUT ROOM 4: ARTS, CULTURE, INNOVATION & PLACEMAKING

Sugar Mill + STEAM 3/2 Community Forum

MARINE SIOHAN MAR 01, 2022 09:41PM

Instructions

Goals 3 & 4 Questions

Should STEAM extend the existing character of Main Street, or create a complementary feel?

Should STEAM and Sugar Mill promote single 'feel', or establish themselves as separate areas? What is that 'feel'?

What places or projects have you seen that could inspire the study area? What parts did you like?

Goal 3: Define a Sense of Place

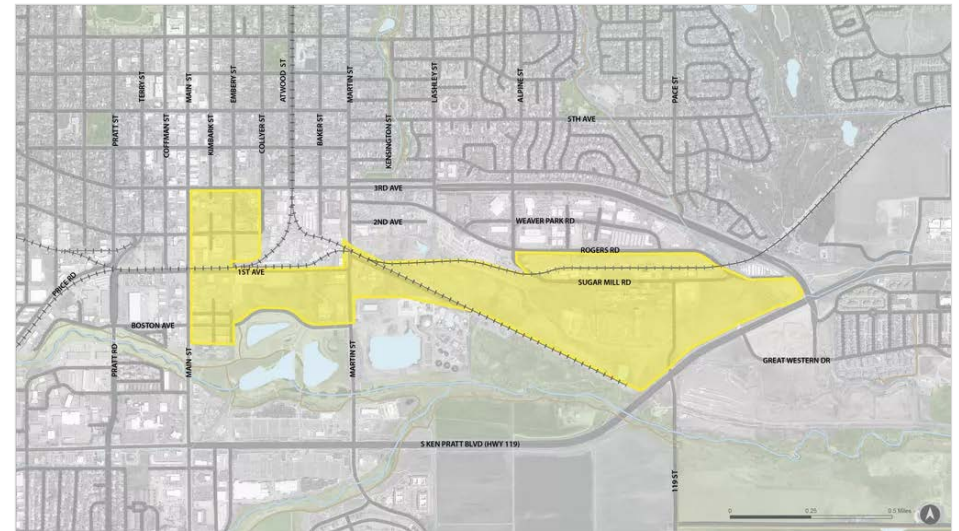
- Capitalize on and enhance westward views at the site's highpoint near Ken Pratt/3rd Ave.
- Provide transitional uses between Sugar Mill and Rogers Road, to connect new development to existing stable uses north of Rogers Rd.
- Use the site's location to create a visual gateway at the City's eastern entry and to the southern end of downtown.
- Integrate historic and industrial facilities and character into new development.

Goal 4: Create Community

- Identify a location for cultural facilities that reflect Longmont's history and culture.
- Provide space for flexible multi-use buildings that can evolve or adapt to fit a variety of uses.

- Provide public spaces, parks and/or plazas that provide a variety of formal and informal community programming and gathering.

Project Area Map



Question 1: Main Street Extension & STEAM

integrated mix - more life all hours even in downtown - safer, walkable,

Don't like "all hours even" think about negative impacts on surrounding natural areas. wildlife moves at night and less disturbance is preferred — SAM IAM

I don't want so much crowding it becomes the main tourist area. So wise use but keep the tourists on main street, and let them Discover this area

I would say it should be its own node or neighborhood - Downtown needs to remain compact for its identity.

STEAM and Sugarmill are completely different spaces in terms of Land. I don't understand why they're being linked like this??? — ANONYMOUS

I agree 100% with the comment above. — SAM IAM

STEAM

The use of STEAM is confusing to me, what proposals have been presented for the Science Technology Engineering and Math part of this?

Need to think about impacts of post covid realities in terms of work, recreation, entertainment etc — SAM IAM

STEAM

Re: multi-use facilities, lost of military facilities have auto hobby shops (garages with common tools, lifts, etc.). It would be wonderful to have a facility like that open to the community with some resident experts (could be volunteers or employees) to help teach and keep people safe.

Question 2: STEAM & Sugar Mill "Feel"

Performing arts (indoor) in Steam - outdoor live music venue (like fiddlers green) in Sugar mill.

A performing arts venue requires a large footprint in order to do it right and there is no space in the downtown area available. That is why the Sugar Mill site is more suitable.
— ANONYMOUS

integrate and connect STEAM and Sugarmill - River corridor could be part of the link.

Placemaking

Complementary areas with distinct identities.

I see no reason not to change style between the two areas. On its own, it adds to the experience.

These are two very distinct areas — SAM IAM

Connection between Main St and new area

I feel the SugarMill/STEAM should be separate and distinct from downtown in order to not create competition with downtown, but instead complimentary with a live/work, arts/community area with many opportunities to convene the community that downtown has no space for

I think there should be creative spaces in both locations. I've always thought it would be cool to have a local market including artists, the farmers market, etc. in the large silver building at the Sugar Mill. Maybe a small-dense neighborhood surrounding it to serve as an artists colony with galleries, etc.. In addition to a Performing Arts Center, concert venue in STEAM.

like all but the performing art center which is elitist and will hurt our fabulous Longmont museum — ANONYMOUS

Makerspace combos

When reviewing previous plans I feel like Arts and Science were kept so separate....if you want to capitalize on this as a STEAM area, technology (areas to innovate WITH the arts) would be good. Why isn't makerspace next to performing arts center?

there is a makerspace in Longmont <https://tinkermill.org/> — ANONYMOUS

We could us a more family friendly makerspace like a library sponsored makerspace
— ANONYMOUS

I personally think the sugarmill area would be better for new types of housing and steam would be better for a performing arts venue. The high end type of venue that has state of the art acoustics and attracts big names. Like Sanders theater at Harvard.

don't want/need a performing Arts Center. — ANONYMOUS

integrate light industrial/smaller manufacturing uses as par of the mix of uses

have a national competition for a mural on the roof of the large metal building - great gateway art opportunity...

rock climbing tourism

install rock climbing walls inside the silos. ...could be some of the tallest indoor top-rope rock climbing walls... would speak to the rock climbing culture part of Colorado

It should try to save as much of the original sugar mill structures as possible, along the lines of the way the fiber mills were repurposed in the Boston area.

Question 3: Inspirations

Green Music Center

Concert hall with a rear wall that opens up to an outdoor amphitheater

Google Image Result for https://gmc.sonoma.edu/wp-content/uploads/2021/09/@brennanspark_photography-3-of-3-scaled-1.jpg

GOOGLE



outdoor amphitheater/loud noise in close proximity to our greenway and natural areas would very negatively impact wildlife including raptors, deer, turkeys, bobcats, beavers, coyotes etc. --alll of which don't adapt like geese and raccoons.. — SAM IAM

Pearl Brewery - San Antonio is a useful model

Anything resembling San Antonio's riverwalk is problematic- where you see statues of the wildlife that used to BE there. — ANONYMOUS

Nice how they reused this space. Nice if could use the brick and mortar of sugar mill.

Nocturne | RiNo Art District

CATEGORIES: Places to Eat + Drink: Upscale | Places to Eat + Drink: Bar Tucked in an artfully restored warehouse at 1330 27th Street, lies Nocturne. Nocturne marries the culinary, cocktail and musical arts to create an unforgettable hospitality experience.

RIVER NORTH ART DISTRICT



Alamo Draft House Style Movie Theater

For more indie films.

Short rail running from main street out to Sugar mill might pull folks to downtown. Thinking along the lines of san francisco where the trolleys are almost an attraction.

Similar Venue

I think something along the lines of the Railyards in Santa Fe with galleries, an independent movie theater, restaurants, retail, etc.

Providence Rhode Island - great adaptive reuse of historic manufacturing buildings - arts , education, maker spaces, small business light industrial manufacturing

Examples...

Ponce Market in Atlanta - On the Atlanta High Line, mixture of retail, food hall, office/maker uses

Silos would be fantastic mural locations.

Similar spaces

Minneapolis Mill District - includes mixture of modern performing arts center and historic flour mills.

Love the riverwalk idea

love the riverfront walk idea - kind of like Estes Park, Riverwalk in San Antonio, TX

Please consider the payoff of all that cement, light, noise and disturbance on wildlife/habitat. Riverwalks are nice, but I'd rather keep our greenway/riparian habitat and see evidence of actual wildlife vs. statues of what used to exist.. — ANONYMOUS

Business incubators

For creatives

For restaurateurs (like Rosetta in Boulder)

For retail

But then how to tie in Science and Tech?

Consider housing that looks different and could function for work/live and intergenerational for example Pueblo style like in New Mexico

Castleberry Hill area in Atlanta had open galleries and artwalks once a month

Performing Arts Center, Museum, etc.

I believe the City completed a Market Study for both Hospitality and a Performing Arts Center? What were the results of the Study? Are these uses viable in this area? What do the property owners in this area want to do and who will provide funding for the new developments?

Performing arts center would need to be multipurpose and lit every night - not far fetched.

Maybe not. True multipurpose venues are difficult and expensive to build and operate. What if instead it was focused on being a great space for orchestra/choir/mostly unamplified music without trying to accommodate EVERY type of performance in a mediocre way? — ANONYMOUS

I agree with the opportunity to be connected to the Museum.

Performing arts facility to be located at the Sugar Mill site. Preserve the view of the mountains. Above the flood plain and there is a RR spur into the area.

What is a RR spur? — ANONYMOUS

We do not currently have a convention center or host space in town since the double tree (?) closed down.

I think the Performing Arts Center needs to be close to Main St to support walkability to downtown businesses. Financially, it needs to draw regionally and those people won't continue into downtown from the Sugar Mill.

Not sure ... even if PAC was near Main St, I think a lot of people would drive to/from downtown anyway. — ANONYMOUS

I would include outdoor performance areas using the existing topography.

If residential areas are also included, then environmental noise impact of an outdoor performance space should be considered — ANONYMOUS

ditto above comment — SAM IAM

Live Music Venue??

Longmont does not have a dedicated music venue for amplified music. I believe there is a need for a flat floor venue with 200-400 person capacity that could host regional and local bands.

This would be a different space than the LPAI Performing Arts Center which has been proposed, but instead more like the Aggie Theater in Fort Collins or the Fox Theater in Boulder. These are spaces with great sound, flat open floors for dancing and that also sell drinks. The symphony and other traditional performing arts groups need a space (which I also support!) with fixed seats and excellent acoustics for unamplified sound, but I believe these are different types of venues.

Performing Arts Center

Needs to be a multipurpose space that would be activated throughout the year - different sizes venues, small outdoor amphitheater.

Black Box Theater: YES!

RE: Performing arts center

Longmont already has several possible outdoor venues (Roosevelt Park, Country Fair grounds, main street/side streets) so this needs to be focused on an indoor center FIRST to be used all year round.

If there is a way to create an amphitheater near the refurbished river sides would be nice.

I sometimes wish we had a "River Walk" - for pop-up events as well as some resident commercial tenants - shops, restaurants).

There is a river walk event every year here in Longmont. Should we have an continuous river walk? — ANONYMOUS

A permanent home for Rhythm on the River would be nice! — ANONYMOUS

The price paid for a river walk is wildlife - where you see sculptures and statues of the animals that are displaced.. — SAM IAM

Longmont Museum?

How do these plans undermine and canabalize the Longmont Museum and the Stewart Auditorium?

If the PAC has a large capacity venue with a full size stage/fly space then it would complement Stewart Auditorium, not compete with it. — ANONYMOUS

The Arts facility would not compete with the Museum.

COMEDY CLUB

Comedians are DYING for a place to perform

What is the ideal size of a comedy club? — ANONYMOUS

An arts facility has very specific requirement to be successful

And who runs it when it can't afford to run itself? — ANONYMOUS

Operating budget should be really considered. — ANONYMOUS

Indoor Concert Venue for Amplified Music

I want to second Tim's statement about the need for an indoor music venue for up to 300ish for local, regional bands (ie. rock shows). Perhaps something like a House of Blues.

agreed — ANONYMOUS

What about the Stewart Auditorium?? — ANONYMOUS

While it is a great venue for some things, Stewart Auditorium has fixed seats. That doesn't work for shows with standing audiences/dancing that most rock shows have. — ANONYMOUS

Maybe like Washington's in Fort Collins? — ANONYMOUS

The Lyceum auditorium at Skyline HS is the biggest venue in Bldr. Co (besides CU). Available on weekends and weeknights w/additional parking at Centennial Park. Updating and improving this space is much more practical than building a new facility & makes sense economically in terms of tax payer investment. — SAM IAM

Community groups are always in search of affordable/accessible practice and performance space.

I love the practice rooms, recording booths and of course an indoor black box and mainstage ideas. Like Musician's Playground in Boston. — ANONYMOUS

Music District in Fort Collins is another local example of this! — ANONYMOUS

Other / General Comments

Whatever we build lets make it unique, aesthetically pleasing to look at, and state of the art efficiency with zero gas lines. Most buildings these days are boring boxes surrounded by a large parking lot. Research Kulturhus in Jonkoping, Reykjavik or generally Scandinavia for some inspiration.



Include Public more

I'm very disappointed that there was one on-line survey (with very limited options and no "none of the above" or write in options) and now this ONE public meeting engagement mtg. only - as was explained last week, the next phase will be bringing back a concept for people to weigh in on. Feels limited...

Entities that exist not being driven out

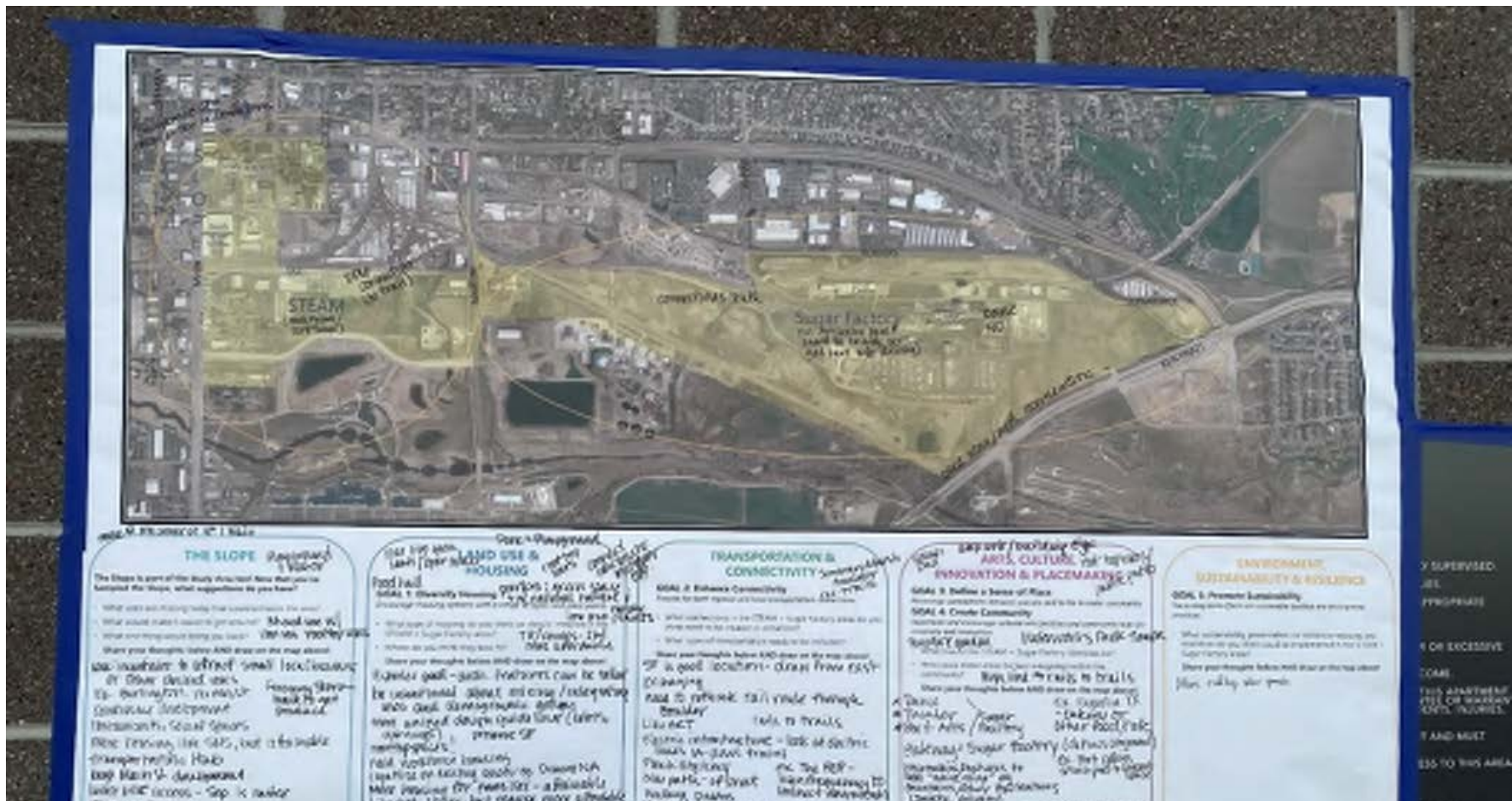
Make sure we don't drive businesses out that are there.

also hope to retain existing business affordability of the space

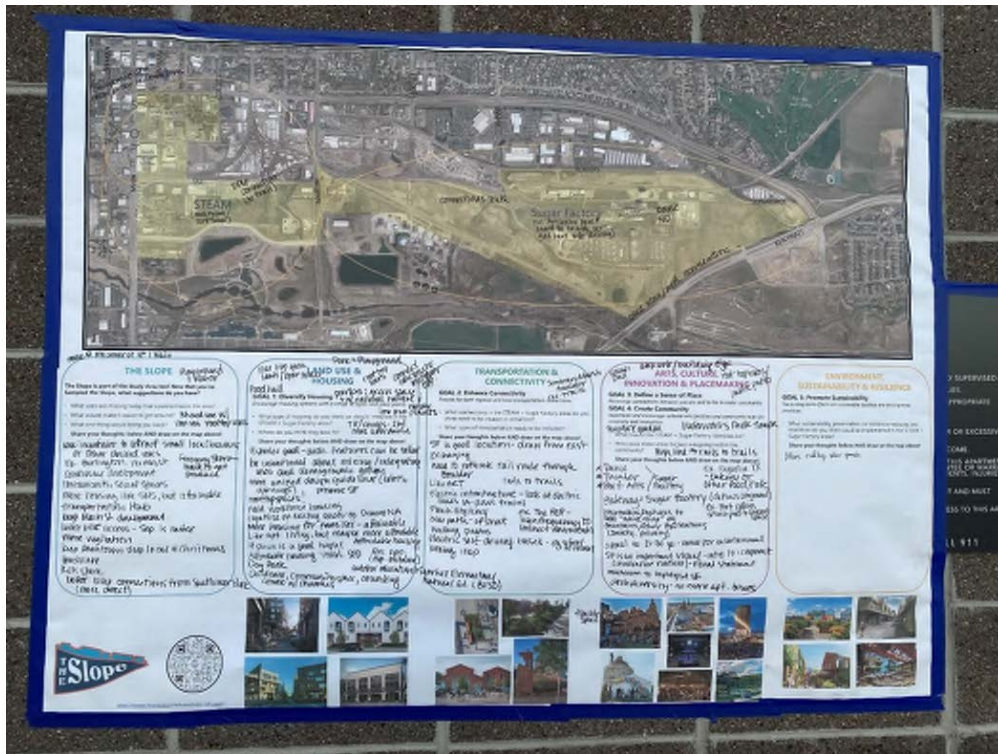
Air and space. Quality of life is important. Everything I've seen is dense.



Sample the Slope Event Community Input March 12, 2022



Sample the Slope Event Community Input March 12, 2022



The Slope

- Create more of a focal point in Downtown
- Need a more direct bike connection to greenway
- Re use @ NW corner of 4th & Main – the bank isn't an active enough use for this corner
- Use incentives to attract small local businesses or other desired uses

- Playground & Water
- Mixed uses w/non res rooftop uses
- Grocery store – want to be able to walk to get produce
- Ex. Burlington, Vermont
- Continue development that has already been taking place
- More restaurants and social spaces
- More housing like South Main Station, but attainable
- Transportation hub in this area is key (e.g. future transit station)
- Keep main street development
- Better bike access – separated facilities are better
- More vegetation
- Keep downtown shop local campaigns like the one that happens at Christmas
- Consider a hardware store
- Kids store – games, books, etc.
- Better bike connections from Southmoor Park (more direct)

Land Use & Housing

- Reuse the Sugar Factory buildings
- Parks & Playground – active uses, including pocket parks
- Roof Top Bars
- Food Hall
- Like big open lawn / open space where people can gather (e.g. picture under arts space on the board)
- Connect housing to outdoor opportunities
- TH/condos – 2 or more bedrooms that are more affordable than what we see now
- 4 stories is probably good, but architectural features could be taller
- Be intentional about mixing/integrating uses and demographic groups
- More unified design guidelines (colors, awnings)
- Preserve Sugar Factory
- Rooftop spaces (pubic)
- Need work force housing
- Capitalize on existing assets – e.g. Dickens Nature Area



- More housing for families – attainable
- Like apartment living (moved from NYC), but maybe more affordable housing
- Affordable housing
- 4 stories is a good height
- More affordable housing including single family options
- Dog park
- Childcare, community space, co working (could potentially combine some of these uses with churches)
- Outdoor education – e.g. Sanchez Elementary – Natural Ed (BVSD) - <https://www.lafayetteco.gov/2155/Nature-Kids-Discovery-Zone-Inspire-Trail>
- The factory is an eyesore – get rid of it

Transportation & Connectivity

- Need more direct bike connections, especially from downtown to greenway
- Would like to see a bike/ped connection from the greenway near E. Ken Pratt Blvd. up to the trail at 3rd Ave., on the north side of E. Ken Pratt Blvd.
- People need to be able to get to the Sugar Factory without driving, especially if there is something like an arts center here
- Need better connections to the south from the Sugar Factory area (i.e. from 3rd through the site)
- Sombra Marsh in Boulder is a good example (rec. trails)
- Sugar Factory is good location – draw from east
- EV charging
- Need to rethink rail route through Boulder
- Like BRT
- Rails to trails
- Electric infrastructure - look at electric buses vs. diesel trains
- Need high frequency to connections to downtown (e.g. the Hop)
- Flexibility is key
- Bike path – off street
- Walking paths



- Electric self-driving buses – (e.g. buses at the School of Mines)
- Biking loop

Arts, Culture, Innovation & Placemaking

- Change the name of STEAM – this is confusing and misleading – consider something like Park Front or Park Slope (because of proximity to Dicken’s Farm Nature Area)
- Splash Pad
- Keep the look and the buildings at the Sugar Factory
- Sculpture garden (e.g. Loveland)
- Waterworks Park – Tampa, FL
- Rooftop bars – public patios
- Highline project in NYC is amazing – rails to trails opportunities here
- Lots of excitement for dance, theatre, performing arts in the Sugar Factory area
- Ex. Magnolia, TX
- Bakery or other food/café
- Gateway = Sugar Factory (defines Longmont)
- Place making features to add “white noise” (e.g. fountains, other water features, which adds to the feel of safety and privacy)
- Appeal to 21-30 year olds with venues for entertainment
- Sugar Factory is an important visual – intro to Longmont (innovative nature) – visual statement
- Museum to highlight history of Sugar Factory
- Need more architectural diversity – not more apartment boxes
- Flexible space

Environment Sustainability & Resilience

- More rooftop solar panels
- Parks & Green Space with natural habitat & hardy low water plants
- Electric vehicle infrastructure



Growing Up Boulder & Indian Peaks Elementary School's Sugar Beet Mill Project Final Report

Summary of Findings
Published May 9, 2022



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Overview

About Growing Up Boulder

Growing Up Boulder (GUB) is Boulder's Child and Youth-friendly City Initiative established in 2009 as a formal partnership between the City of Boulder, Boulder Valley School District, and the University of Colorado. **GUB's mission is to make Boulder an exemplary child and youth-friendly city by offering young people opportunities to participate, deliberate and influence local issues that affect their lives.** Since its inception, GUB has engaged with 7,500 young people on more than 100 projects. In April 2021, after 12 years successfully housed at the University of Colorado's CEDaR Center, GUB became a nonprofit under the fiscal sponsorship of the Colorado Nonprofit Development Center (CNDC). GUB continues to partner closely with CU's CEDaR Center, BVSD, the City of Boulder and other key partners while expanding its efforts to impact communities beyond Boulder. In fulfilling its mission, GUB is helping create a generation of civically engaged citizens while promoting resilient and sustainable communities where all members feel they belong.



Sugar Beet Mill Redevelopment Project

Originally constructed in 1905, The Great Western Sugar Mill in Longmont, Colorado has been shut down since 1977. At the eastern end of the roughly 250-acre “Sugar Mill + STEAM Sub Area” is the old Great Western Sugar Company site, which has been a part of the city’s eastern gateway for almost 120 years. During Longmont’s most recent comprehensive plan update in 2016, the Sugar Mill buildings and surrounding land were identified as a priority for redevelopment, preservation, and adaptive reuse. Although the factory is located in unincorporated Boulder County, its historical ties make it an interest to Longmont. In August 2020, Urban Land Institute (ULI) Colorado convened a Technical Advisory Panel (TAP) composed of volunteer experts to provide recommendations related to the revitalization and reuse of the Sugar Mill¹. Currently, a city project team has been working with property owners, developers and the community as it develops a vision for Longmont’s “Sugar Mill + STEAM Sub Area”.

High level considerations identified for the redevelopment of the area:

1. Historical Preservation
2. Adaptive Reuse
3. Agricultural Heritage
4. Connectivity & Transportation
5. Sustainability & Green Infrastructure
6. Diverse Housing Options
7. STEAM Businesses & Collaboration
8. Environmental Remediation



Project Description: GUB & Indian Peaks Elementary School

Growing Up Boulder was invited to identify a group of Longmont youth who would be the end users of future redevelopment plans. An excellent partnership was found with the third grade community of teachers and students at Indian Peaks Elementary School. Indian Peaks, a bi-lingual STEM school where 90% of families receive free or reduced lunch services, has an instructional focus on Design Thinking, creativity, and innovation.

Given GUB and Indian Peaks’ close educational methods alignment, a unit highlighting the goals of the Sugar Beet Mill redevelopment project was outlined, and “The Sugar Beet Factory Design Challenge” was born.



¹ ULI Longmont Sugar Mill TAP Report.pdf

The Five Stages of Design Thinking

- Stage 1: Empathize—Research Your Users’ Needs
- Stage 2: Define—State Your Users’ Needs and Problems
- Stage 3: Ideate—Challenge Assumptions and Create Ideas
- Stage 4: Prototype—Start to Create Solutions
- Stage 5: Test—Try Your Solutions Out

Summary of Findings

Student Recommendations by Theme

Most frequently requested themes are listed below with examples of each:

- 1) **Active Play, Entertainment, Sports & Exercise:** students voiced their hopes for parks, playgrounds, obstacle courses and gyms; they recommend sports fields and courts for free and league play and ice rinks; they want areas to skateboard, play NERF gun and laser tag; students recommended arcades, go-carts, ziplines, and an interactive play center.
- 2) **Natural Outdoor Spaces:** students noted appreciation for outdoor spaces like community parks, gardens, an amphitheater, and water features such as water fountains and ponds; a treehouse, picnic spaces, and landscaping with native plantings are all included in their designs.
- 3) **Family-friendly:** to keep them (and their families) comfortable and energized, students want indoor and outdoor activities with expanded facilities like bathrooms, trash cans, access to food and refreshments, outdoor seating, and even day care; students desire gift and souvenir shops.
- 4) **Community-oriented & Inclusive:** young people recommended a large variety of activities for people of all ages and abilities; they thought about spaces for the very young (babies and toddlers) to those with special needs (a movie theater designed especially for those who experience physical disabilities).
- 5) **History of the Sugar Beet Mill & Museums:** in every project, students showcased ways to bring the factory’s past into the present day with ideas like virtual reality (VR) history tours and VR games (like imagine you’re a mill worker); they imagine photography, artifact displays, and historical centerpieces like sugar beet statues in water fountains.
- 6) **Sugar Beet Branding:** students want strong Sugar Beet branding and naming of the parks, museums, town center and recreation centers that are built on-site.
- 7) **Water Play:** students desire indoor and outdoor pools with upgraded amenities for enhanced fun and challenge such as slides, wave pool, diving boards, lazy river, hot tub; they want the very first underwater roller coaster in Colorado with real fish!
- 8) **Education & Art:** students want to learn new things and engage in STEM activities; they see a library with computers as important, and having opportunities to create their own art, build with Legos and enjoy art exhibits.

Afterwards, teachers led students in a discussion on what businesses were available to families in Longmont and what the community was in need of. All three classes agreed that ideas needed to meet four criteria. Each place would be: 1) a place for all ages, 2) a place that was family-friendly, 3) a place to have fun, and 4) a place which would include the history and agriculture of the Sugar Beet Mill.

Students believe that Longmont lacks family fun activities and their hope is that the Sugar Beet Mill remodel project can fill that gap. Students began the brainstorming process with a class “circle map” (see example below) which included a wide range of activities students wanted available to families, such as obstacle courses, water parks, museums, and theme parks. After looking at all three circle maps, the three classes narrowed their ideas down to four structures: a Museum, an Activity Center, a Park and a Town Center.



Next, classes were divided into small interest groups. Students were given time to research places around the world with the most unique structures. Some students looked at theme parks from around the world, while others researched key features in the United States, such as the Smithsonian Museum in Washington D.C. During their research, students completed entries in a daily engineering notebook. Once their research was complete, students discussed what their structures would look like.

During this design stage, students worked in their own groups and also gave peer feedback to other groups. At first, students drew their blueprints on whiteboards so they could erase and rearrange as much as they needed. Groups designed several rough drafts before completing their final blueprints, using “real” blueprint paper donated from one of the real estate developers. This process took a lot of patience and time for students, as they worked hard to complete a detailed blueprint to present to the group. Once the final blueprint was complete, groups worked on their presentation notecards and speaking parts to make sure their design was articulated clearly.



Recreation Center whiteboard draft



Museum group works on 2D blueprint

Student Presentations: Part 1

On Thursday, March 10, 2022, students presented their 2D project plans to adult guests connected to the project via a WebEx call. (See the google workbook in the [appendix](#) which holds student recommendations by group, frequency, and theme).

“Something I want Growing Up Boulder to know is this is the kind of activity that will make the next generation of Longmont citizens more thoughtful and more engaged in building and improving our communities in the future. These 3rd graders got creative and expressed their dreams about a place that has been idled and a blighting influence on their city for 47 years! Students were truly engaged!”

—President, St. Charles Town Company



Student groups 1-8, in order, present their recommendations during the March 10, 2022 presentation meeting

March - April: Design stages 4 & 5

After spring break, students reviewed their original 2D plans and began working in stages 4 & 5. Students sought to incorporate the feedback they received during the March presentation meeting, and from other research, to think about what their final prototypes would look like. Given a limited

City Council Meeting Attendance

Student Presentations: Part 2

"We had the opportunity to work within our community on a project that is important to us. GUB gave us time to see the progression in our work and how we can impact the community."

"I was pleasantly surprised by how much thought and effort went into these presentations and their models – what a creative way to present these plans! The students came up with some really creative uses, like zip lines, roller coasters, water parks, and laser tag, but they also identified some really practical uses that we know represent big needs in our community like housing, day cares, parks, and others."

A word cloud featuring various positive words and activities. The most prominent words are 'happy' and 'feel' in large, bold, blue font. Other words include 'excited', 'good', 'amazed', 'welcome', 'comfortable', 'joy', 'joyful', 'great', 'relaxing', 'special', 'center', 'entertained', 'rested', 'alone', 'families', 'park', 'kids', 'field', 'activities', 'placed', 'calm', 'home', 'fill', 'waterpark', 'area', 'recreation', 'hotel', 'basketball', 'welcomed', 'changed', 'go', 'just', 'stay', 'soccer', 'wellness', 'wellness center', 'wellness program', 'wellness services', 'wellness team', 'wellness department', 'wellness committee', 'wellness council', 'wellness board', 'wellness alliance', 'wellness partnership', 'wellness collaboration', 'wellness partnership', 'wellness collaboration', 'wellness partnership', 'wellness collaboration'. The words are arranged in a circular pattern, with 'happy' and 'feel' at the top and 'welcome' at the bottom. The colors are primarily blue, green, and yellow, with some words in red and orange. The background is white.

3

Conclusion

Students at Indian Peaks Elementary School took very seriously the opportunity to participate in community engagement efforts to reimagine the former Sugar Beet Factory site. Using a Design Thinking process framework (empathize, define, ideate, prototype, and test), students first learned about the history of the factory and the role it played in Longmont's early history. Going through the 5 step process, they designed 2D and 3D representations of what they imagined the redeveloped Sugar Beet Mill site to be. All eight groups (History Museums, Theme Park, Town Center, Rec Centers, Activity Center, and Community Park) seek to make Longmont a better, more livable community. Students have prioritized creating a site that is family-friendly, inclusive, and one that involves time outdoors, engaged learning and a whole lot of active fun! The details students shared represent a great variety of activities for people of all ages, so that there's literally something for everyone. Having conveniently located family-friendly amenities in Longmont means that families won't have to drive long distances for their entertainment pursuits. Finally, students speak adamantly about the importance of creating a legacy site where the history of the Sugar Beet Factory is preserved for future generations.

Partners and Appreciation

GUB would like to thank the partners who gave their time and energy to make these engagements possible, especially the teachers and students at Indian Peaks Elementary School. We acknowledge that partner work such as this requires more time and energy, and we appreciate everyone's efforts. Adult guests who were involved in this project were especially respectful—listening to every word students shared—and considerate in their interactions with students. It was a great experience all around. Many thanks to all!

- City of Longmont
 - Erin Fosdick, Principal Planner, City of Longmont
 - Sandra Seader, Assistant City Manager
 - Glen Van Nimwegen, Planning Director, City of Longmont
 - Tim Waters, City Council
- Growing Up Boulder
 - Cathy Hill, Education Director
 - Mara Mintzer, Executive Director
 - Nettie Skievaski, CU ENVD Undergraduate GUB Intern
 - Olivia Szeliga, Development and Communications Coordinator
 - Deryn Wagner, Senior Project Planner
- St. Vrain Valley School District
 - Indian Peaks Elementary School
 - Alexandra Downing, STEM Coordinator
 - Susie Hidalgo-Fahring, 3rd Grade Teacher
 - Alice Nixon, 3rd Grade Teacher
 - Nick Searls, 3rd Grade Teacher
 - Kathi Jo Walder, Principal Planner

- Board of Education
 - Karen Ragland, School Board Member
- Remington Homes Colorado
 - Matt Cavanaugh, Director of Land Acquisition & Entitlements
- Stantec
 - Nancy Locke, Principal
- St. Charles Town Company
 - Charles Wooley, Founding Principal & President
 - Elsa Harberg, Development Assistant
- Trestle Strategy Group
 - Marine Siohan, Project Manager
 - Danica Powell, Founder & Owner

Appendix

[Guest reflections](#) after students presented their prototypes

[Painting prototype](#) photos

[Part 1 samples of students working on concepts](#)

[Student recommendations workbook](#) (data representing group ideas, frequency chart, and themes for entire project)

[Student research tidbits of note](#)

Final note: The April 25, 2022 WebEx recording of students presenting their prototypes, additional photos, and other project materials are available upon request. If interested, please email cathy@growingupboulder.org.

Project Report

13 February 2019 - 22 May 2022

Engage Longmont Sugar Mill + STEAM Sub Area Plan

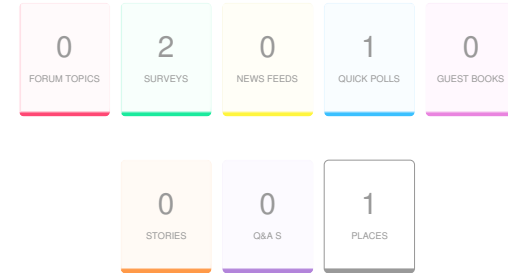


Visitors Summary



Aware Participants		Engaged Participants			
3,556		681			
Aware Actions Performed	Participants	Engaged Actions Performed	Registered	Unverified	Anonymous
Visited a Project or Tool Page	3,556				
Informed Participants	1,129	Contributed on Forums	0	0	0
		Participated in Surveys	15	0	469
Informed Actions Performed	Participants	Contributed to Newsfeeds	0	0	0
Viewed a video	0	Participated in Quick Polls	9	2	228
Viewed a photo	0	Posted on Guestbooks	0	0	0
Downloaded a document	98	Contributed to Stories	0	0	0
Visited the Key Dates page	13	Asked Questions	1	3	0
Visited an FAQ list Page	0	Placed Pins on Places	0	0	0
Visited Instagram Page	0	Contributed to Ideas	0	0	0
Visited Multiple Project Pages	411				
Contributed to a tool (engaged)	681				

ENGAGEMENT TOOLS SUMMARY



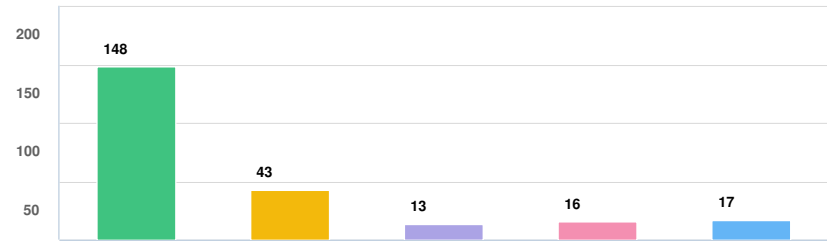
Tool Type	Engagement Tool Name	Tool Status	Visitors	Contributors		
				Registered	Unverified	Anonymous
Qanda	Q & A About This Project	Draft	148	1	3	0
Place	Test Marine	Draft	0	0	0	0
Survey Tool	Project Goals Survey	Archived	505	5	0	247
Survey Tool	Feedback on Plan Alternatives	Archived	346	10	0	228
Quick Poll	What kinds of future use would you like to see incorporat...	Published	256	9	2	228

ENGAGEMENT TOOL: SURVEY TOOL

Feedback on Plan Alternatives

Visitors	346	Contributors	238	CONTRIBUTIONS	239
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Two options have been presented for the STEAM area. Once focused on creating a Cultural District with space for a new perfor-

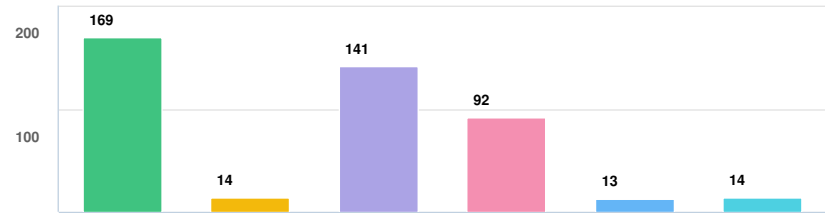


Question options

- Definite preference for Cultural District Alternative
- Slight preference for Cultural District Alternative
- No distinct preference for either alternative in the STEAM area
- Slight preference for Mixed-use Campus Alternative
- Definite preference for Mixed-use Campus Alternative

Optional question (236 response(s), 3 skipped)
Question type: Checkbox Question

Both alternatives contemplate an extension of Emery as a shared street. As we think about possible design alternatives for ...

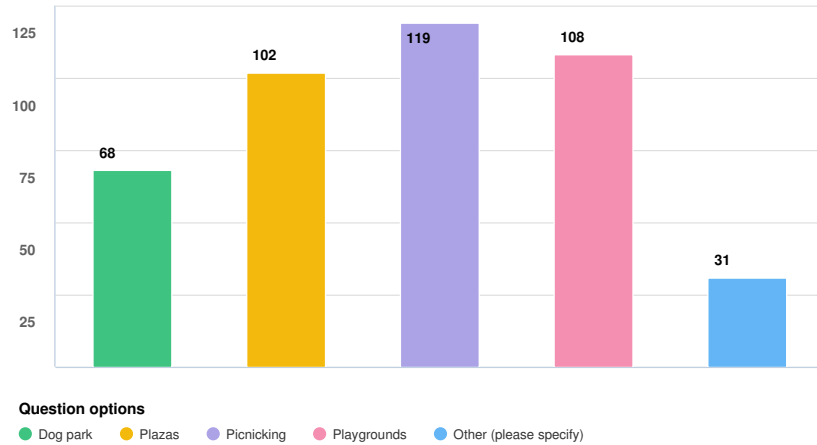


Question options

- A facility that prioritizes people walking and on bikes
- A street that moves cars
- A space that can be closed for events and public gathering
- A multimodal or "complete" street that accommodates all travel modes in a truly shared space (e.g. woonerf)
- I don't think Emery should be extended
- Other (please specify)

Optional question (234 response(s), 5 skipped)
Question type: Checkbox Question

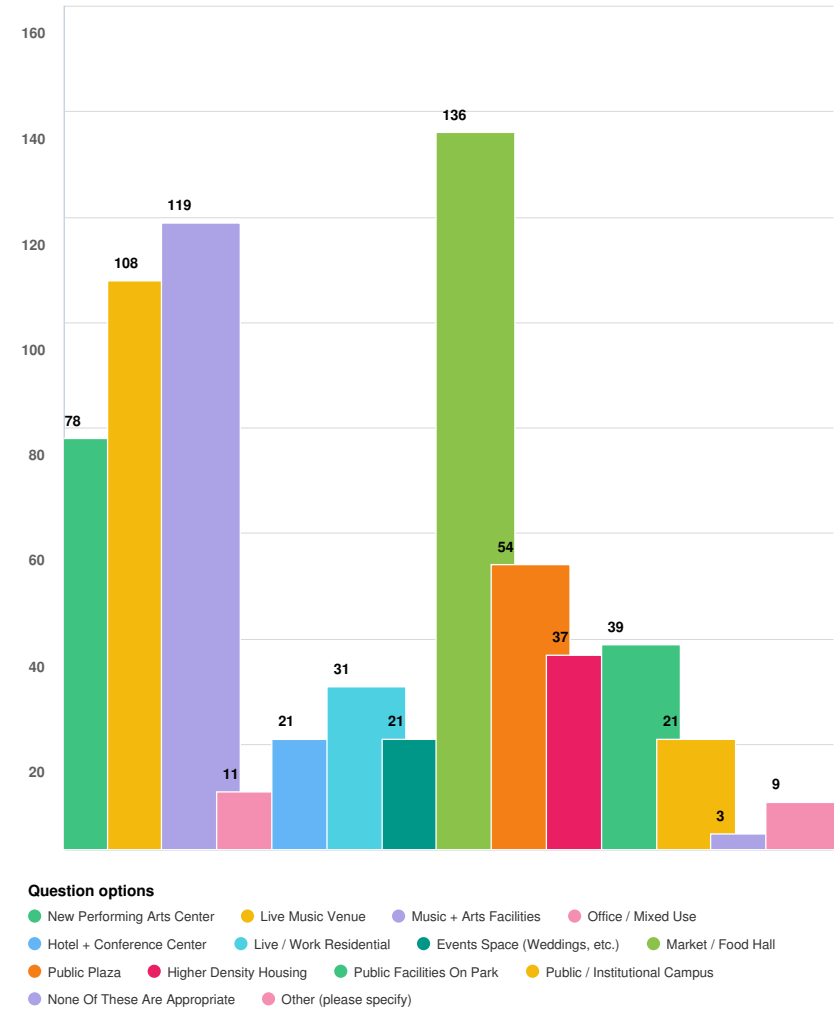
The STEAM area is adjacent to the Dickens Farm Nature Area and provides access to natural areas and many amenities. In thin...



Optional question (235 response(s), 4 skipped)

Question type: Checkbox Question

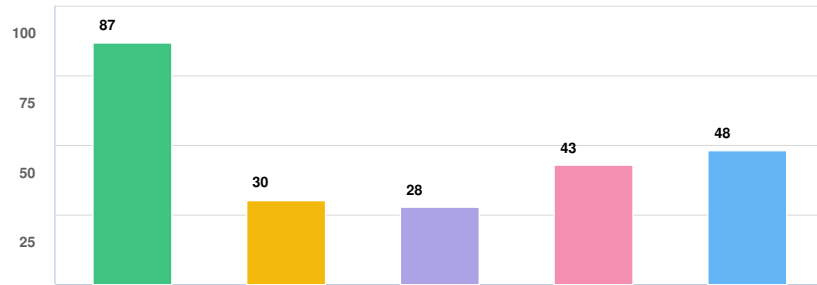
In thinking about the types of uses proposed in the alternatives for the STEAM area, what uses do you think fit best here? ...



Optional question (238 response(s), 1 skipped)

Question type: Checkbox Question

Two options have been presented for the Sugar Factory area. Once focused on creating a Cultural Campus with space for indoor...



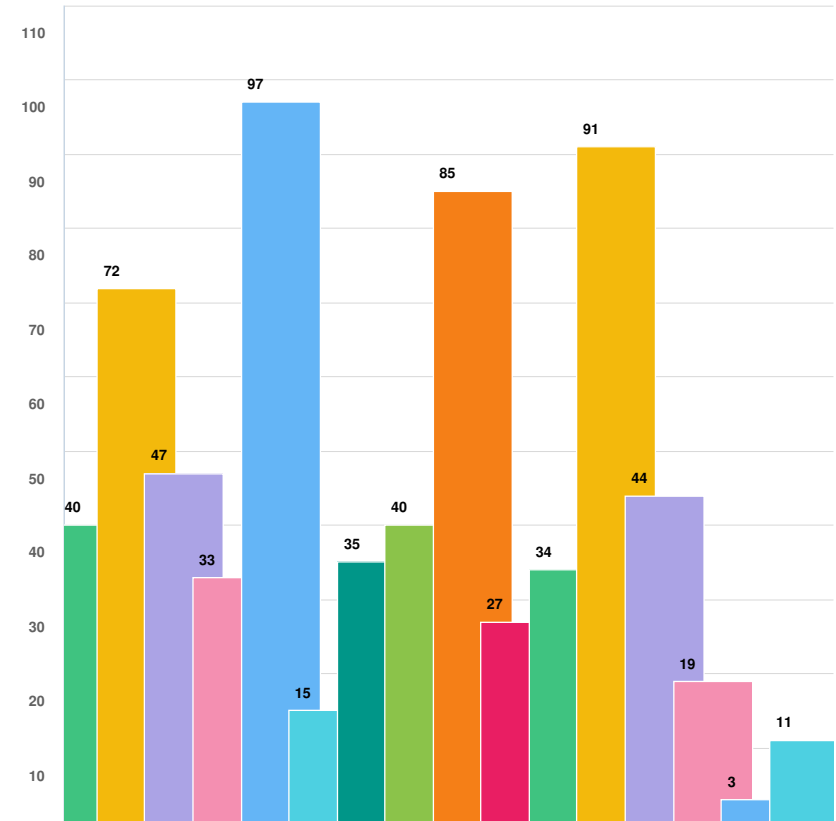
Question options

- Definite preference for Cultural Campus Alternative
- Slight preference for Cultural Campus Alternative
- No distinct preference for either alternative in the STEAM area
- Slight preference for Agricultural Hub + Maker Doer Campus Alternative
- Definite preference for Agricultural Hub + Maker Doer Campus Alternative

Optional question (235 response(s), 4 skipped)

Question type: Checkbox Question

In thinking about the types of uses proposed in the alternatives for the Sugar Factory area, what uses do you think fit bes...



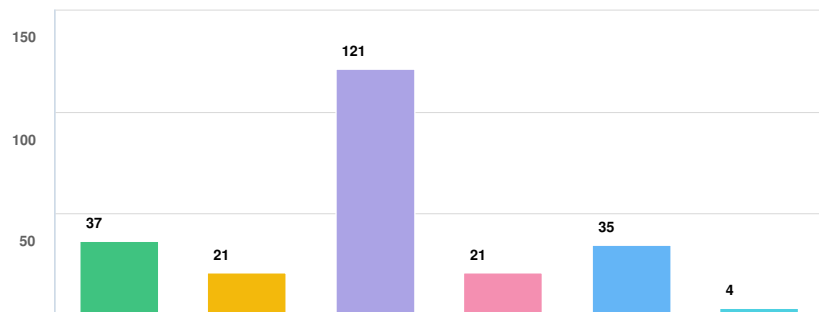
Question options

- Indoor Music Venue
- Outdoor Music Venue
- Cultural Park
- Artists Studios
- Farmers Market
- Events Space (Weddings, Etc)
- Food Incubator Space
- Agricultural Tech/Industry
- Urban Greenhouses
- Learning Centers
- Neighborhood Retail
- Food & Beverage Uses (Restaurants, Breweries, Distilleries, etc.)
- Missing Middle Housing (e.g. Townhomes, 2-, 3-, 4-plex, Smaller Apartments)
- Higher Density Housing
- None Of These Are Appropriate
- Other (please specify)

Optional question (237 response(s), 2 skipped)

Question type: Checkbox Question

Are you more likely to visit a redeveloped Sugar Factory area if it is focused on events (like concerts and museums) or on ...



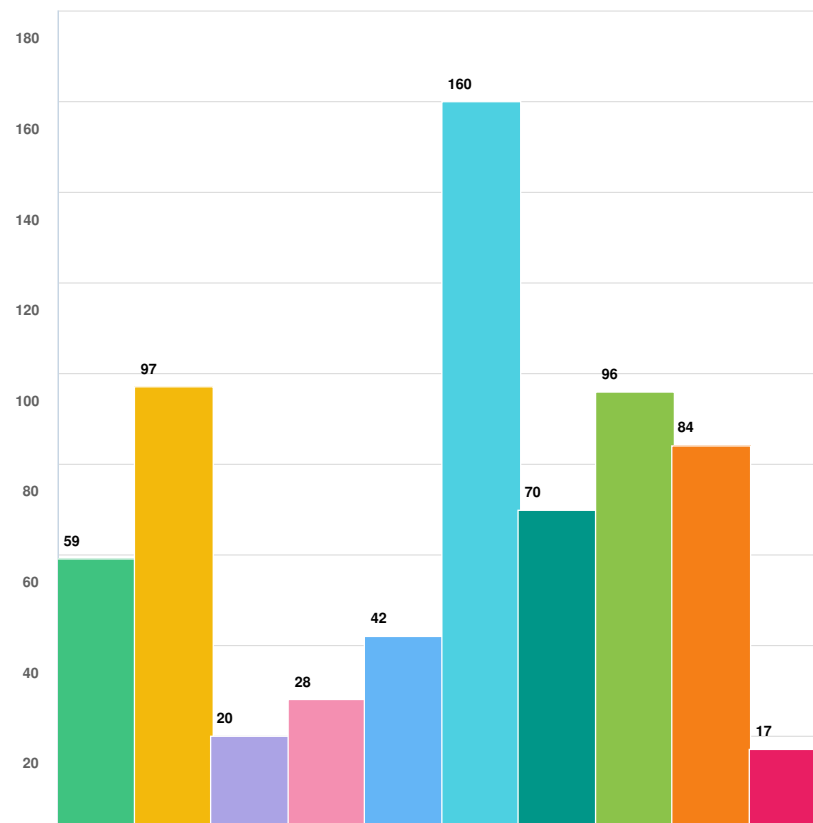
Question options

- Definitely more likely to visit for events
- Somewhat more likely to visit for events
- I would visit for both events and everyday activities
- Somewhat more likely to visit for everyday activities
- Definitely more likely to visit for everyday activities
- I would not be likely to visit the area

Optional question (238 response(s), 1 skipped)

Question type: Checkbox Question

In thinking about the design and character of the Sugar Factory area, what elements are important to consider from a placem...



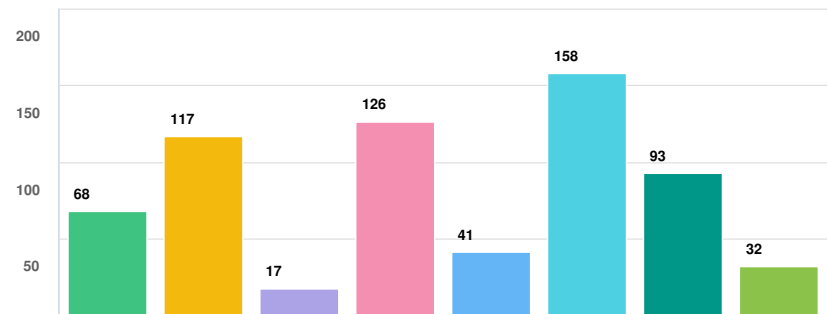
Question options

- Consistent architecture and/or design through the district
- Architecture and design similar to the Sugar Factory buildings
- Architecture and design that are different from the Sugar Factory buildings
- Signage and wayfinding elements
- Gateway elements (e.g. signage and/or art at key locations)
- Public spaces where people can gather
- Architectural or design elements that add visual interest to buildings (e.g. towers, corner elements, awnings, etc.)
- Public art
- Complete streets that accommodate all modes of travel safely
- Other (please specify)

Optional question (238 response(s), 1 skipped)

Question type: Checkbox Question

There are lots of opportunities for integrating parks and open space within the Sugar Factory District. What are your top t...

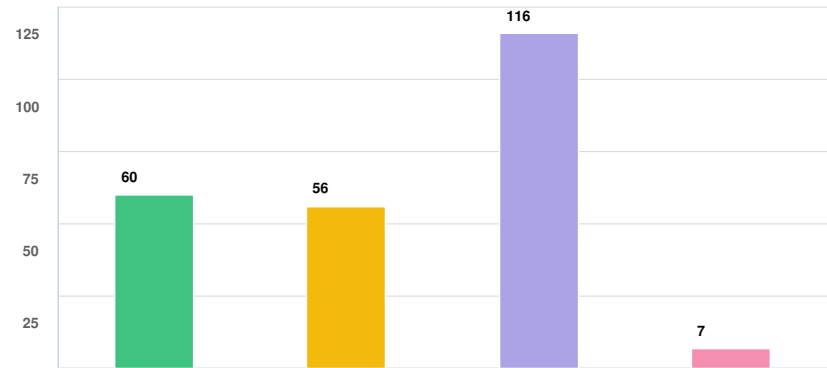


Question options

- Dog park
- Informal individual/small group play opportunities (e.g. frisbee / kickball)
- Formal team play opportunities (e.g. soccer / baseball)
- Picnicking
- Traditional playgrounds
- Playgrounds focused on nature play
- Adventure / ropes courses – all ages challenges
- Other (please specify)

Optional question (237 response(s), 2 skipped)
Question type: Checkbox Question

Council has directed that a future performing arts center be located in the STEAM area; if the sub area plan also includes ...



Question options

- STEAM district
- Sugar Factory district
- No preference
- This use is not appropriate for either area

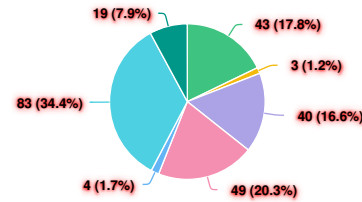
Optional question (238 response(s), 1 skipped)
Question type: Checkbox Question

ENGAGEMENT TOOL: QUICK POLL

What kinds of future use would you like to see incorporated into the restoration/reuse of the historic Sugar Mill buildings?

Visitors	256	Contributors	239	CONTRIBUTIONS	241
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What kinds of future use would you like to see incorporated into the restoration/reuse of the historic Sugar Mill buildings?



Question options

- Entertainment activities
- Office, co-working, or other entrepreneur space
- Community or civic uses (e.g., museum, theater, etc.)
- Arts and creative industries
- Retail shops
- Food hall, restaurants, indoor market, or other food/beverage type uses
- Experiential learning experiences or other educational uses

Mandatory Question (240 response(s))

Question type: Radio Button Question

5.4 MARKET CONDITIONS

Market Assessment Review

Residential Market Snapshot



TECHNICAL MEMORANDUM

Longmont Area Market Assessment Review STEAM + Sugar Mill Subarea Plan

I. Introduction

In December 2020, a *Longmont Area Market Assessment* was completed for the Longmont Economic Development Partnership and the Longmont Downtown Development Authority. This assessment provided information on the Longmont area office and industrial markets and demand potential that could be captured in the Longmont area over the next decade.

For this task, CIVISTRUCT Strategy + Development ("CIVISTRUCT") reviewed the *Longmont Area Market Assessment* to recheck the market assumptions and the demand potential for office and industrial uses outlined in the report. In addition, while retail was not evaluated in the market assessment, CIVISTRUCT has provided market observations with respect to retail opportunities that could potentially be targeted within the STEAM + Sugar Mill study area. The goal of this review is to provide commercial market information to help inform the optimal land use plan for the STEAM + Sugar Mill study area.

II. Office Market

Market Overview

The Longmont area office market comprises nearly 2.9 million square feet of office space. Longmont's office inventory is dominated by older Class B/C, single-story office space. Longmont's office inventory is smaller when compared to other competing communities. The average office building size for the Longmont area is approximately 10,700 square feet (compared to 20,300 square feet for City of Boulder and 44,200 square feet for the Denver NW market). More than half of Longmont's office buildings are 5,000 square feet or less in size.

The 2020 *Longmont Area Market Assessment* reported Longmont's office vacancy rate of 12.1% as of 3Q 2020 (the highest vacancy in the region) with average lease rate of \$15.80 per square foot as the office market experienced deteriorating market conditions brought on by the pandemic-induced recession. The Longmont office market reported negative office absorption of -37,780 square feet (3Q 2020) as more office space was vacated than leased up (including rise in sub-lease space).

Over the past year since the *Longmont Area Market Assessment* was completed, market fundamentals for the Longmont office market have improved as office demand has increased and companies commence reoccupying office space. Longmont's vacancy rate dropped to 10.1% and average asking rents increased to \$16.10 per square foot (Catalist Research 4Q 2021 Office Market Trends). While Longmont vacancy rate is higher than the Boulder County / NW Denver average (9.8%), it is well below the Metro Denver (19.9%) and the US average (17.6%) as of the 4Q 2021. In addition, Longmont experienced positive net absorption of +9,215 square feet of office space at year end 2021 as more office space was leased up than vacated compared to Boulder County / NW Denver (-19,250 square feet) and Metro Denver (-108,000 square feet) which both experienced negative absorption of office space as more space was vacated than leased up and occupied.

Estimated Market Demand

Employment growth (or contraction) in office-based industries, location of jobs, densification (or de-densification), and remote working / flexible schedules, play a central role in determining demand for office space in a locality, whether through development of new office space, absorption (e.g., leasing up) of existing vacant or underutilized space, or adaptive reuse of existing space. The COVID-19 pandemic has had a dramatic impact on office demand not only in Longmont but throughout Metro Denver and the nation. The pandemic has injected a large dose of uncertainty into both the amount and location of office demand. One of the most difficult challenges is what the permanent long-term office demand impacts will be on how and where people work when so much is still unknown at this point and companies are still making business decisions on how much office space they need, where office talent wants to live, and how often employees should come into the office, if at all.

The *Longmont Area Market Assessment* estimated Longmont's projected office demand potential between 2020 – 2030 at 152,500 - 190,000 square feet of space. This demand forecast took into consideration the ongoing impact of the pandemic on office space and expected less demand over the next several years. For example, the demand forecast was based on a forecasted slower office employment growth (7.9% between 2020-2030), office de-densification (increase to 250 square feet per employee) and an increase of work from home and hybrid work schedules (5-20% depending on the industry) that is expected to reduce demand for office space over the next several years. One year after the *Longmont Area Market Assessment* was completed we believe these assumptions are still relevant on the potential demand for office space in the Longmont area as much of the office market demand remains uncertain and hybrid / work from home becomes more permanent for certain office-using sectors.

According to the latest research by the National Bureau of Economic Research, the report authors project that the share of remote workdays will ultimately quadruple to 20% from 5% prior to the pandemic. CBRE Economic Advisors forecasts a 9.1% decrease in office space demand, while Green Street Advisors report suggests that demand could drop by up to 15%. In addition, a survey of real estate development professionals as part of *2022 Emerging Trends in Real Estate* (ULI and PwC) believe that no more than 75% of office workers will come to the office three or more days a week in 2022 with quarter of survey participants expecting a decline of 15% or more. While the report found that there is optimism within the industry for office prospects in 2022, there has been a substantial shift in the functionality of offices moving forward. Survey respondents believe that the office market is unlikely to return to its pre-COVID state, and with the success of work from home and hybridization of the workplace, demand for office space will fall between 5% and 15% in the next three years.

The most recent employment projection for the Longmont area forecast an increase in office employment of 13% between 2021-2031 (Emsi) as jobs return at a faster rate when compared to earlier in the pandemic. This forecast is higher when compared to the 2020-2030 forecast of 7.6% that was part of the *Longmont Area Market Assessment* (Emsi) which was projecting slower employment growth primarily due to job losses that occurred early in the pandemic. Emsi's projections are forward projections of past employment trends and are based on historical employment data with slight adjustments for BLS QCEW datasets and state projections.

The market assessment assumes 5-20% (depending on industry) of office employees will work from home or have a hybrid work schedule. This is in line with the latest research data and market forecasts which are showing a decline in office space as the office market resets itself due to the pandemic and accelerate office trends of work from home / hybrid work schedules. For example, an *Office Space Demand Forecast* report by the NAIOP Research Foundation posits that while an increase in remote and hybrid work arrangements will probably reduce demand for office space in the long run, any such reduction will be offset in the coming years by employment growth in office-using industries.

CIVISTRUCT reviewed the *Longmont Area Market Assessment* and assumptions utilized to determine the potential office demand forecast. After reviewing the latest market research data on the potential long-

term impact of the pandemic on office demand and most recent office employment forecasts for the Longmont area, it is our opinion that the 10-year forecast of potential office demand between 152,500 - 190,000 square feet of space is reasonable. Should Emsi's office employment forecast of 13% growth between 2021-2031, or 1.1% annually, come to fruition, and Longmont continues to gain jobs as businesses add employees and new companies grow and locate in the area, this could point to stronger demand for office space in the Longmont area above this demand forecast.

Target Opportunities for STEAM + Sugar Mill Study Area

The STEAM + Sugar Mill study area offers target opportunities to capture Longmont's office demand potential over the next decade. As noted in the *Longmont Area Market Assessment*, the Longmont area has a dearth of modern, high quality, flexible and sustainable office space that is attractive to today's modern office users, including larger floorplate space to attract tenants, such as technology companies. In addition, Longmont is also strategically positioned to capture potential spill over demand from Boulder for companies seeking attractive, lower cost options.

The STEAM + Sugar Mill study area offers the opportunity to meet this demand with construction of new modern office space (if financially viable), leasing existing vacant and underutilized office space, and adaptive reuse of non-office space to office space (including the historic sugar mill buildings) as a stand-alone project or part of a mixed-use development. Some of these target opportunities were identified in the Urban Land Institute Colorado – *Revitalizing Longmont's Great Western Sugar Mill* Technical Advisory Report (August 20-21, 2020) that identifying creating an Agri-Hub for the Sugar Mill study area to build off Longmont's agricultural heritage.

Specific target opportunities include:

- Focus on Agri-Hub employment related uses such as agricultural technologies and research and development, such as temperature and moisture sensors, weather tracking, satellite imaging, minichromosomal technology, RFID technology and robotics
- Food and beverage business incubation and entrepreneurship seeking flex office space, including natural food products, organics, breweries, and distilleries
- Professional and business services, including finance, engineers, architects, designers, consulting, management and other professional services, which are forecasted to have the strongest growth in office employment in the Longmont area over the next decade (+20%)
- Live-work and co-working spaces to support Longmont's entrepreneurship and startup culture for small businesses and startups with space to grow their businesses
- Technology and information businesses looking for flex/office space (Boulder / Broomfield County the technology hub of Colorado), including spill-over demand from Boulder due to their limited supply and higher cost environment
- Medical office / healthcare sector to capitalize on projected steady growth driven by Longmont's aging population (+21% growth over next 10 years) lifting the need for in-person services
- Building new modern office product, including larger floorplate space, incorporating the latest office design to attract targeted industries and make the buildings safer and more efficient
- Transform outdated, older office product within the study area with modern, flexible, sustainable office space attractive to today's modern office users

III. Industrial Market

Market Overview

The Longmont area industrial market comprises 9.1 million square feet of industrial space in 279 buildings. Longmont's industrial inventory is diversified with 55% of this space classified as flex/R&D space. Warehouse/office space represents 28% of the total Longmont area industrial inventory, manufacturing space comprises 7% of inventory, and warehouse/distribution space comprises 5% of the inventory.

The 2020 *Longmont Area Market Assessment* reported Longmont's industrial vacancy rate at 14.6% as of 3Q 2020 with approximately 1.3 million square feet of vacant industrial space. The vacant space was concentrated in the flex/R&D sector which accounted for 92% of Longmont vacant industrial space. Most of this vacancy was concentrated in the former Maxtor space (460,000 square feet). If you remove the former Maxtor space out of the industrial vacancy, the Longmont area industrial vacancy would drop to 9.6%. Longmont area's average industrial market lease rates (triple net) was \$9.71 per square foot as of 3Q 2020.

Since the *Longmont Area Market Assessment* was completed in 2020, the regional and national industrial market has been one of the strongest performing real estate sectors through the pandemic, primarily driven by strong demand due e-commerce. Although the pandemic brought on new challenges for the industrial market, with port congestion, material shortages, and commodity pricing skyrocketing, the market is stronger than ever with market fundamentals (vacancy, rents, absorption) in the US hitting new records in 2021.

Market fundamentals for the Longmont industrial market have strengthened as industrial demand increased and manufacturing employment has regained jobs. Longmont's industrial vacancy rate dropped to 7.23% and average asking rents are holding steady at \$9.26 per square foot (Catalist Research 4Q 2021 Industrial Market Trends). While these trends are positive, they are underperforming when compared to Denver Metro and national industrial trends. Longmont's industrial vacancy rate is higher than the Boulder County / NW Denver average (4.7%), Metro Denver (6.1% vacancy) and the US average (3.7%) as of the 4Q 2021. Mirroring regional and national industrial trends of strong demand and limited supply, Longmont experienced strong positive net absorption of +136,607 square feet of industrial space as more industrial space was leased up and delivered than vacated as of 4Q 2021. While supply chain issues and lingering employment issues still threaten local and regional and national economic growth, Metro Denver recorded +3.8 million square feet of net absorption and the US market recorded +145.2 million square feet of net absorption as record setting demand outpaced supply for the fourth quarter in a row.

Estimated Market Demand

The *Longmont Area Market Assessment* estimated the 2020-2030 market demand for industrial space by examining the demand potential for three sub-categories of industrial space, each of which have their own drivers of demand for space. These sub-categories included: flex/R&D, warehouse (also known as logistics), and specialized industrial (e.g., manufacturing). These are the predominant types of industrial space found in the Longmont area, as well as, in the larger regional and national industrial markets.

Flex/R&D

While there are many variables that impact flex demand and decisions where businesses locate, the projection of future demand for flex space in the Longmont market is predominately based on projected employment growth in those key industry sectors that locate in flex space, such as information, construction, and professional and business services. Flex space can be loosely defined as any building

that is designed to service a variety of uses, such as office space, research and development, lab, showrooms, warehouse and light manufacturing.

Like the office market demand analysis, the flex-based employment forecast was adjusted to consider the potential longer-term impact of COVID-19 on remote work and hybrid work schedules. Based on the Emsi flex-based employment projections and adjusting this employment change by industry sector to account for employees working in an industrial space (vs. at home or hybrid schedule), the *Longmont Area Market Assessment* estimated the total potential flex demand that could be captured in the Longmont area could range between 150,000 and 250,000 square feet between 2020-2030. This estimate was based on a slower growth forecast (7.7%) for these industry sectors during this 10-year period. More recent Emsi data are forecasting stronger employment growth (13%) in these industries in the Longmont area between 2021-2031 which suggests potential stronger demand for flex space over the next decade above the demand potential as outlined in the market assessment. The assumptions utilized in terms of the allocation of employment to an industrial/flex building (50%-100%) vs. an office building and percentage of employees working in an industrial space (50%-95% depending on occupation) vs. remotely are reasonable.

Warehouse

As noted in the *Longmont Area Market Assessment*, the primary drivers of determining warehouse demand are complex, have changed over time and are continuing to change especially with the rise of e-commerce. There are many economic and real estate variables that go into determining warehouse demand and space to accommodate this demand such as building and property characteristics, locational attributes, access to labor and population centers, replacement demand, and international trade issues that impact warehouse demand. The Longmont area warehouse demand projection considered these factors with a focus on historic and projected net absorption trends for the Longmont area, to a limited extent warehouse-based employment change for the period 2020-2030, and the increasing impact of e-commerce on the warehouse market.

Despite the pandemic and ensuing impact to supply chain issues, warehouse space is still one of the top performing real estate assets in the US today with strong demand, low vacancy rates, rising rents and strong absorption, primarily with the rise in e-commerce demand. The warehouse market is well positioned to weather this challenging time and into the foreseeable future. The market assessment estimated the total potential warehouse demand for the Longmont area between 225,000 and 450,000 square feet over the next 10 years. This assessment was based on several factors, including warehouse-based employment growth, historic net absorption trends and the impact of e-commerce. Based on recent market trends for the Denver Metro area, continued strong demand for warehouse space because of e-commerce, and lack of new warehouse construction in the Longmont area (there is only one warehouse/flex space currently under construction – 97,500 sq. ft. at the Longmont Business Park), there could likely be stronger demand for modern warehouse space (with at least 24 feet clear height and multiple loading capabilities) in the Longmont area over the next decade above this demand estimate as demand outstrips supply.

Manufacturing

Longmont has a relatively small manufacturing inventory comprising around 657,000 square feet of space, representing 7% of Longmont's total industrial inventory. While Longmont's inventory may be small, it is a diversified manufacturing base including food production, craft breweries, high tech manufacturing (storage batteries, surgical instruments), computers and electronics, energy components and agricultural technology.

As noted in the *Longmont Area Market Assessment*, estimating market demand for manufacturing is challenging due to the unique nature and customizable requirements of manufacturing buildings, most of which are owner-occupied and are not typically marketed, sold or leased in the competitive industrial

market. Most manufacturing buildings are built to suit type construction as they typically have significant capital investment in equipment and other building requirements needed by the business to operate the facility. In addition, there are many site selection factors that businesses evaluate when determining to locate a manufacturing building, such as access to a good labor pool, building and site characteristics, existing infrastructure, building and land costs, zoning regulations, available incentives and access to rail and transportation routes.

To estimate manufacturing demand in the Longmont area, the *Longmont Area Market Assessment* utilized Emsi employment projections for manufacturing and historic absorption trends for manufacturing space. Emsi projections showed manufacturing employment remaining relatively flat for the Longmont area, increasing only 1% between 2020 and 2030 with historic net absorption trends of around 13,500 square feet of space annually. Overall, it was estimated that the Longmont area could potentially capture anywhere from 90%-120% of the average net absorption which translates into 12,000-16,000 square feet annually, or 120,000 - 160,000 square feet over the 2020-2030 period. The assessment noted that this demand does not consider any specialized build to suit manufacturers that decide to build a manufacturing facility in Longmont.

Based on recent Emsi employment projections for the Longmont area, manufacturing employment is projected to grow 12% during 2021-2031 (compared to 1% for period 2020-2030 used in the market assessment). While manufacturing employment is just one factor used to estimate demand for manufacturing space, stronger employment growth in this sector suggests stronger manufacturing demand for the Longmont area above what the market assessment projected. In addition, the estimated vacancy rate for manufacturing space in Longmont is 1.87% (Catalist, 4Q 2021) which supports additional demand for manufacturing space due to the limited inventory of high-quality space in the Longmont area.

Target Opportunities for STEAM + Sugar Mill Study Area

Longmont has larger, older industrial buildings that may not be designed by today's modern standards and is less adaptable to attract industrial tenants. The STEAM + Sugar Mill study area offers target opportunities to capture Longmont's industrial demand potential over the next decade with adaptive reuse of existing buildings and construction of new industrial space desired by today's industrial users with a focus on developing modern, tech-centric and sustainable industrial space.

Specific target opportunities include:

- Focus on developing modern smaller-scale flex/warehouse/manufacturing opportunities of 25,000 square feet or less that can be built at a lower cost to attract smaller, entrepreneurial industrial users (and sub-divided to smaller tenant spaces such as 2,000 square feet or less)
- Agricultural / entrepreneurial hub for small scale food producers / research and development with space for research and development, processing, production, and ghost kitchens / food labs
- Explore potential to develop micro-fulfillment centers and distribution hubs - smaller light-industrial warehouse space growing in demand with rise of e-commerce and need to be located closer to customers
- Adaptive reuse of older warehouse space (such as the former sugar mill site) to accommodate increased industrial demand for warehousing, fulfillment, and small-scale manufacturing with focus on increased technology to store and move goods, with high clear height (24 feet or higher), multiple loading capabilities, sustainable design and health and safety improvements
- Capitalize on continued growing national and regional demand from technology and life science companies for flex/R&D space to locate to the STEAM + Sugar Mill study area
- Smaller-scale manufacturing opportunities such as natural sciences, renewable energy, natural and organic foods, and beverage (breweries, distilleries) with Longmont's supportive entrepreneurship and business climate and incentives for smaller-scale manufacturers

IV. Retail Market

Market Overview

Longmont has approximately 6.25 million square feet of retail space located in a wide variety of retail product spaces, such as community retail centers, strip centers, power centers (e.g., big box stores), grocery anchored neighborhood centers, restaurants, freestanding retail buildings, gas stations, auto repair, cannabis dispensaries, bars, and mixed-use centers.

Approximately 834,000 square feet of this space, or 13% of Longmont's total retail space, is located neighborhood centers, community/specialty centers, and restaurants. This smaller scale type of retail space is the type of retail product type envisioned for the STEAM + Sugar Mill study area and is the focus on this market overview. Based on the latest market data from Catalist, approximately 163,530 square feet of this neighborhood retail space in Longmont is vacant, representing a vacancy rate of 20%. This includes large retail vacancies in neighborhood and community shopping centers such as Burlington Center, Parkway Promenade (former Lucky's grocery store), Horizon Park shopping center, Westview Plaza, and Centennial Square.

The pandemic accelerated the retail property sector's long-term slide as store closings and vacancies soared through the end of 2021. While continued weakness in the retail market is expected to continue with increasing vacancy rates, certain sub-sectors of the retail market, such as grocery anchored centers, dollar stores, and home improvement retailers are all thriving. The significant shift to e-commerce during the pandemic, compounded with existing oversupply, shifting consumer demand and other operational issues has challenged the retail sector created unprecedented uncertainty that was exacerbated by the immense logistical, health and staffing challenges presented by the pandemic.

Longmont's retail market is not immune to these structural changes, changes in consumer preferences and trends that have been accelerated during the pandemic as evidence by the high vacancy rate for what is primarily older, dated retail centers. Retail was able to survive during the pandemic in part due to unprecedented level of government support packages that helped consumers continue spending through the recession. For example, ULI's *2022 Emerging Trends in Real Estate* reports that retail sales quickly rebounded after the pandemic lockdown, with overall sales (including e-commerce) recovering to pre-COVID levels by the fall of 2021 and physical retailers matching pre-COVID levels by mid-year 2021.

Looking ahead, the "great retail reset" continues to unfold as convenience (accelerated by e-commerce and BOPIS "buy online, pick up in store") and movement toward more mixed-use centers and "neighborhood-centric" shopping gains steam. As one retail owner noted in *2022 Emerging Trends in Real Estate*, "We won't see as many megabrand stores. But we might see more brands in general, including from digitally native brands. So, we'll have fewer, but much more impactful stores." This trend may drive more diverse tenant mixes for retail shopping centers in the future. The challenge remains how to repurpose all that excess retail space that Longmont and many other communities throughout the US are seeing. Adding mixed uses elements, such as residential, office and micro-fulfillment, and density could help address this challenge and, in the end, create more compelling and attractive retail centers.

Target Opportunities for STEAM + Sugar Mill Study Area

The STEAM + Sugar Mill study area offers the opportunity to provide community and neighborhood serving retail and food and beverage space as part of redevelopment of the sugar mill site and identified redevelopment opportunities within the STEAM area. Some of these target opportunities were identified in the Urban Land Institute Colorado – *Revitalizing Longmont's Great Western Sugar Mill Technical Advisory Report* (August 20-21, 2020) that identified the desire to create a community hub centered around the historic buildings with integrated retail and support for local businesses.

Specific target opportunities include:

- Food and beverage driven opportunities, such as restaurants, coffee shops and espresso bars, and food halls that curate food tenants around a theme or concept, with a mix of upstart and more established tenants who combine to create a unique place. The historic sugar mill site offers an excellent opportunity to showcase Longmont's agricultural heritage with a focus on natural and organic foods
- Incubator kitchens and culinary "maker spaces" – a mash up of food hall, coworking, and business incubator – that provides aspiring chefs and small batch food companies a permitted and ready to use kitchen for a lower cost and production spaces with access to everything a growing food-making business needs: flexible cold, dry and frozen storage, demonstration kitchens, coworking space, and warehouse space to assist with receiving and logistics
- Fast growing neighborhood and community-serving retail sectors such as service and specialty tenants including healthcare and wellness (minute clinics), education (e.g., Mathnasium, Kumon), fitness centers, garden and housewares, as well as specialty tenants (e.g., Code Ninjas, organics) that are focused on creating a more personalized experience for consumers through improved services such as great design and online content, providing staffing merchandising and providing great analytics
- Focus on authenticity and building a sense of place with expanded niche retail opportunities and creating great experiences for the consumer - experiential and interactive retail, health and wellness, active entertainment, and curated goods and services that is functional (e.g., provides the shopping opportunity), convenient (BOPIS, grocery and prepared meal deliveries) and is a unique experience that draws the customer in
- Opportunities to capture retail leakage (*Downtown Longmont Market Study*, April 2020) with focus on experiential retail, active entertainment, and smaller, independent shops (e.g., clothing boutiques, specialty food and beverage stores, health and wellness, and restaurants) to capture shifting retail market and consumer preferences

5.5 GREEN INFRASTRUCTURE STRATEGIES

STEAM Cultural District

Green Infrastructure Strategies

Green Infrastructure within the STEAM development area should adhere to best practices outlined by the Mile High Flood District and EPA. Due to the site's proximity to the Spring Gulch floodway & the St Vrain Creek, Green Infrastructure should focus first and foremost on Low Impact Development strategies, applying more intensive strategies toward the development's core.

- Green infrastructure strategies within the **upland residential blocks** should focus on reducing downstream storm rates by preserving open space, reducing impervious surfaces, and utilizing softscape bioswales and shallow rain gardens where possible.

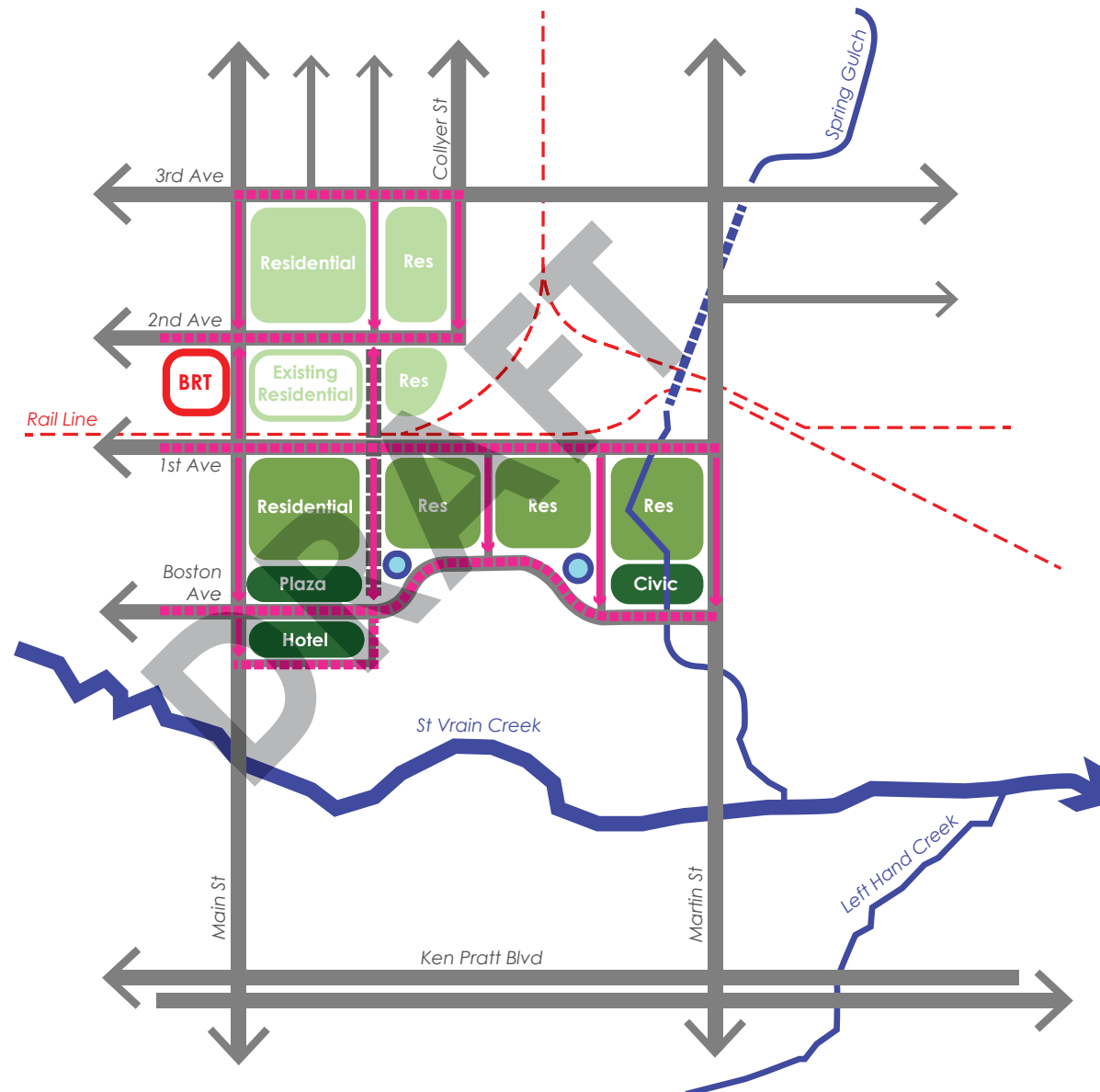
- Strategies within the **middle residential blocks** should balance open space preservation with the denser nature of development. Disconnected roof drainage, streetside storm planters, and infiltration rain gardens provide further WQ treatment & Storm capacity.

- Strategies within the **creekside civic block** should focus on applying intensive water quality and stormwater detention within open spaces that provide highly active & accessible public realm experiences. Permeable paving, cisterns, streetside storm planters, and infiltration rain gardens provide further WQ treatment & Storm capacity.

- Boston Ave properties may provide additional area for regional extended detention basin consideration in this area where EDB opportunities are limited.

Green Infrastructure Legend

- Low Impact / Extensive Permeability
- Moderate Impact / Combination
- High Impact / Focused GI Areas
- Streetside Collect & Convey
- Streetside Treat WQ & Infiltrate
- Extended Detention Basin



Sugar Factory Cultural Campus

Green Infrastructure Strategies

Green Infrastructure within the Sugar Mill development area should adhere to best practices outlined by the Mile High Flood District and EPA. Due to the site's proximity to existing wetlands & the St Vrain Creek, strategies should focus on wetland resources and reducing storm flows into the Creek.

- Green infrastructure strategies within the **outer residential & agricultural blocks** should focus on reducing downstream storm rates by preserving open space, reducing impervious surfaces, and utilizing softscape bioswales and shallow rain gardens where possible.

- Strategies within the **inner residential blocks** should balance open space preservation with the denser nature of development. Disconnected roof drainage, streetside storm planters, and infiltration rain gardens provide further WQ treatment & Storm capacity.

- Strategies within the **cultural core** should focus on applying intensive water quality and stormwater detention within open spaces that provide active & accessible public realm experiences. Permeable paving, cisterns, streetside storm planters, and infiltration rain gardens provide further WQ treatment & Storm capacity.

Existing wetlands may be reconstructed as a wetland park to provide WQ & storm capacity while rehabilitating wetlands into a parklike amenity for the greater community.



Green Infrastructure Legend

- Low Impact / Extensive Permeability
- Moderate Impact / Combination
- High Impact / Focused GI Areas
- Agricultural Production *
- Streetside Collect & Convey
- Streetside Treat WQ & Infiltrate
- Extended Detention Basin

